



Staff Report

To: Planning Commission

Through: Chris Kerr, Community Development Director *C.K.*

From: Colin Cortes, AICP, CNU-A, Senior Planner

Meeting Date: February 27, 2020 (Prepared February 20)

Item: 1785 N. Front Street “Woodburn High School” (CU 2019-07, DR 2019-13, EXCP 2019-07, & PP 2019-03)

Tax Lot(s): 051W07A 00600

TABLE OF CONTENTS

ISSUE BEFORE THE PLANNING COMMISSION	2
EXECUTIVE SUMMARY	2
RECOMMENDATION	3
CONDITIONS OF APPROVAL	6
ACTIONS	14
ATTACHMENT LIST	14

Issue before the Planning Commission

Action on a consolidated land use application package, Conditional Use CU 2019-07 (Type III) for the use of a public K-12 school, specifically Woodburn High School (H.S.) in the Public and Semi-Public (P/SP) zoning district, with Design Review DR 2019-13, Exception to Street Right of Way and Improvement Requirements (“Street Exception”) EXCP 2019-07, and Phasing Plan PP 2019-03.

Executive Summary

The subject property is at the northwest corner of the interchange of N. Front Street and Oregon Highway 214 / Newberg Highway and also fronts Hazelnut Drive. It is situated north of St. Luke’s Cemetery, east of Meridian Drive, south of the Ironwood at Tukwila Subdivision, and west of the railroad. It is occupied by a high school campus dating from 1977.

The proposal is about partial site redevelopment including a main building addition/expansion at the north, removal of a few existing modular buildings, addition of a north outbuilding, and adding a driveway on Hazelnut Drive aligned with Graystone Drive. The addition/expansion is in place of the demolished remains of a portion of the main building destroyed by fire accidentally set by students on May 11, 2012. The original portion was one story, and the addition/expansion is two.

The project comes with a Street Exception request to not upgrade the N. Front Street frontage to present standards and a Phasing Plan request for a project of Phases 1 & 2 – including to defer to Phase 2 all frontage/public/street improvements that remain required. The application materials indicate that Phase 2 would be as late as 2027, and there is no condition requiring that the applicant do any of construct Phase 2, construct the public improvements, or pay a fee in-lieu.

The recommended conditions include obtaining a public access easement 20 feet wide minimum along the Mill Creek Greenway Trail corridor, and the Assistant City Administrator, who oversees parks and recreation, had declined to require the applicant either to construct bicycle/pedestrian trail improvements based on the [Mill Creek Greenway Master Plan](#) or pay a fee in-lieu.

Conditions do obtain on-site improvements including in public/visitor and staff parking areas in the form of marked walkway crossings of drive aisles, additional tree islands and bicycle parking, two staff carpool/vanpool (C/V) stalls, additional lighting being “dark sky”, and upgrades to certain walkway routes and ramps to bring them into compliance with the Americans with Disabilities Act (ADA).

The applicant is the Woodburn School District 103.

Hazelnut Drive

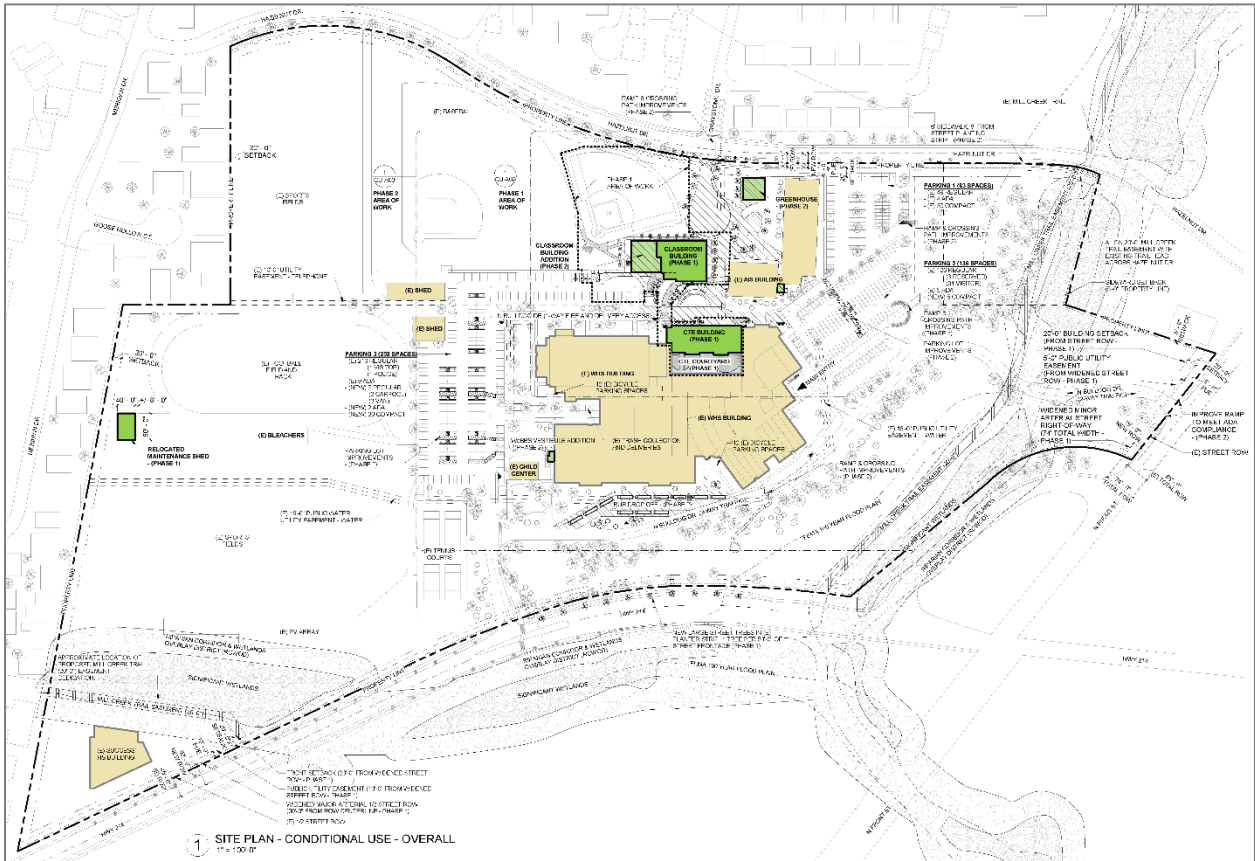


N. Front Street / railroad

Oregon Highway 214/ Newberg Highway

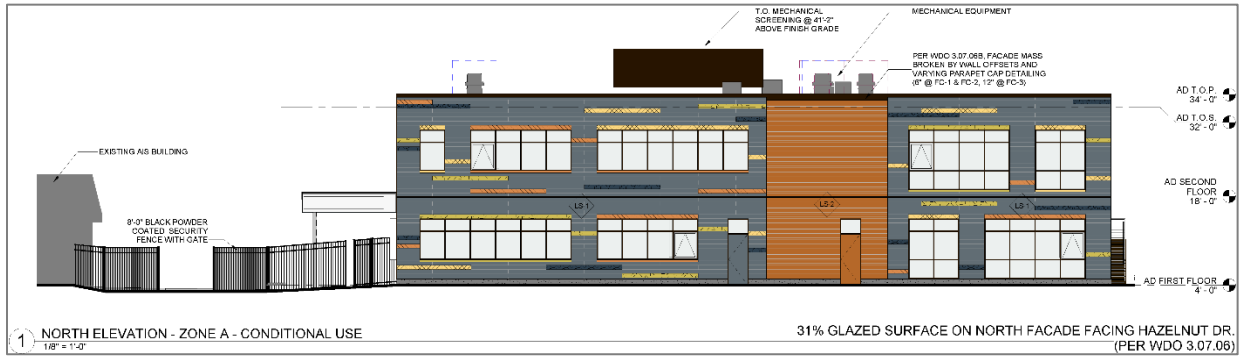


Vicinity Map

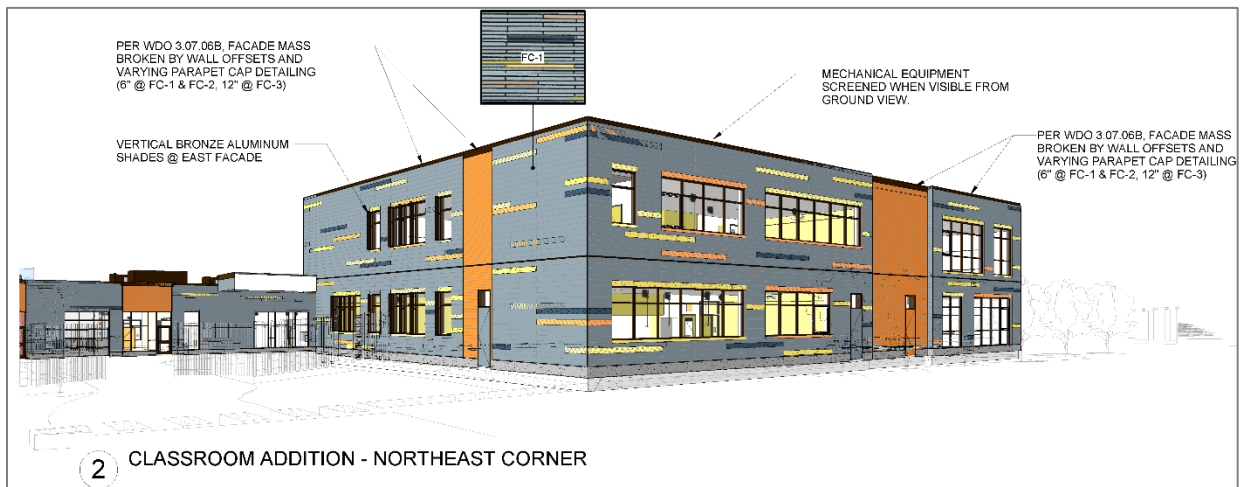


Overall site plan (Sheet CU-A01) cropped and colored by staff





Outbuilding elevation view: North façade facing Hazelnut Drive



Outbuilding perspective view: North façade facing Hazelnut Drive

EXTERIOR ELEVATIONS LEGEND	
NOTE: COLORS ARE SHOWN AS AN APPROXIMATION TO TRUE MATERIAL COLOR ONLY.	
	FIBER CEMENT - ALLURA COLOR: PACIFIC BLUE W/ 15% CUSTOM (BELOW) SMOOTH LAP SIDING - 8" LAP EXPOSURE
CUSTOM COLORS:	
	FIBER CEMENT - ALLURA COLOR: MAHOGANY GRAIN LAP SIDING - 4" LAP EXPOSURE
	FIBER CEMENT - ALLURA COLOR: CAVALRY SMOOTH PANEL SIDING - 4'X8' PANEL W/ ALUMINUM REVEL

Colors and materials (from Sheet CU-A09)



Google Street View (2016) east along the highway of street trees missing from the planter strip



Google Street View (2016) east along Hazelnut Drive of sidewalk gap up to Mill Creek culvert crossing sidewalk

Recommendation

Approval with conditions: Staff recommends that the Planning Commission consider the staff report and its attachments and approve the application with the conditions recommended by staff below. The conditions can also be found towards the end of the Analyses & Findings (Attachment 102).

Conditions of Approval

General

G1. Either prior to or as part of building permit application, the applicant shall submit revised site plans meeting the conditions of approval to the attention of the Planning Division and obtain Division approval or permit sign-off.

G2. The applicant or successor shall develop the property in substantial conformance with the final plans submitted and approved with these applications, except as modified by these conditions of approval. Were the applicant to revise plans other than to meet conditions of approval or meet building code, even if Planning Division staff does not notice and signs off on building permit issuance, Division staff retains the right to obtain restoration of improvements as shown on an earlier land use review plan set in service of substantial conformance.

G3. References: The descriptions below define certain words, phrases, and assumptions in the context of the conditions of approval:

- “ADA” refers to the federal Americans with Disabilities Act of 1990.
- “Director” refers to the Community Development Director.
- “District” refers to the Woodburn School District.
- “Highway” refers to Oregon Highway 214 / Newberg Highway.
- “H.S.” refers to high school.
- “Public Works” refers to the Public Works Department, especially the Engineering Division.
- “PUE” means public utility easement.
- “ROW” means right-of-way.
- “WDO” refers to Woodburn Development Ordinance.

G4. Due date: Unless a condition specifies otherwise, meeting the conditions is due by building permit issuance for the implied phase.

G5-PW. Public Works: Follow the appended "Public Works Comments February 18, 2020" (Attachment 102A).

G6. Recordation(s): Dedications of ROWs, PUEs, and other easements involve recordation(s) with Marion County. The applicant shall:

- a. Prior to recordation with the County, follow City Public Works Department review and dedication process as directed by the City Engineer or higher authority, including the use of Public Works document templates;
- b. Submit a draft copy of the ROW dedications and easements, including text and drawing(s), to the attention of both the Director and City Engineer for their reviews and directions. Easement text shall contain references to “City of Woodburn Conditional Use CU 2019-07”, “Woodburn High School”, street address, and tax lot number. Submit through the Planning Division both Adobe PDF and written copies under a cover or transmittal letter providing project context, including land use / planning / zoning case file numbers, street

address, tax lot number, and project name and an explanation of the purpose of the submittal that refers to this condition of approval;

- c. Record dedications prior to building permit final inspection;
- d. Upon recordation, submit proof to pass building permit final inspection; and
- e. Apply to Marion County for recordation(s) no later than six (6) months past the Planning Commission tentative decision date of February 27, 2020, i.e. by August 31, 2020, and complete recordation(s) no later than a year past, i.e. by February 28, 2021. The due date to complete recordation(s) shall not supersede when recordation(s) are due relative to the building permit stage.

G7. Inspection: The applicant shall construct, install, or plant all improvements, including landscaping, prior to City staff verification. At least three (3) City business days prior to a desired date of planning and zoning inspection of site improvements, the applicant shall request a land use / planning / zoning inspection through the Accela / Oregon ePermitting building permit record(s) for the project. This inspection is required and separate from and in addition to the usual building code and fire and life safety inspections. Note that Planning staff are not primarily inspectors, do not have the nearly immediate availability of building inspectors, and are not bound by any building inspector's schedule or general contractor convenience. For help with ePermitting, call the Oregon Building Codes Division help line at (503) 373-7396.

Conditional Use 2019-07

C1. ADA:

- a. On-site: The applicant shall make ADA-compliant the walkway ramp transition at the location Sheets CU-A01 & 3 illustrate and note for Phase 1.
- b. Highway and road sidewalks: See Street Exception (EXCP) conditions.

C2. Bicycle parking:

- a. Ratio: Required off-street bicycle parking shall number a total of the existing amount of twenty-four (24) stalls plus at least two (2) stalls per additional classroom. (This would equal 24 existing stalls, plus 18 stalls for Phase 1 nine classrooms, and plus for Phase 2 six classrooms 12 stalls yielding 54 stalls total.)
- b. Distribution: The number shall be divided among at least four (4) areas: the main entrance with sixteen (16) stalls minimum, a southeast entrance with twelve (12) stalls minimum, a west staff entrance with twelve (12) stalls minimum, and an addition/expansion or outbuilding entrance with fourteen (14) stalls minimum.
- c. Facility: Each stall shall adjoin a facility designed for the securing of one or more bicycles, such as a U-rack or wave rack.
- d. Stall size: The minimum size of additional or altered stalls shall be two (2) feet wide by six (6) feet long, and minimum vertical clearance shall be eight (8) feet. If a stall is wall-

mounted, the minimum stall depth shall be five (5) feet. The minimum maneuvering area to access any stall shall be at least five (5) feet deep. The dimensional standards do not apply to the existing bicycle parking at the south and west.

This condition is due by building permit final inspection for each phase because additional bicycle parking is allocated between the two phases.

C3. Bond / bonding / performance guarantee: Were the applicant to bond any improvements as allowed by and pursuant to WDO 4.02.08, the amount shall be 200% instead of 120%.

C4. Carpool/vanpool (C/V) parking:

- a. Number: Within a staff-only parking area there shall be at least two C/V stalls located within ninety (90) feet of a building occupied by District staff and limited to carpools and vanpools.
- b. Striping: Each stall shall be striped "CARPOOL/VANPOOL ONLY" in lettering one (1) foot high minimum.
- c. Signage: Each stall shall have a wall-mounted or pole-mounted sign for "Carpool/Vanpool Only". Minimum sign face size shall be one (1) foot wide by one-and-a-half (1½) feet high. The top of a posted sign shall be between five-and-a-half (5½) and six-and-a-half (6½) feet above vehicular grade.
- d. If there are State of Oregon standards that conflict with b. and c., the applicant may go by state standards instead and provide a copy of them to staff.

This condition is due by building permit final inspection for each phase because the improvements manifest in Phase 1 and are displaced and reproduced in Phase 2.

C5. Driveway widths:

- a. Two-way: Any additional driveway or reconstructed driveway shall have a maximum width of either twenty-four (24) feet or, were the Oregon Fire Code (OFC) to apply such that a fire apparatus access road of minimum 26 feet would be required such as for a multi-story building, then twenty-six (26) feet. The maximum may increase by up to eight (8) feet were a driveway to include an outbound left-turn lane or pocket.
- b. One-way: Any additional driveway or reconstructed driveway shall have a maximum width of twelve (12) feet if one-lane and twenty (20) feet if two-lane.

C6. Fire access plan: Through the City building permit application and review process, the applicant shall submit for the Woodburn Fire District Fire Marshal a detailed plan sheet illustrating and noting: fire apparatus access and lane routes, lane widths, lane inside and outside turning radii of the minimums per the Oregon Fire Code (OFC), lane height clearance, fire suppression water line, hydrants, a PUE for the fire suppression water line and hydrants, fire department connections (FDCs), and fire riser rooms.

C7. Greenway: The applicant shall by Phase 1 dedicate to the City an easement for public pedestrian and cyclist access along the likely alignment of the Mill Creek Greenway Trail based on the image on plan p. 17, "North Tributary: Goose Creek Mill Creek" (Attachment 107), and existing topography. The easement or easements shall extend at least (a) between Hazelnut Drive ROW and highway ROW and (b) between highway ROW along the Goose Creek corridor to the west property line. The easement may be continuous from Hazelnut Drive ROW to the west property line if it has a segment that parallels roughly the highway ROW and extends to or abuts the highway ROW. The minimum greenway trail easement width shall be twenty (20) feet.

C8. Lighting: This applies to additional or altered exterior lighting fixtures.

- a. Full cut-off: They shall be full cut-off or fully shielded models.
- b. Heights: As measured to the underside of the light fixture:
 - i. Wall: Wall-mounted fixtures shall be no higher than eight (8) feet above walkway finished grade. (This height limit is not applicable to emergency egress lighting and permanent wall signs allowed through WDO 3.10 were they to have interior illumination.)
 - ii. Parking pole: Exterior pole-mounted fixtures within five (5) feet of or in parking, loading, and vehicular circulation areas shall be no higher than fourteen (14) feet above vehicular finished grade.
 - iii. Other pole: Remaining exterior pole-mounted fixtures, excepting sports field lighting, shall be no higher than fifteen-and-a-half (15½) feet above grade.
- c. Monument signage: Any monument signage illuminated by lighting exterior to it shall be full cut-off or fully shielded, and any monument signage that has electronic changing image shall have it off from 9:00 p.m. to 7:00 a.m. Pacific time.

This condition is due by building permit final inspection.

C9. Modular buildings / modulars: Consistent with the land use review plan set submitted February 4, 2020, Sheets CU-A00 Overall Existing Site Plan and CU-A01 Overall Master Plan, the number of modular buildings shall decrease by at least ten (10) by Phase 1. The removal of modular buildings is due by building permit final inspection for Phase 1. (The City may later supersede this condition by taking action on a future land use application submittal as to allow one or more modular buildings.)

C10. Parking area improvements:

- a. East front student/visitor: As proposed, per Sheets CU-A01 & 3 and L01 & 2, the applicant shall modify the parking area to stripe a walkway at least six (6) feet wide across the drive aisles and install and plant four landscaped islands with trees. The middle two islands shall each be at least fifty-two (52) square feet within inside of curbing with narrowest dimension of six-and-a-half (6½) feet minimum within inside of curbing. The trees shall conform to Table 3.06B. Additional or altered compact stalls shall be striped with "C", "COMPACT", or "COMPACT ONLY".

- b. West rear staff: As proposed, per Sheets CU-A01 & 3 and L01 & 2, the applicant shall modify the parking area to stripe walkway crossings of the east drive aisle.

This condition is due by building permit final inspection for Phase 2.

C11. Signage: Based on WDO 3.10 and the needs of the Public Works Department:

- a. Permanent signage for the school campus shall not encroach within any PUE.
- b. Electronic changing image: Based on the hours in Ordinance No. 2338, Section 5A Light Trespass, any monument sign electronic changing image shall be off every evening or night during the hours of 9:00 p.m. to 7:00 a.m. Pacific time.
- c. The applicant shall through the sign permit process of 5.01.10 demonstrate that any new, relocated, or structurally altered permanent signage conforms to WDO 3.10.

C12. Discontinuance/Revocation: Because the WDO does not specify if and when a conditional use approval would expire were a use to cease, based on WDO 4.02.04B the approval shall expire if the use of public school ceases and three (3) years pass without the use recommencing.

C13. CU Modification: Because the WDO, including 5.03.01, does not specify how changes to an approved conditional use (CU) and related site improvements might trigger another CU or modification of a CU approval, for Director determination the following serve as criteria and – where noted – as factors:

- a. Significant expansion of the use, factors being an increase in any of: total gross floor area (GFA) by 25.0% or more or by an absolute value of ten thousand (10,000) square feet or more, the number of buildings (excluding modulars) by two (2) or more, the number of modular buildings by one (1) or more, and the number of classrooms by six (6) or more;
- b. Increase in off-street parking by twelve (12) or more stalls, even if the existing supply were in excess of the minimum required ratio(s);
- c. Net Increase in impervious surface totaling at least two-and-half (2½) acres;
- d. Development as defined in WDO 1.02 within twenty (20) feet of a property boundary and not already conditioned through the subject approval;
- e. Installation of additional or altered sports field lighting taller than twenty (20) feet and within twenty (20) feet of ROW or other property line;
- f. Any proposal necessitating a request for Exception to Street Right-of-Way and Improvement Requirements (“Street Exception”);
- g. Any proposal necessitating a request to vary from the WDO;
- h. Any proposal necessitating a Type III or IV land use application type;
- i. Any proposal increasing either student capacity or actual student population by ninety (90) or more students;
- j. Any proposal predicted based on the Institute of Transportation Engineers (ITE) *Trip Generation Manual* to increase peak hour vehicle trip generation such that per WDO 3.04.05A the City could require a traffic impact analysis (TIA);

- k. An application for Modification of Conditions pursuant to WDO 4.02.07;
- l. City adoption of a unified development ordinance replacing the WDO were to have intervened; and
- m. Proposal by the Woodburn School District for the City to approve a District-led long-range master plan for the Woodburn H.S. campus.

Modification of a specific condition of approval remains pursuant to WDO 4.02.07. Were the City to have amended the WDO to establish modification provisions for conditional uses, the Director may decide that the provisions supersede this condition of approval.

C14. Architecture: As proposed, the north outbuilding labeled as “classroom building” shall meet or exceed the window area percentage provision of WDO 3.07.06B.2b(1) on the north façade that faces Hazelnut Drive.

C15. Driveway: The additional driveway along Hazelnut Drive shall be aligned with a public street that intersects Hazelnut Drive and shall have a maximum width of either twenty-six (26) feet or, if later changed to have an outbound left-turn pocket between an inbound and an outbound lane, thirty (30) feet.

Design Review 2019-13

D1. Fences / fencing: To meet WDO 2.06.02C.1 (height maximum), 2.06.02D.2 (coating and slats), vision clearance area (3.03.06), and, the applicant shall through the fence permit process of 5.01.03 demonstrate the proposed fencing conforms.

D2. ROWs & PUEs: To meet WDO 3.01, the applicant shall illustrate, note, and dedicate for:

- a. Hazelnut Drive, additional ROW of three (3) feet and a PUE of five (5) feet width;
- b. N. Front Street, additional ROW of nineteen (19) feet and a PUE of five (5) feet width; and
- c. the highway, additional ROW of five (5) feet and a PUE of ten (10) feet width.

D3. Screening of parking: To meet WDO 3.06.05B, the applicant shall screen surface parking in the north yard from Hazelnut Drive. The applicant shall revise Sheet CU-L02 to clarify that proposed shrubbery amid the row of trees would be evergreen shrubbery forming a screen hedge or row as a vertical visual screen, growing to a minimum height at maturity of three-and-a-half (3½) feet from the abutting street grade.

D4. Screening of roof-mounted equipment: To meet WDO 3.07.06B.4 fully, the applicant shall revise the building elevation Sheets CU-A07 through 9 to clarify that all such equipment will be screened.

D5. On-site PUEs other than streetside ones: To meet WDO 3.02.01 & 3.02.02, were the City Engineer to identify later a nonconforming situation in need of correction regarding any on-site utilities and easements in order to protect public health, safety, and welfare, upon City Engineer written direction, the applicant shall record a document or documents that accomplish the directed correction.

Street Exception 2019-07

The street exception request is approved for the three frontages as modified by the following conditions:

EX1. ROWs & PUEs: Approval of the street exception request shall not exempt the proposal from dedications of required ROWs and PUEs.

EX2. Hazelnut Drive: The required improvements shall be per the land use review plan set submitted February 4, 2020, Sheets CU-A01 & 3 and L01.

EX3. N. Front Street: The required improvements shall be per the land use review plan set submitted February 4, 2020, Sheets CU-A01 & L01.

EX4. Highway: The required improvements shall be per the land use review plan set submitted February 4, 2020, Sheets CU-A01 & L01.

EX5. Street trees:

- a. Specifications: The applicant shall plant in conformance with WDO 3.06.03A and Tables 3.06B & C:
- b. Public Works Standard Specifications & Drawings: The applicant shall revise the site plans to reproduce standard details as Public Works directs and include Drawing 1 Street Tree Planting New Construction and Detail No. 4150-8 Sidewalks.

EX6. ADA: The applicant shall upgrade to provide ADA compliance where it does not exist along sidewalks and pedestrian crossings of streets and do so by any of Phase 1 or 2.

EX7. Interpretation: Where there is uncertainty, contradiction, or conflict concerning what street improvements are or are not excepted, the Director shall determine upon guidance by the Public Works Department.

EX8. Future applications: Any future land use / planning / zoning application for the subject property that triggers street improvements supersedes this Street Exception 2019-07.

Phasing Plan 2019-03

PP1. Phasing:

- a. Number: The City acknowledges the phasing plan of two Phases 1 & 2.
- b. Improvements by phase:
 - i. Improvements are due by the phases with which they are associated as noted and delineated per the revised land use review sheets submitted February 4, 2020: CU-A01, 2, & 3 and CU-L02.
 - ii. Clarification: Though frontage/public/street improvements are deferred to Phase 2, the applicant shall record any and all required ROW, PUE, and Mill Creek Greenway Trail easement dedications prior to Phase 1 building permit final inspection.
- c. Timeframe: The City acknowledges the build-out timeframe per the applicant's letter of January 22, 2020 (p. 7) that was in response to the incompleteness letter of January 9, 2020: Phase 1 is through June 2021, and Phase 2 is through 2027, which through this condition staff interprets to mean through September 3, 2027. The applicant shall revise the site plans to indicate the phasing timeframe that the letter had indicated.

Actions

The Planning Commission may instead act on the land use application to:

1. Approve per staff recommendations,
2. Approve with modified conditions, or
3. Deny, based on WDO criteria or other City provisions.

If the Planning Commission were to act upon the recommendation, staff would prepare a final decision for approval with the conditions that staff recommends.

Attachment List

101. Marked Tax Map

101A. Partition Plat 2012-031 (5 sheets)

102. Analyses & Findings

102A. Public Works Comments February 18, 2020

103. Applicant narrative (revised Jan. 22, 2020; 22 pages)

104. Transportation System Plan (TSP) Figure 2 (2019)

105. TSP Figure 8 "Pedestrian Plan Elements" (2019)

106. Traffic letter

107. Mill Creek Greenway Master Plan (2006-07; excerpt of north map)

108. Site plans excerpted (submitted through Feb. 4, 2020; 12 sheets)

05 1W 07A WOODBURN



MARION COUNTY, OREGON
NE1/4 SEC7 T5S R1W W.M.
SCALE 1" = 200'

LEGEND

- LINE TYPES**
- Taxlot Boundary
 - Road Right-of-Way
 - Railroad Right-of-Way
 - Private Road ROW
 - Subdivision/Plat Bndry
 - Waterline - Taxlot Bndry
 - Historical Boundary
 - Easement
 - Railroad Centerline
 - Taxcode Line
 - Map Boundary
 - Waterline - Non Bndry

- CORNER TYPES**
- + 1/16TH Section Cor.
 - ⊙ DLC Corner
 - ⊕ 1/4 Section Cor.
 - ⊕ 16, 15 Section Corner
 - ⊕ 21, 22

NUMBERS

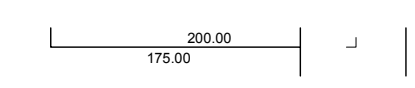
Tax Code Number
000 00 00 0

Acreege
0.25 AC

All acres listed are Net Acres, excluding any portions of the taxlot within public ROWs

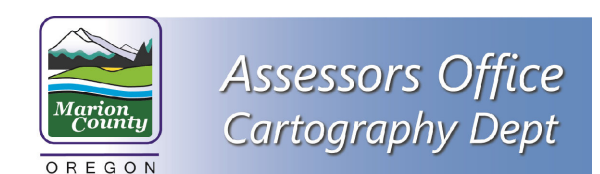
NOTES

Tick Marks: A tick mark in the road indicates that the labeled dimension extends into the public ROW



CANCELLED NUMBERS	
300	
400	
500	
800	
1000	
1100	
1200	
3201	

DISCLAIMER: THIS MAP WAS PREPARED FOR ASSESSMENT PURPOSES ONLY



FOR ADDITIONAL MAPS VISIT OUR WEBSITE AT www.co.marion.or.us Attachment 101

PLOT DATE: 1/23/2018

WOODBURN 05 1W 07A

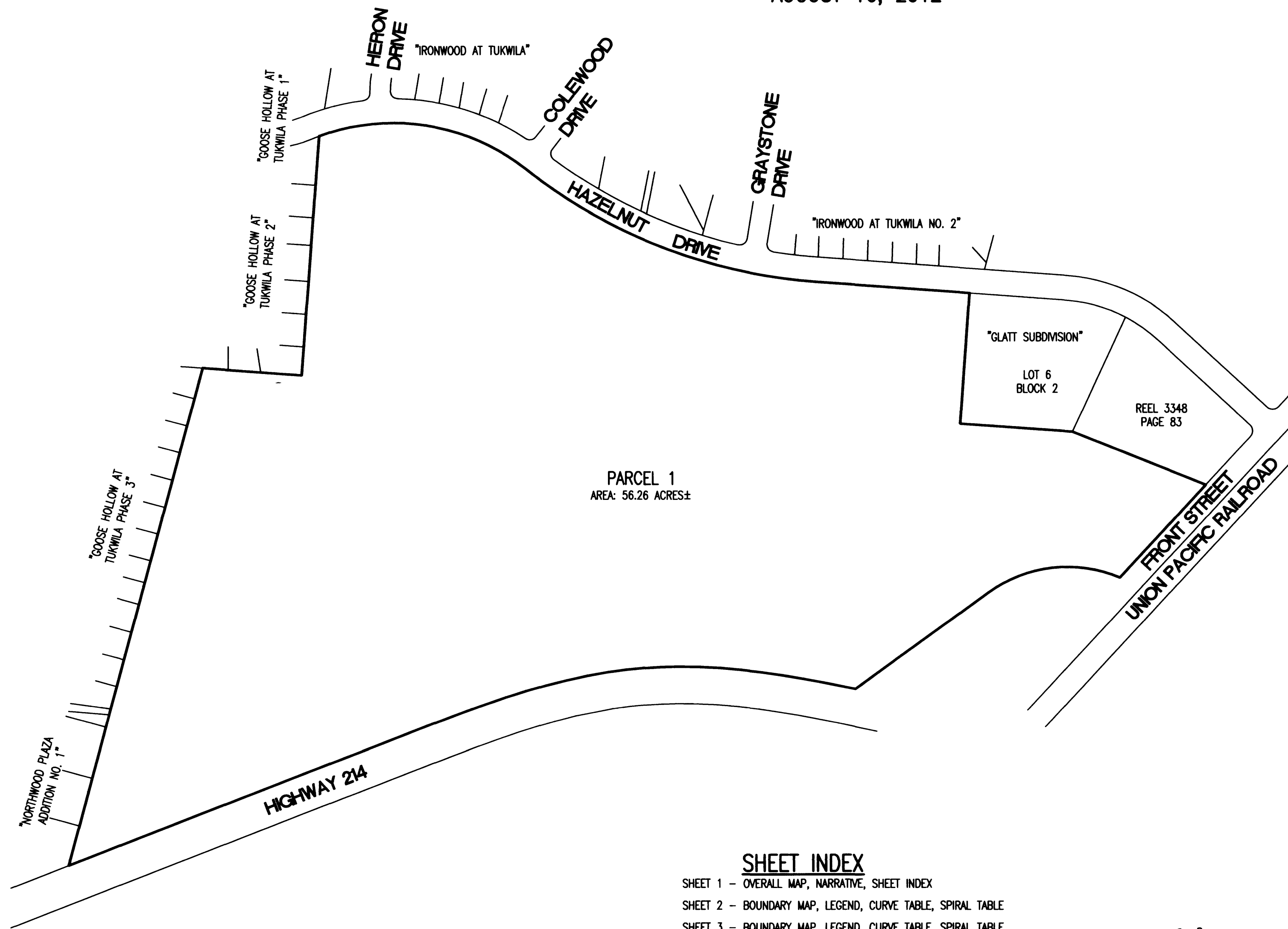
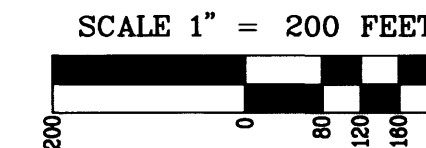


2012-31

PARTITION PLAT NO. 2012-31

PREPARED FOR
WOODBURN SCHOOL DISTRICT
965 N BOONES FERRY ROAD
WOODBURN, OR 97071

A REPLAT OF LOTS 2, 3, 4, AND 5, BLOCK 2, OF THE PLAT "GLATT SUBDIVISION", AND OTHER LANDS, LOCATED IN THE NORTHEAST AND SOUTHEAST ONE-QUARTERS OF SECTION 7, AND THE NORTHWEST ONE-QUARTER OF SECTION 8, TOWNSHIP 5 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, CITY OF WOODBURN, MARION COUNTY, OREGON
AUGUST 16, 2012



NARRATIVE

THE PURPOSE OF THIS PARTITION WAS TO PLAT THE TRACT OF LAND PER REEL 26 PAGE 806 (10/02/1975), REEL 26 PAGE 808 (10/02/1975), REEL 26 PAGE 809 (10/02/1975), REEL 26 PAGE 811 (10/02/1975), REEL 298 PAGE 1463 (12/27/1982), REEL 1130 PAGE 530 (12/28/1993), REEL 1258 PAGE 258 (12/29/1995), MARION COUNTY DEED RECORDS INTO ONE PARCEL. THE BASIS OF BEARING IS THE NORTHWEST RIGHT-OF-WAY LINE OF HIGHWAY 214 PER SURVEY NUMBER 36702, MARION COUNTY SURVEY RECORDS.

THE RIGHT-OF-WAY OF HIGHWAY 214 WAS ESTABLISHED BY HOLDING FOUND MONUMENTS, RECORD BEARINGS, DISTANCES, SPIRAL INFORMATION, AND CURVE INFORMATION PER SAID SURVEY NUMBER 36702. SURVEY NUMBER 36702 USES PROPER PROCEDURE BY HOLDING FOUND MONUMENTS AND RIGHT-OF-WAY GEOMETRY PER ODOT MAP 9B-8-14.

THE EAST LINES OF THE PLATS "NORTHWOOD PLAZA ADDITION NO. 1" AND "GOOSE HOLLOW AT TUKWILA PHASE 3" WAS ESTABLISHED BY HOLDING FOUND MONUMENTS PER SAID PLATS, AND SURVEY NUMBER 24758. IT APPEARS THE LEGAL DESCRIPTIONS OF REEL 26 PAGE 806 (10/02/1975) AND REEL 26 PAGE 811 (10/02/1975) ARE BASED UPON SURVEY NUMBER 24758, SAID PLATS HELD FOUND MONUMENTS PER SAID SURVEY NUMBER 24758 FOR BOUNDARY MONUMENTS.

THE SOUTH LINE OF THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 2" WAS ESTABLISHED HOLDING FOUND MONUMENTS PER SAID PLAT AND THE PLATS "TUKWILA" AND "GLATT SUBDIVISION". SAID PLAT HELD FOUND MONUMENTS PER SAID SURVEY NUMBER 24758 FOR BOUNDARY MONUMENTS.

THE EAST LINE OF THE PLATS "GOOSE HOLLOW AT TUKWILA PHASE 1" AND "GOOSE HOLLOW AT TUKWILA PHASE 2" AND WERE ESTABLISHED BY HOLDING FOUND MONUMENTS PER SAID PLATS AND THE PLAT "GLATT SUBDIVISION". SAID PLATS "GOOSE HOLLOW AT TUKWILA PHASE 1" AND "GOOSE HOLLOW AT TUKWILA PHASE 2" HELD FOUND MONUMENTS PER "GLATT SUBDIVISION" FOR BOUNDARY MONUMENTS.

THE CENTERLINE AND RIGHT-OF-WAY OF HAZELNUT DRIVE WAS ESTABLISHED BY HOLDING FOUND MONUMENTS AND CURVE INFORMATION PER THE PLATS "GLATT SUBDIVISION" AND "GOOSE HOLLOW AT TUKWILA PHASE 1".

THE WEST AND SOUTH LINES OF LOT 6 BLOCK 2 OF THE PLAT "GLATT SUBDIVISION" WAS ESTABLISHED BY HOLDING FOUND MONUMENTS AND OFFSETS PER SAID PLAT.

THE SOUTHWEST LINE OF REEL 3348 PAGE 83 WAS ESTABLISHED BY HOLDING FOUND MONUMENTS PER SURVEY NUMBER 34166. THIS RESOLUTION AGREES WITH SAID SURVEY NUMBER 24758.

THE CENTERLINE OF FRONT STREET WAS ESTABLISHED BY HOLDING FOUND MONUMENTS PER SURVEY NUMBER 31573 AND SURVEY NUMBER 2357.

THE NORTHWEST RIGHT-OF-WAY OF FRONT STREET WAS ESTABLISHED BY HOLDING A 30.00 FOOT OFFSET FROM CENTERLINE AS CONVEYED TO MARION COUNTY PER REEL 713 PAGE 686.

THE FRONT STREET CONNECTION TO HIGHWAY 214 WAS ESTABLISHED BY HOLDING A FOUND MONUMENT PER SURVEY NUMBER 36302 AND RECORD INFORMATION PER SURVEY NUMBER 36702.

SHEET INDEX

- SHEET 1 - OVERALL MAP, NARRATIVE, SHEET INDEX
- SHEET 2 - BOUNDARY MAP, LEGEND, CURVE TABLE, SPIRAL TABLE
- SHEET 3 - BOUNDARY MAP, LEGEND, CURVE TABLE, SPIRAL TABLE
- SHEET 4 - BOUNDARY MAP, LEGEND, CURVE TABLE
- SHEET 5 - CITY OF WOODBURN APPROVALS, MARION COUNTY APPROVALS, DECLARATION, ACKNOWLEDGEMENT, SURVEYOR'S CERTIFICATE, NARRATIVE

10-5-12
 REGISTERED PROFESSIONAL LAND SURVEYOR

 OREGON
 JANUARY 9, 2007
 NICK WHITE
 70652LS
 RENEWS: 6/30/14

JOB NAME:	WDBRN HIGH
JOB NUMBER:	3246
DRAWN BY:	JOH
CHECKED BY:	NSW
DRAWING NO.:	3246PART

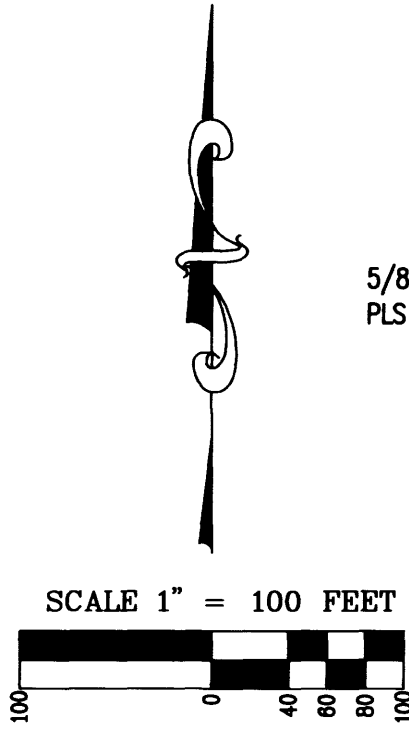
ENGINEERING • PLANNING • LANDSCAPE ARCHITECTURE
 FORESTRY • SURVEYING
AKS
 ENGINEERING & FORESTRY
 LICENSED IN OR & WA
 13910 SW GALBREATH DRIVE, SUITE 100
 SHERWOOD, OR 97140
 PHONE: (503) 925-8799
 FAX: (503) 925-8969
 OFFICES LOCATED IN SALEM, OR & VANCOUVER, WA

PARTITION PLAT NO. 2012-31

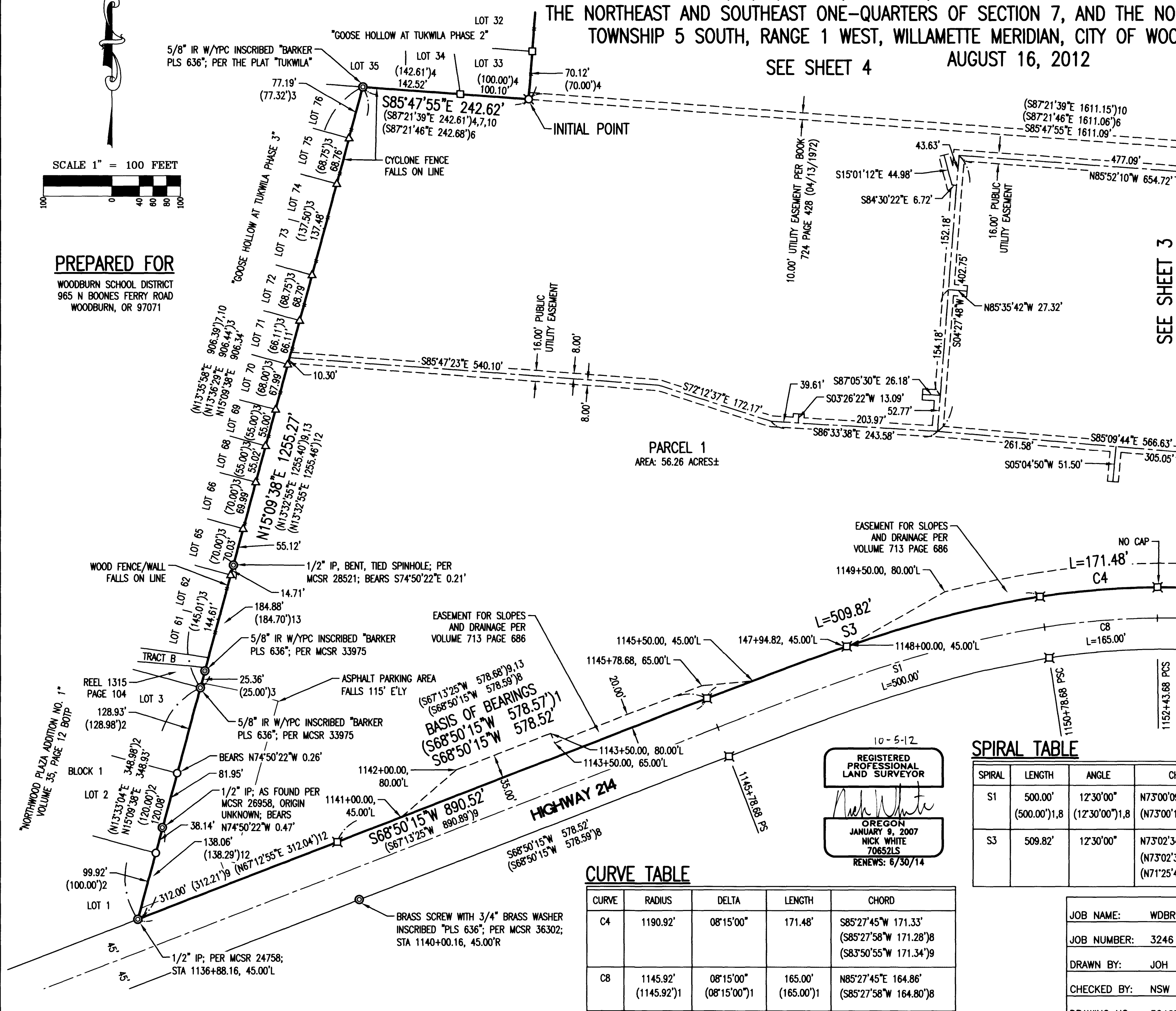
A REPLAT OF LOTS 2, 3, 4, AND 5, BLOCK 2, OF THE PLAT "GLATT SUBDIVISION", AND OTHER LANDS, LOCATED IN THE NORTHEAST AND SOUTHEAST ONE-QUARTERS OF SECTION 7, AND THE NORTHWEST ONE-QUARTER OF SECTION 8, TOWNSHIP 5 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, CITY OF WOODBURN, MARION COUNTY, OREGON

SEE SHEET 4

AUGUST 16, 2012



PREPARED FOR
WOODBURN SCHOOL DISTRICT
965 N BOONES FERRY ROAD
WOODBURN, OR 97071



PARCEL 1
AREA: 56.26 ACRES±

SEE SHEET 3

LEGEND

- SET 5/8" X 30" IRON ROD W/YPC INSCRIBED "AKS ENGR." ON: 08/16/12
- ▲ SET 1-1/8" BRASS BERNTSEN DISK INSCRIBED "AKS ENGR." ON: 08/16/12
- FOUND 5/8" IRON ROD; PER THE PLAT "NORTHWOOD PLAZA ADDITION NO. 1"; HELD
- △ FOUND 5/8" IRON ROD W/YPC INSCRIBED "MULTI/TECH INC."; PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 3"; HELD
- FOUND 5/8" IRON ROD W/YPC INSCRIBED "MULTI/TECH INC."; PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 2"; HELD
- ✕ FOUND 5/8" IRON ROD W/YPC INSCRIBED "MULTI/TECH INC."; PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 1"; HELD
- ⊠ FOUND 5/8" IRON ROD W/YPC INSCRIBED "DLT ASSOC. L.S. 1362"; PER THE PLAT "GLATT SUBDIVISION" UNLESS NOTED OTHERWISE; HELD
- ⊡ FOUND 5/8" IRON ROD W/YPC INSCRIBED "W&H PACIFIC"; PER MCSR 36702; HELD
- ⊙ DENOTES FOUND MONUMENT AS NOTED; HELD UNLESS NOTED OTHERWISE
- IP IRON PIPE
- IR IRON ROD
- W/YPC WITH A YELLOW PLASTIC CAP
- PP NO. PARTITION PLAT NUMBER PER MARION COUNTY SURVEY RECORDS
- MCSR SURVEY NUMBER PER MARION COUNTY SURVEY RECORDS
- BOTP BOOK OF TOWN PLATS
- ()1 RECORD INFORMATION PER MCSR 36702
- ()2 RECORD INFORMATION PER THE PLAT "NORTHWOOD PLAZA ADDITION NO. 1", VOLUME 35, PAGE 12 BOTP
- ()3 RECORD INFORMATION PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 3", VOLUME 45, PAGE 141 BOTP
- ()4 RECORD INFORMATION PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 2", VOLUME 45, PAGE 73 BOTP
- ()5 RECORD INFORMATION PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 1", VOLUME 45, PAGE 33 BOTP
- ()6 RECORD INFORMATION PER THE PLAT "GLATT SUBDIVISION", VOLUME 38, PAGE 30 BOTP
- ()7 RECORD INFORMATION PER PP NO. 97-43
- ()8 RECORD INFORMATION PER MCSR 36302
- ()9 RECORD INFORMATION PER MCSR 24758
- ()10 RECORD INFORMATION PER THE PLAT "TUKWILA", VOLUME 40, PAGE 95 BOTP
- ()11 RECORD INFORMATION PER MCSR 34166
- ()12 RECORD INFORMATION PER MCSR 26958
- ()13 RECORD INFORMATION PER MCSR 28521
- ()14 RECORD INFORMATION PER MCSR 31573

10-5-12
REGISTERED PROFESSIONAL LAND SURVEYOR

 OREGON
 JANUARY 9, 2007
 NICK WHITE
 70652LS
 RENEWS: 6/30/14

SPIRAL TABLE

SPIRAL	LENGTH	ANGLE	CHORD
S1	500.00' (500.00')1,8	12°30'00" (12°30'00")1,8	N73°00'09"E 498.94' (N73°00'15"E 498.87')8
S3	509.82'	12°30'00"	N73°02'34"E 508.73' (N73°02'39"E 508.67')8 (N71°25'40"E 508.73')9

CURVE TABLE

CURVE	RADIUS	DELTA	LENGTH	CHORD
C4	1190.92'	08°15'00"	171.48'	S85°27'45"W 171.33' (S85°27'58"W 171.28')8 (S83°50'55"W 171.34')9
C8	1145.92' (1145.92')1	08°15'00" (08°15'00')1	165.00' (165.00')1	N85°27'45"E 164.86' (S85°27'58"W 164.80')8

JOB NAME: WDBRN HIGH
 JOB NUMBER: 3246
 DRAWN BY: JOH
 CHECKED BY: NSW
 DRAWING NO.: 3246PART

ENGINEERING • PLANNING • LANDSCAPE ARCHITECTURE
 FORESTRY • SURVEYING

AKS
 ENGINEERING & FORESTRY

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 SHERWOOD, OR 97140
 PHONE: (503) 925-8799
 FAX: (503) 925-8969
 OFFICES LOCATED IN SALEM, OR & VANCOUVER, WA

2012 - 31

CURVE TABLE

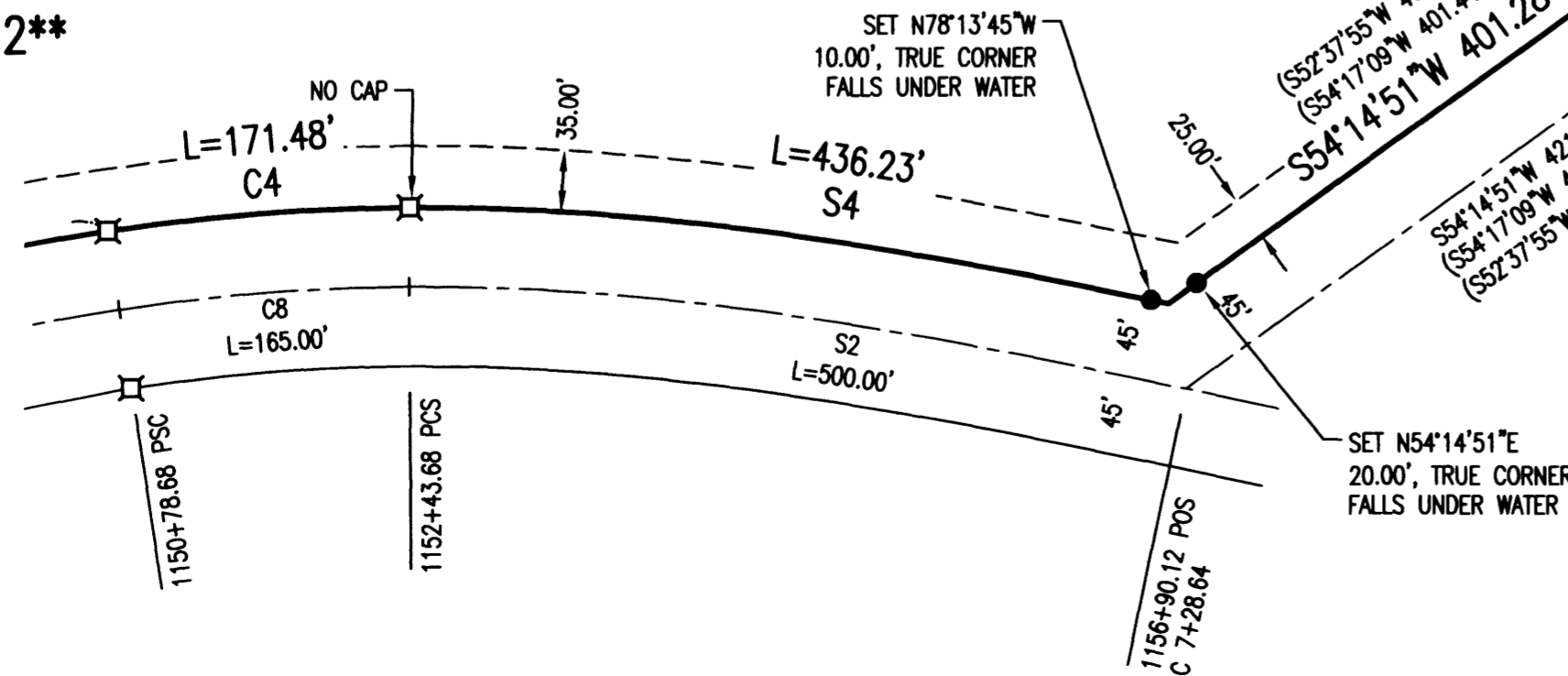
CURVE	RADIUS	DELTA	LENGTH	CHORD
C2	1296.05' (1296.05') ⁶	33°59'40"	768.97'	S68°47'23"E 757.74'
C3	331.48' (331.48') ⁸	58°29'48"	338.43'	S83°29'45"W 323.92'
C4	1190.92'	08°15'00"	171.48'	S85°27'45"W 171.33' (S85°27'58"W 171.28') ⁸ (S83°50'55"W 171.34') ⁹
C6	1266.05' (1266.05') ^{6,7}	33°59'40" (34°00'00") ^{6,7}	751.17' (751.29') ^{6,7}	S68°47'23"E 740.20' (S70°23'00"E 740.31') ⁶ (S70°21'33"E 740.31') ⁷
C7	286.48' (286.48') ¹	61°32'59" (61°27'01") ¹	307.75' (307.25') ¹	S85°01'20"W 293.16'
C8	1145.92' (1145.92') ¹	08°15'00" (08°15'00") ¹	165.00' (165.00') ¹	N85°27'45"E 164.86' (S85°27'58"W 164.80') ⁸
C12	1296.05'	28°46'46"	651.00'	S66°10'56"E 644.18'
C13	1296.05' (1296.05') ^{6,14}	05°12'54"	117.97'	S83°10'46"E 117.93' (S84°46'28"E 117.99') ^{6,14}
C16	331.48'	39°42'26"	229.72'	N87°06'35"W 225.15'
C17	331.48'	18°47'21"	108.70' (108.21') ⁹	S63°38'32"W 108.22'
C18	400.00' (400.00') ^{11,14}	38°14'57" (38°18'01") ¹⁴	267.03' (267.39') ¹⁴	N66°39'45"W 262.10' (N68°16'06"W 262.20') ¹¹ (N68°14'03"W 262.44') ¹⁴

SPIRAL TABLE

SPIRAL	LENGTH	ANGLE	CHORD
S2	500.00' (500.00') ^{1,8}	12°30'00" (12°30'00") ^{1,8}	S82°04'39"E 498.94'
S4	436.23'	12°13'14"	N82°48'46"W 435.37' (N82°48'48"W 435.32') ⁸ (N84°25'40"W 435.30') ⁹

LEGEND

****SEE SHEET 2****

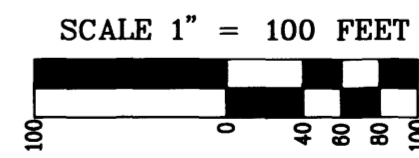
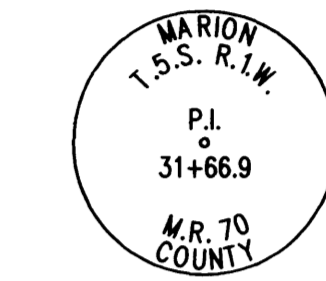
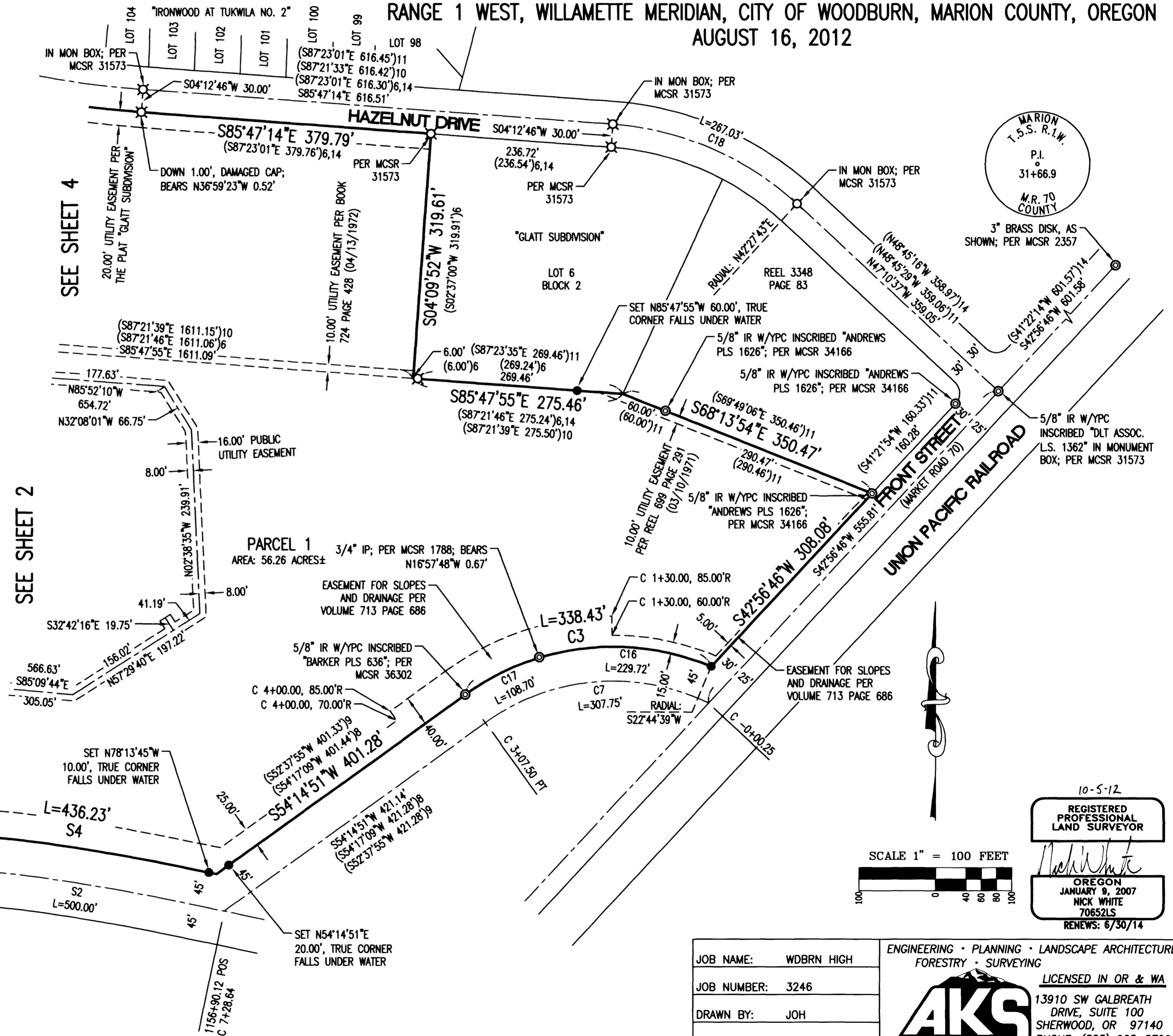


PREPARED FOR

WOODBURN SCHOOL DISTRICT
965 N BOONES FERRY ROAD
WOODBURN, OR 97071

PARTITION PLAT NO. 2012-31

A REPLAT OF LOTS 2, 3, 4, AND 5, BLOCK 2, OF THE PLAT "GLATT SUBDIVISION", AND OTHER LANDS, LOCATED IN THE NORTHEAST AND SOUTHEAST ONE-QUARTERS OF SECTION 7, AND THE NORTHWEST ONE-QUARTER OF SECTION 8, TOWNSHIP 5 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, CITY OF WOODBURN, MARION COUNTY, OREGON
AUGUST 16, 2012



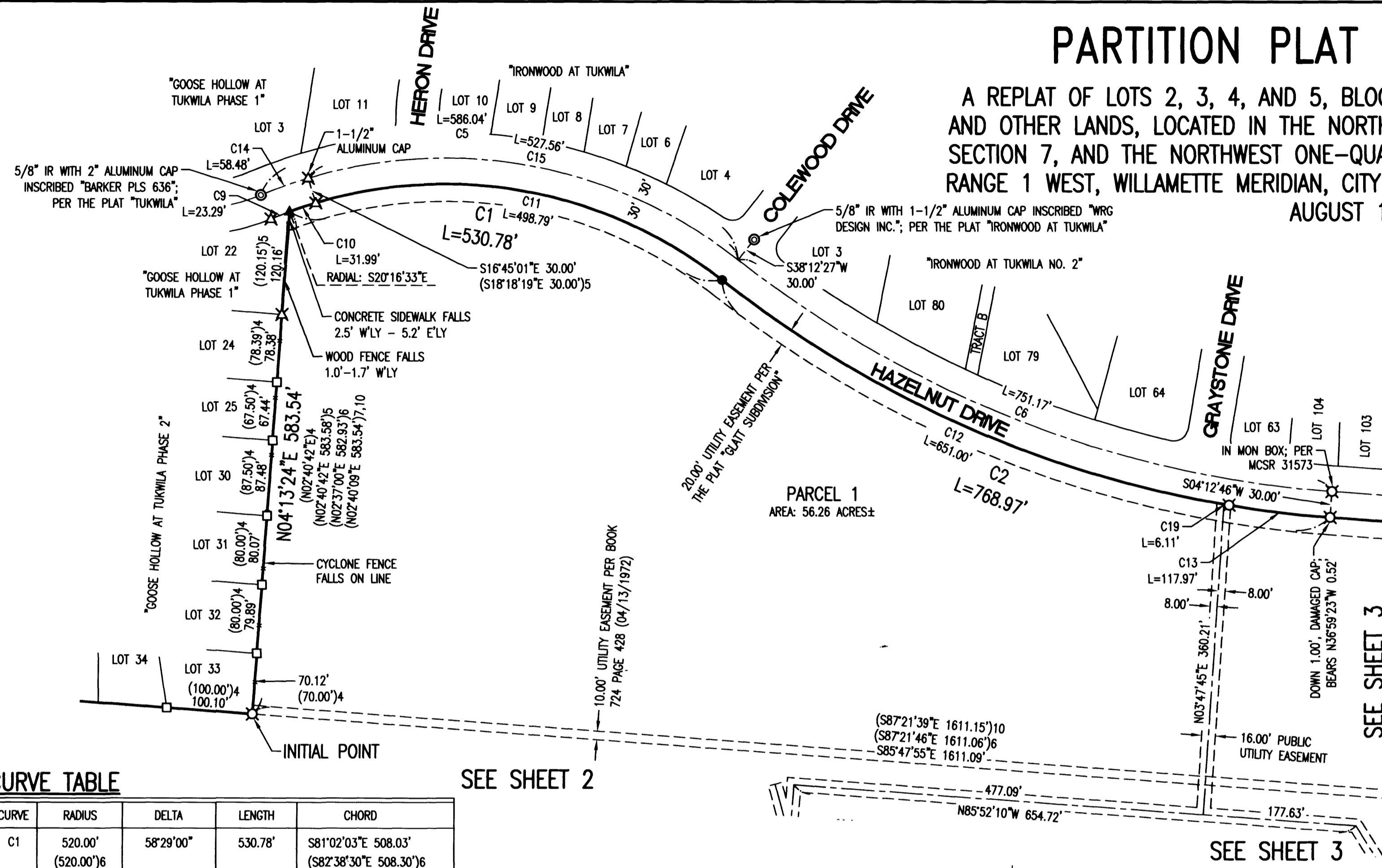
10-5-12
REGISTERED PROFESSIONAL LAND SURVEYOR
Nick White
OREGON
JANUARY 9, 2007
NICK WHITE
70652LS
RENEWS: 6/30/14

JOB NAME:	WDBRN HIGH	ENGINEERING • PLANNING • LANDSCAPE ARCHITECTURE
JOB NUMBER:	3246	FORESTRY • SURVEYING
DRAWN BY:	JOH	AKS ENGINEERING & FORESTRY
CHECKED BY:	NSW	LICENSED IN OR & WA
DRAWING NO.:	3246PART	13910 SW GALBREATH DRIVE, SUITE 100 SHERWOOD, OR 97140 PHONE: (503) 925-8799 FAX: (503) 925-8969
		OFFICES LOCATED IN SALEM, OR & VANCOUVER, WA

2012-31

PARTITION PLAT NO. 2012-31

A REPLAT OF LOTS 2, 3, 4, AND 5, BLOCK 2, OF THE PLAT "GLATT SUBDIVISION", AND OTHER LANDS, LOCATED IN THE NORTHEAST AND SOUTHEAST ONE-QUARTERS OF SECTION 7, AND THE NORTHWEST ONE-QUARTER OF SECTION 8, TOWNSHIP 5 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, CITY OF WOODBURN, MARION COUNTY, OREGON
AUGUST 16, 2012



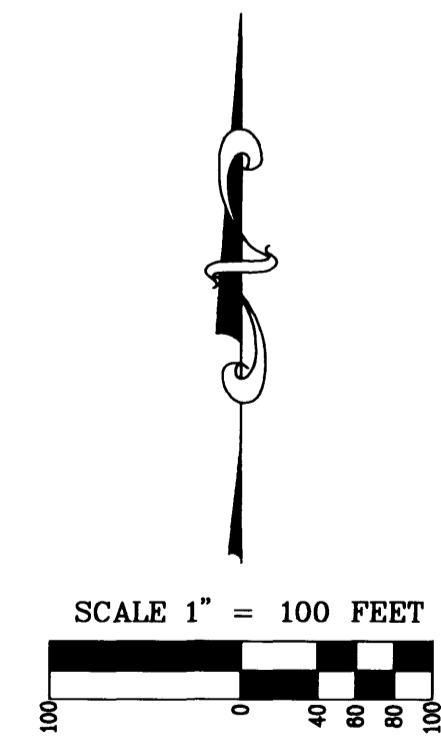
- LEGEND**
- SET 5/8" X 30" IRON ROD W/YPC INSCRIBED "AKS ENGR." ON: 08/16/12
 - ▲ SET 1-1/8" BRASS BERNTSEN DISK INSCRIBED "AKS ENGR." ON: 08/16/12
 - FOUND 5/8" IRON ROD; PER THE PLAT "NORTHWOOD PLAZA ADDITION NO. 1"; HELD
 - △ FOUND 5/8" IRON ROD W/YPC INSCRIBED "MULTI/TECH INC."; PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 3"; HELD
 - FOUND 5/8" IRON ROD W/YPC INSCRIBED "MULTI/TECH INC."; PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 2"; HELD
 - ✕ FOUND 5/8" IRON ROD W/YPC INSCRIBED "MULTI/TECH INC."; PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 1"; HELD
 - ⊠ FOUND 5/8" IRON ROD W/YPC INSCRIBED "DLT ASSOC. L.S. 1362"; PER THE PLAT "GLATT SUBDIVISION" UNLESS NOTED OTHERWISE; HELD
 - ⊡ FOUND 5/8" IRON ROD W/YPC INSCRIBED "W&H PACIFIC"; PER MCSR 36702; HELD
 - ◎ DENOTES FOUND MONUMENT AS NOTED; HELD UNLESS NOTED OTHERWISE
 - IP IRON PIPE
 - IR IRON ROD
 - W/YPC WITH A YELLOW PLASTIC CAP
 - PP NO. PARTITION PLAT NUMBER PER MARION COUNTY SURVEY RECORDS
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 - ()4 RECORD INFORMATION PER THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 2", VOLUME 45, PAGE 73 BOTP
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 - ()12 RECORD INFORMATION PER MCSR 26958
 - ()13 RECORD INFORMATION PER MCSR 28521
 - ()14 RECORD INFORMATION PER MCSR 31573

CURVE TABLE

CURVE	RADIUS	DELTA	LENGTH	CHORD
C1	520.00' (520.00') ⁶	58°29'00"	530.78'	S81°02'03"E 508.03' (S82°38'30"E 508.30') ⁶
C2	1296.05' (1296.05') ⁶	33°59'40"	768.97'	S68°47'23"E 757.74'
C5	550.00' (550.00') ^{7,10}	61°03'00" (61°01'36") ^{7,10}	586.04' (585.81') ^{7,10}	N82°19'03"W 558.71' (N83°52'22"W 558.51') ^{7,10}
C6	1266.05' (1266.05') ^{6,7}	33°59'40" (34°00'00") ^{6,7}	751.17' (751.29') ^{6,7}	S68°47'23"E 740.20' (S70°23'00"E 740.31') ⁶ (S70°21'33"E 740.31') ⁷
C9	520.00'	02°34'00"	23.29'	S68°26'27"W 23.29'
C10	520.00' (520.00') ⁵	03°31'29" (03°31'23") ⁵	31.99' (31.97') ⁵	S71°29'11"W 31.98' (S69°57'55"W 31.97') ⁵
C11	520.00'	54°57'31"	498.79'	N79°16'19"W 479.89'
C12	1296.05'	28°46'46"	651.00'	S66°10'56"E 644.18'
C13	1296.05' (1296.05') ^{6,14}	05°12'54"	117.97'	S83°10'46"E 117.93' (S84°46'28"E 117.99') ^{6,14}
C14	550.00' (550.00') ⁵	06°05'32" (06°05'26") ⁵	58.48' (58.47') ⁵	S70°12'13"W 58.45' (S68°40'47"W 58.44') ⁵
C15	550.00'	54°57'28"	527.56'	N79°16'17"W 507.56'
C19	1296.05'	00°16'12"	6.11'	S80°26'13"E 6.11'

SEE SHEET 2

SEE SHEET 3



PREPARED FOR
WOODBURN SCHOOL DISTRICT
965 N BOONES FERRY ROAD
WOODBURN, OR 97071

10-5-12
REGISTERED PROFESSIONAL LAND SURVEYOR
Nick White
OREGON
JANUARY 9, 2007
NICK WHITE
70652LS
RENEWS: 6/30/14

JOB NAME:	WDBRN HIGH
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PARTITION PLAT NO. 2012-31

A REPLAT OF LOTS 2, 3, 4, AND 5, BLOCK 2, OF THE PLAT "GLATT SUBDIVISION", AND OTHER LANDS, LOCATED IN THE NORTHEAST AND SOUTHEAST ONE-QUARTERS OF SECTION 7, AND THE NORTHWEST ONE-QUARTER OF SECTION 8, TOWNSHIP 5 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, CITY OF WOODBURN, MARION COUNTY, OREGON
AUGUST 16, 2012

PREPARED FOR

WOODBURN SCHOOL DISTRICT
965 N BOONES FERRY ROAD
WOODBURN, OR 97071

CITY OF WOODBURN APPROVALS

[Signature] 10/11/12
CITY OF WOODBURN DATE
DIRECTOR OF ECONOMIC AND DEVELOPMENT SERVICES
PLANNING CASE NO: PLA 2012-02
[Signature] 11/2/12
CITY OF WOODBURN, ENGINEER DATE

MARION COUNTY APPROVALS

Mark E. Riggins, by
[Signature] 10/19/2012
MARION COUNTY SURVEYOR DATE
W.A. Shawn Beaton
by: [Signature] 10-23-12
MARION COUNTY ASSESSOR DATE

TAXES HAVE BEEN PAID TO THIS DATE 06/30/13
W.A. Shawn Beaton by: DATE
[Signature] 0/23/12
MARION COUNTY TAX COLLECTOR DATE

STATE OF OREGON }
COUNTY OF MARION } ss

I DO HEREBY CERTIFY THAT THIS PARTITION PLAT NO. 2012-31
WAS RECEIVED FOR RECORD ON THIS 23rd DAY OF
October, 2012,
AT 12:17 O'CLOCK P M
AND RECORDED IN THE BOOK OF PARTITION PLATS. IT IS
RECORDED IN MARION COUNTY DEED RECORDS IN REEL
3436 AT PAGE 481.

BILL BURGESS, MARION COUNTY CLERK
[Signature]
DEPUTY COUNTY CLERK

DECLARATION

KNOW ALL PERSONS BY THESE PRESENTS THAT WOODBURN SCHOOL DISTRICT 103 AND WOODBURN SCHOOL DISTRICT 103C, MARION COUNTY OREGON, BEING THE OWNERS OF THE LAND DESCRIBED IN THE SURVEYOR'S CERTIFICATE SHOWN HEREON AND DESIRING TO DISPOSE OF THE SAME INTO A PARCEL HAS CAUSED THE SAME TO BE SURVEYED AND PARTITIONED AS SHOWN ON THE ATTACHED MAP ALL IN ACCORDANCE WITH ORS 92. WE HEREBY GRANT THE EASEMENTS AS SHOWN.

[Signature]
DAVID BAUTISTA - SUPERINTENDENT/CHIEF
ADMINISTRATIVE OFFICER AS DISTRICT SCHOOL CLERK,
WOODBURN SCHOOL DISTRICT 103 AND WOODBURN
SCHOOL DISTRICT 103C, MARION COUNTY OREGON

ACKNOWLEDGMENT

STATE OF OREGON }
COUNTY OF Marion } ss

THIS INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON 10-8-2012,
2012 BY David Bautista OF WOODBURN SCHOOL DISTRICT 103 AND
WOODBURN SCHOOL DISTRICT 103C, MARION COUNTY, OREGON.

[Signature]
NOTARY SIGNATURE
[Signature]
NOTARY PUBLIC-OREGON

COMMISSION NO. 452765
MY COMMISSION EXPIRES 10-12-2014

SURVEYOR'S CERTIFICATE

I, NICK WHITE, DO HEREBY CERTIFY THAT I HAVE CORRECTLY SURVEYED AND MARKED WITH PROPER MONUMENTS THE LAND REPRESENTED ON THE MAP SHOWN, A REPLAT OF LOTS 2, 3, 4, AND 5, BLOCK 2, OF THE PLAT "GLATT SUBDIVISION", AND OTHER LANDS, LOCATED IN THE NORTHEAST AND SOUTHEAST ONE-QUARTERS OF SECTION 7, AND THE NORTHWEST ONE-QUARTER OF SECTION 8, TOWNSHIP 5 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, CITY OF WOODBURN, MARION COUNTY, OREGON BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE INITIAL POINT, BEING A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "DLT ASSOC. L.S. 1362" AT THE SOUTHEAST CORNER OF LOT 33 OF THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 2"; THENCE ALONG THE EAST LINES OF SAID PLAT AND "GOOSE HOLLOW AT TUKWILA PHASE 1" NORTH 04°13'24" EAST 583.54 FEET TO A POINT ON THE SOUTH RIGHT-OF-WAY LINE OF HAZELNUT DRIVE (30.00 FEET FROM CENTERLINE); THENCE ALONG SAID RIGHT-OF-WAY LINE ALONG A NON-TANGENT CURVE TO THE RIGHT (RADIAL: SOUTH 20°16'33" EAST) WITH A RADIUS OF 520.00 FEET, A DELTA OF 58°29'00", A LENGTH 530.78 FEET, AND A CHORD OF SOUTH 81°02'03" EAST 508.03 FEET TO A POINT; THENCE CONTINUING ALONG SAID RIGHT-OF-WAY LINE ALONG A REVERSE CURVE TO THE LEFT WITH A RADIUS OF 1296.05 FEET, A DELTA OF 33°59'40", A LENGTH OF 768.97, AND A CHORD OF SOUTH 68°47'23" EAST 757.74 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "DLT ASSOC. L.S. 1362"; THENCE CONTINUING ALONG SAID RIGHT-OF-WAY LINE SOUTH 85°47'14" EAST 379.79 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "DLT ASSOC. L.S. 1362"; THENCE LEAVING SAID RIGHT-OF-WAY LINE ALONG THE WEST LINE OF LOT 6, BLOCK 2 OF THE PLAT "GLATT SUBDIVISION" SOUTH 04°09'52" WEST 319.61 FEET TO A POINT FROM WHICH A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "DLT ASSOC. L.S. 1362" BEARS SOUTH 85°47'55" EAST 6.00 FEET; THENCE ALONG THE SOUTH LINE OF SAID LOT 6 SOUTH 85°47'55" EAST 275.46 FEET TO THE SOUTHEAST CORNER OF THE ANDREW J. VAUGHN D.L.C. NO. 52; THENCE ALONG THE SOUTHWEST LINE OF REEL 3348 PAGE 83 SOUTH 68°13'54" EAST 350.47 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "ANDREWS PLS 1626"; THENCE ALONG THE NORTHWEST RIGHT-OF-WAY OF FRONT STREET (30.00 FEET FROM CENTERLINE) SOUTH 42°56'46" WEST 308.08 FEET TO A POINT; THENCE ALONG A NON-TANGENT CURVE TO THE LEFT (RADIAL: SOUTH 22°44'39" WEST) WITH A RADIUS OF 331.48 FEET, A DELTA OF 58°29'48", A LENGTH OF 338.43 FEET, AND A CHORD OF SOUTH 83°29'45" WEST 323.92 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "BARKER PLS 636"; THENCE SOUTH 54°14'51" WEST 401.28 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY OF HIGHWAY 214 (45.00 FEET FROM CENTERLINE); THENCE ALONG SAID NORTH RIGHT-OF-WAY ALONG A SPIRAL TO THE LEFT WITH A LENGTH OF 436.23 FEET, AN ANGLE OF 12°13'14", AND A CHORD OF NORTH 82°48'46" WEST 435.37 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "W&H PACIFIC"; THENCE CONTINUING ALONG SAID NORTH RIGHT-OF-WAY ALONG A CURVE TO THE LEFT WITH A RADIUS OF 1190.92 FEET, A DELTA OF 08°15'00", A LENGTH OF 171.48 FEET, AND A CHORD OF SOUTH 85°27'45" WEST 171.33 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "W&H PACIFIC"; THENCE CONTINUING ALONG SAID NORTH RIGHT-OF-WAY ALONG A SPIRAL TO THE LEFT WITH A LENGTH OF 509.82 FEET, AN ANGLE OF 12°30'00", AND A CHORD OF SOUTH 73°02'34" WEST 508.73 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "W&H PACIFIC"; THENCE CONTINUING ALONG SAID NORTH RIGHT-OF-WAY SOUTH 68°50'15" WEST 890.52 FEET TO A 1/2 INCH IRON PIPE; THENCE ALONG THE EAST LINES OF THE PLATS "NORTHWOOD PLAZA ADDITION NO. 1" AND "GOOSE HOLLOW AT TUKWILA PHASE 3" NORTH 15°09'38" EAST 1255.27 FEET TO A 5/8 INCH IRON ROD WITH A YELLOW PLASTIC CAP INSCRIBED "BARKER PLS 636"; THENCE ALONG THE SOUTH LINE OF THE PLAT "GOOSE HOLLOW AT TUKWILA PHASE 2" SOUTH 85°47'55" EAST 242.62 FEET TO THE INITIAL POINT.

THE ABOVE DESCRIBED TRACT CONTAINS 56.26 ACRES, MORE OR LESS.

10-5-12
REGISTERED PROFESSIONAL LAND SURVEYOR
[Signature]
OREGON
JANUARY 9, 2007
NICK WHITE
70652LS
RENEWS: 6/30/14

JOB NAME:	WDBRN HIGH
JOB NUMBER:	3246
DRAWN BY:	JOH
CHECKED BY:	NSW
DRAWING NO.:	3246PART

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Analyses & Findings

This attachment to the staff report analyzes the application materials and finds through statements how the application materials relate to and meet applicable provisions such as criteria, requirements, and standards. They confirm that a given standard is met or if not met, they call attention to it, suggest a remedy, and have a corresponding recommended condition of approval. Symbols aid locating and understanding categories of findings:

<i>Symbol</i>	<i>Category</i>	<i>Indication</i>
✓	Requirement (or guideline) met	No action needed
✗	Requirement (or guideline) not met	Correction needed
⊖	Requirement (or guideline) not applicable	No action needed
▲	<ul style="list-style-type: none"> Requirement (or guideline) met, but might become unmet because of condition applied to meet separate and related requirement that is not met Plan sheets and/or narrative inconsistent Other special circumstance benefitting from attention 	Revision needed for clear and consistent records
■	Street Exception or Variance	Request to vary from requirement

Table of Contents

Location.....	2
Land Use & Zoning	2
Statutory Dates	3
Design Review Provisions.....	4
Conditional Use Provisions	30
Street Exception Provisions	32
Phasing Plan Provisions.....	33
Recommended Conditions of Approval.....	35
Applicant Identity.....	43
Notes to the Applicant	43

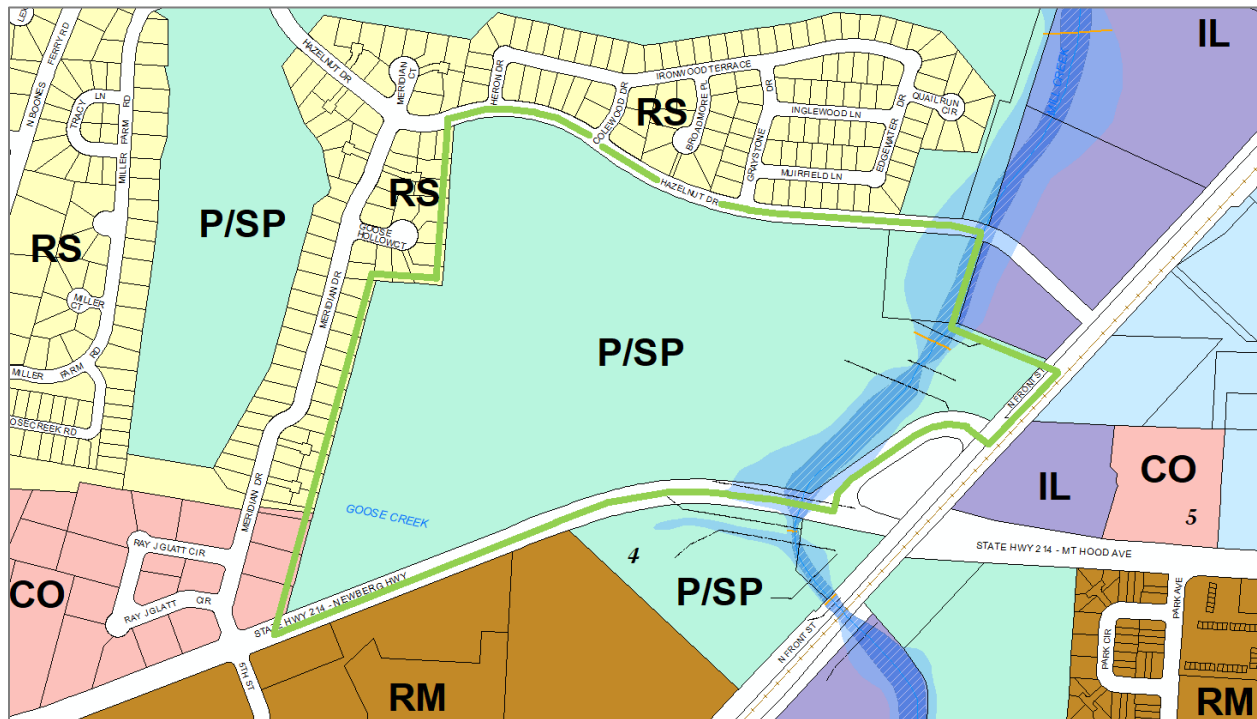
Location

Address	1785 N. Front Street (Woodburn H.S.)
Tax Lot	051W07A 00600 (56.26 acres)
Nearest intersection	N. Front Street & Oregon Highway 214 / Newberg Highway

Land Use & Zoning

Comprehensive Plan Land Use Designation	Public Use
Zoning District	Public and Semi-Public (P/SP)
Overlay District(s)	Riparian Corridor and Wetlands Overlay District (RCWOD; along Mill Creek)
Existing Use(s)	Woodburn School District Woodburn High School (H.S.) campus

For context, the subject property and adjacent zoning are illustrated and tabulated below:



Zoning Map Excerpt

Cardinal Direction	Adjacent Zoning
North	Residential Single Family (RS) / Ironwood at Tukwila Subdivision
East	Northerly: Light Industrial (IL) and – across N. Front Street and railroad rights-of-way (ROWS) – Industrial Park (IP); Southerly: Across N. Front Street and railroad rights-of-way (ROWS) – IL / Mt. Hood Self Storage
South	Easterly: Public and Semi-Public (P/SP) / St. Luke’s Cemetery;

	Westerly: Medium Density Residential (RM) / multiple-dwelling developments
West	Southerly: Commercial Office (CO) / Northwood Plaza; Northerly: RS / Goose Hollow at Tukwila Subdivision

The subject property is composed of two lots. The larger one that contains the vast majority of the campus is Parcel 1 of a partition, Marion County partition plat record P.P. 2012-031, and 56.26 acres. The Marion County Assessor and notes that the high school building dates from 1977. The second, smaller lot is undeveloped, contains a segment of Mill Creek, and is at the upper northeast corner of the site, Glatt Subdivision Lot 6 of Block 2.

The City had adopted its first land division requirements earlier, effective April 16, 1963 as referenced in Woodburn Development Ordinance (WDO) 1.02 "Lot". The subject property is a legal lot of record.

Section references below are to the [Woodburn Development Ordinance \(WDO\)](#).

Statutory Dates

The application was submitted on December 13, 2019 with additional materials submitted through February 4, 2020. Staff deemed it complete as of February 4, 2020, making the Oregon Revised Statutes (ORS) [227.178\(1\)](#) 120-day decision deadline June 3, 2020.

Design Review Provisions

4.01.07 Consolidated Applications

An applicant may request, in writing, to consolidate applications needed for a single development project. Under a consolidated review, all applications shall be processed following the procedures applicable for the highest type decision requested. It is the express policy of the City that development review not be segmented into discrete parts in a manner that precludes a comprehensive review of the entire development and its cumulative impacts.

5.03.02 Design Review, Type III

A. Purpose: The purpose of Type III design review is to ensure that new buildings or additions to existing buildings comply with Land Use and Development Guidelines and Standards of this Ordinance (Sections 2 and 3).

B. Type III Design Review is required for the following:

1. Non-residential structures in residential zones greater than 1,000 square feet in the RS, R1S, RM, and P/SP zones.
2. Multi-family dwellings not meeting all architectural design guidelines and standards.
3. Structures greater than 2,000 square feet in the CO, CG, MUV, DDC, and NNC zones.
4. Structures greater than 3,000 square feet in the IP, IL, and SWIR zones.
5. For sites with existing buildings in the CO, CG, MUV, DDC, NNC, IP, IL, and SWIR zones; expansions or new buildings that increase lot coverage by more 25%.
6. Change of use that results in a greater than 25% increase in required parking.

Because the consolidated applications package includes the master/parent application type of conditional use, which is Type III, the entire package including the Design Review is also Type III.

The applicant submitted site plans on December 13, 2019 and revised site plans through February 4, 2020 (within Attachment 108). (Staff hosted pre-app PRE 2019-22 on October 23, 2019.)

✓ The requirement is met.

1.04.02 Change or Expansion of an Existing Use with Nonconforming Parking, Loading and/or Landscaping

Any additional parking, loading, landscaping, wall or refuse facility required by the WDO to accommodate a change in use, or expansion of an existing use shall be subject to the following:

A. For applications where the change or expansion increases the required area for parking, loading, or landscaping by 25 percent or more, all parking, loading, landscaping, buffer walls and refuse facilities shall conform to the standards of the WDO.

B. For applications where the change or expansion increases the required area for parking, loading, or landscaping by less than 25 percent, the parking, loading, landscaping, buffer walls and refuse facilities required for the expansion shall conform to the standards of the WDO. The property owner is encouraged, but not required, to bring more of the site into conformity.

The applicant’s narrative (p. 9) states:

“Phase 1: The Phase 1 scope of work will occupy a total area of 113,256 SF (2.6 acres). The scope of work is only 4.6% of the total 2,450,686 SF (56.26 acres) that make up Woodburn High School property.

With the 'area of work' being less than 25% of the total property, the Phase 1 is subject to WDO 1.04.02.B. All improvements within the area of work to follow the WDO development standards.

Phase 2: The scope of work will occupy a total area of 191,664 SF (4.4 acres). This area is only 7.8% of the total 2,450,686 SF (56.26 acres) that make up Woodburn High School property.

With the 'area of work' being less than 25% of the total property, Phase 2 is subject to WDO 1.04.02.B. All improvements within the area of work to follow the WDO development standards.”

Staff accepts this, adding that as examined below for 3.04.05, the applicant’s traffic letter states that the proposal does not increase student capacity. Also, because of the opportunity of the conditional use, applies a condition to place more tree islands within the parking areas and stripe walkway crossings to improve pedestrian safety and comfort.

2.04 Industrial and Public Zones

A. The City of Woodburn is divided into the following industrial and public zones:

3. The Public and Semi-Public (P/SP) zone, which is intended for public uses, parks, schools and cemeteries.

B. Approval Types (Table 2.04A)

1. Accessory Uses (A) are allowed outright, subject to the general standards of this Ordinance.

2. Conditional Uses (CU) may be allowed, subject to the general development standards of this Ordinance and conditions of Conditional Use approval.

3. Permitted Uses (P) are allowed outright, subject to the general development standards of this Ordinance.

Uses Allowed in Industrial Zones Table 2.04A	
Use	Zone
Accessory Uses (A) Conditional Uses (CU) Permitted Uses (P) Special Permitted Uses (S) Specific Conditional Uses (SCU)	P/SP
A	Civic Uses

3	Public administration, aquatic facilities, fire protection, government and public utility buildings and storage yards	CU
5	Trade schools	CU

The proposed use has no exact match and mostly closely matches A.3. Based on this and that A.5 trade schools are a conditional use, the Director interprets as he had for CUs 2019-02, 5, & 6 that public K-12 schools are conditional use in the P/SP zoning district.

✓ The requirement is met.

Public/Semi-Public (P/SP) - Site Development Standards Table 2.04D			
Lot Area, Minimum		No minimum	
Lot Width, Minimum		No minimum	
Lot Depth, Minimum		No minimum	
Street Frontage, Minimum		No minimum	
Front Setback and Setback Abutting a Street, Minimum (feet)		20 ¹	
Side or Rear Setback, Minimum (feet)	Abutting P/SP zone or a residential zone or use	20	
	Abutting a commercial or industrial zone	0 or 5 ²	
Setback to a Private Access Easement, Minimum (feet)		5	
Lot Coverage, Maximum		Not specified ³	
Building Height, Maximum (feet)	Primary or accessory structure	Outside Gateway subarea	35
		Gateway subarea	50
	Features not used for habitation		No minimum
<p>1. Measured from the Special Setback (Section 3.03.02), if any.</p> <p>2. A building may be constructed at the property line, or shall be set back at least five feet.</p> <p>3. Lot coverage is limited by setbacks, off-street parking, and landscaping requirements.</p>			

The P/SP zone has no minimum lot size, width, depth, or street frontage or maximum lot coverage.

The proposal exceeds minimum setbacks. Because the application materials indicate no private access easement, the 5-foot setback is not applicable.

Sheets CU-A07, 8, & 9 illustrate and note that the additions are no higher than the maximums permissible by zoning.

✓ The site development provisions are met.

2.05 Overlay Districts

One applies – the Riparian Corridor and Wetlands Overlay District (RCWOD).

2.05.05 Riparian Corridor and Wetlands Overlay District ...

B. Boundaries of the RCWOD

1. The RCWOD includes:

- a. Riparian corridors extending upland 50 feet from the top of the bank of the main stem of Senecal Creek and Mill Creek and those reaches of their tributaries identified as fish-bearing perennial streams on the Woodburn Wetlands Inventory Map; and
- b. Significant wetlands identified on the Woodburn Wetlands Inventory Map. Where significant wetlands are located fully or partially within a riparian corridor, the RCWOD shall extend 50 feet from the edge of the wetland; and
- c. The 100-year floodplain on properties identified as vacant or partly vacant on the 2005 Woodburn Buildable Lands Inventory.

C. Permitted Uses and activities

The following uses and activities are allowed, provided they are designed and constructed to minimize intrusion into the RCWOD:

1. Erosion or flood control measures that have been approved by the Oregon Department of State Lands, the U.S. Army Corps of engineers, or another state or federal regulatory agency
2. Maintenance of existing structures, lawns and gardens
3. Passive recreation uses and activities
4. Removal of non-native plant species and replacement with native plantspecies
5. Streets, roads, and paths that are included in an element of the Comprehensive Plan
6. Utilities
7. Water-related and water-dependent uses, including drainage facilities, water and sewer facilities, flood control projects, drainage pumps, public paths, access ways, trails, picnic areas or interpretive and educational displays and overlooks, including benches and outdoor furniture

✓ Because the site plans illustrate no development or redevelopment within the Riparian Corridor and Wetlands Overlay District, the RCWOD provisions are met.

2.06 Accessory Structures

2.06.02 Fences and Walls

C. Height in Non-Residential Zones

1. In commercial, industrial, or public zones, the maximum height of a fence or wall located in a yard abutting a street shall be 6 feet, relative to the ground elevation under the fence or

wall. Fence height may increase to 9 feet once flush with the building face, or 20 feet from street right-of-way.

2. Fences and walls may be constructed in the special setback provided the property owner agrees to removal at such time as street improvements are made.

D. Fence Materials

1. Fences and walls shall be constructed of any materials commonly used in the construction of fences and walls, such as wood, stone, rock, or brick, or other durable materials.
2. Chain link fences are acceptable as long as the fence is coated and includes slats made of vinyl, wood or other durable material. Slats may not be required when visibility into features such as open space, natural areas, parks and similar areas is needed to assure visual security, or into on-site areas in industrial zones that require visual surveillance.
3. For manufacturing, assembly, fabricating, processing, packing, storage and wholesale and distribution activities which are the principle use of a building in industrial districts, the preceding standards apply when visible from, and within 20 feet of, a public street.

2.06.03 Structures

- A. Accessory structures attached to a primary building shall be considered as a portion of the primary building and subject to the same requirements as the primary building.
- B. The minimum separation between detached accessory structures and the primary building shall be six feet.

Fence/Fencing

The site plans propose additional fencing, and it appears it would conform to 2.06.02; however, because 5.01.03 requires a fence permit, staff applies a condition to confirm conformance through such permit, which is separate from and may come after land use review.

▲ In order to secure conformance with fence regulations, staff applies *Condition D1a*.

Signage/Signs

The applicant proposes no additional or altered permanent signage, and unlike as for CUs 2019-05 & 6, there appear to be no nonconforming readerboard pole signs to remove from existing or widened rights-of-way (ROWS).

⊖ None apply.

2.07 Special Uses

⊖ None apply.

3.01 Streets

3.01.02 General Provisions

A. No development shall be approved, or access permit issued, unless the internal streets, boundary streets and connecting streets are constructed to at least the minimum standards set forth in this Section, or are required to be so constructed as a condition of approval.

D. The standards of this Section may be modified, subject to approval of an Exception to Street Right-of-Way and Improvement Requirements.

3.01.04B. All public streets under the jurisdiction of the City of Woodburn shall comply with the cross-sections depicted in this Section.

3.01.04C. For local residential streets which are not identified in the Comprehensive Plan, rights-of-way and improvements are determined by the Director at the time of development, based upon the existing and future estimated average daily trips of the development and surrounding development.

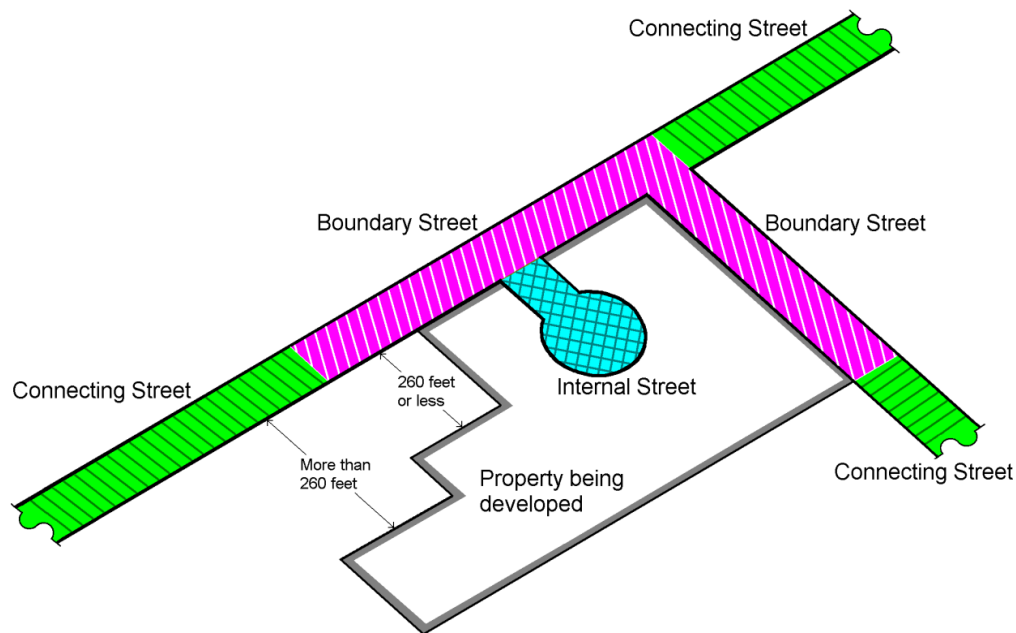


Figure 3.01A – Internal, Boundary, and Connecting Streets

The subject property has three frontages: Hazelnut Drive, N. Front Street, and Oregon Highway 214 / Newberg Highway, a state highway.

Per Transportation System Plan (TSP) Figure 2 “Functional Roadway Classification” (Attachment 104; 2019), the three are as follows:

1. Hazelnut Drive is Access Street class, for which WDO Figure 3.01E applies;
2. N. Front Street is Minor Arterial class, for which WDO Figure 3.01C applies; and
3. Oregon Highway 214 / Newberg Highway is Major Arterial class, for which WDO Figure 3.01B applies.

Figures are below.

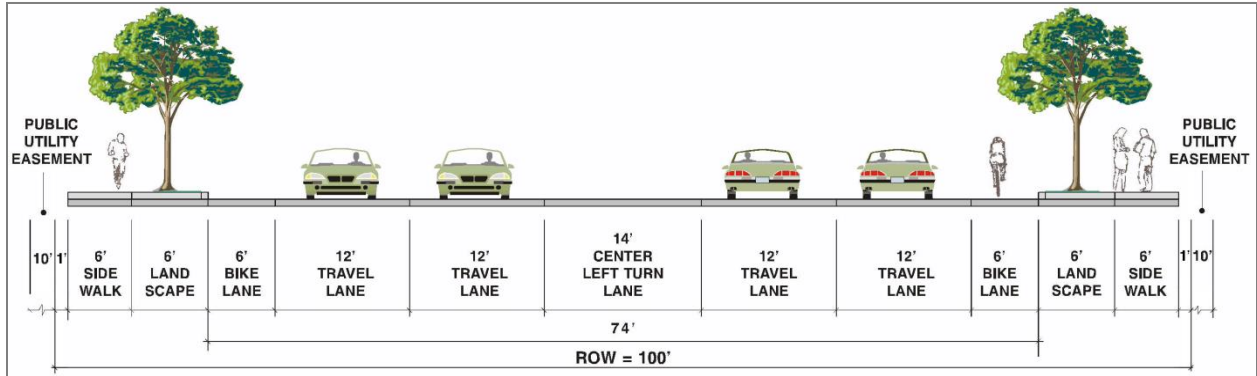


Figure 3.01B – Major Arterial

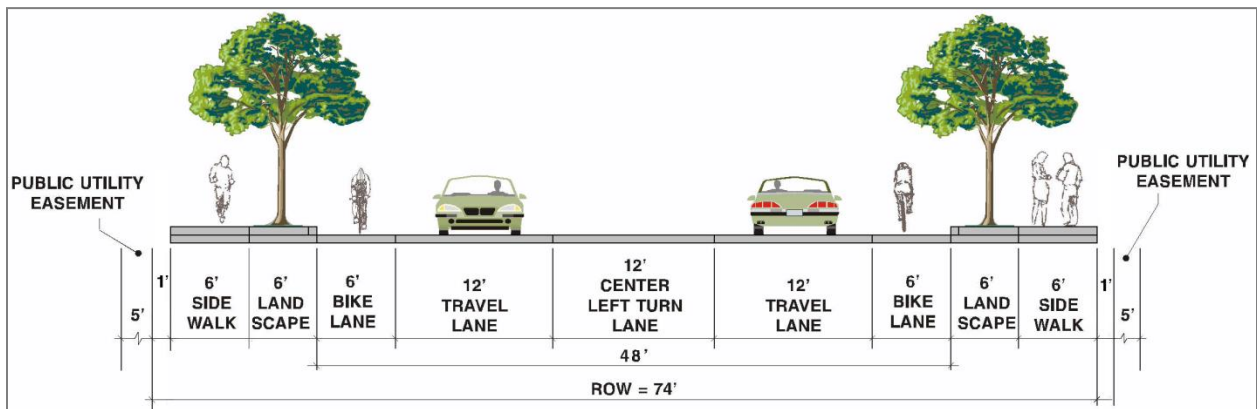


Figure 3.01C – Minor Arterial

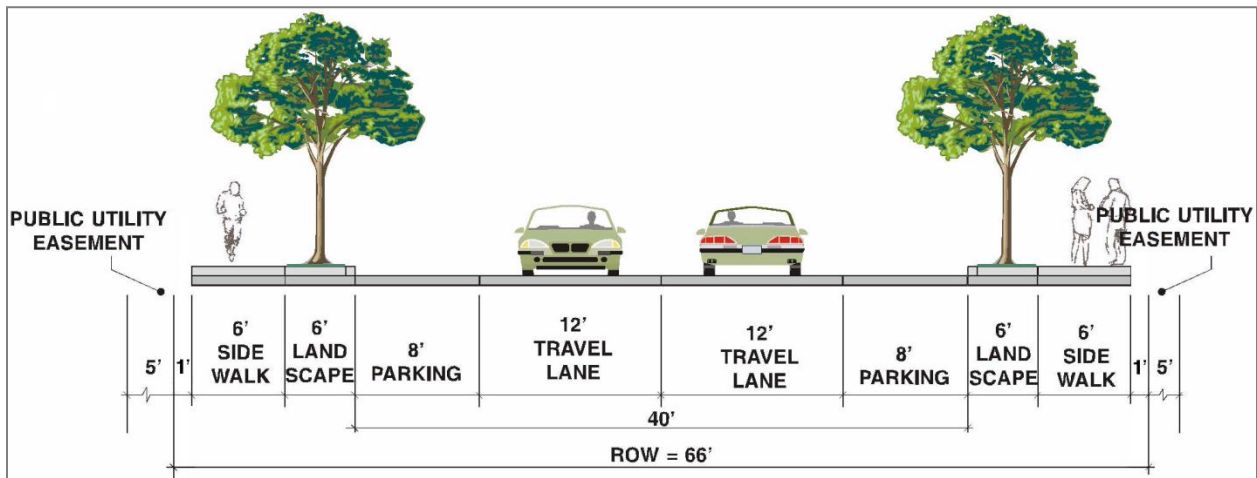


Figure 3.01E – Access Street / Commercial Street

(Note: On September 23, 2019, the City Council adopted the 2019 major update via Legislative Amendment LA 2018-01 and Ordinance No. 2575, which per the City Charter took effect 30 days later on October 23, 2019. On that date, application submittals including the subject one became subject to this new TSP that looks through 2039.)

Frontage/public/street improvements are required to upgrade the frontages to present standards; however, the applicant applied for an Exception to Street Right of Way and Improvement Requirements (EXCP; “Street Exception”), explicitly for N. Front Street.

The proposal is conditioned to confirm for all three streets required dedications of ROWs and streetside public utility easements (PUEs).

The site plans identify required what appear to be all street improvements as required by the WDO and defer them to Phase 2. However, questions about implementation and construction details may later arise that might require an increase in scope by the applicant to noted improvements in order for the applicant to obtain right-of-way or other permit types from the Public Works Department in order to have the City Engineer and associated staff review and approve civil engineering plans prior to construction of street improvements.

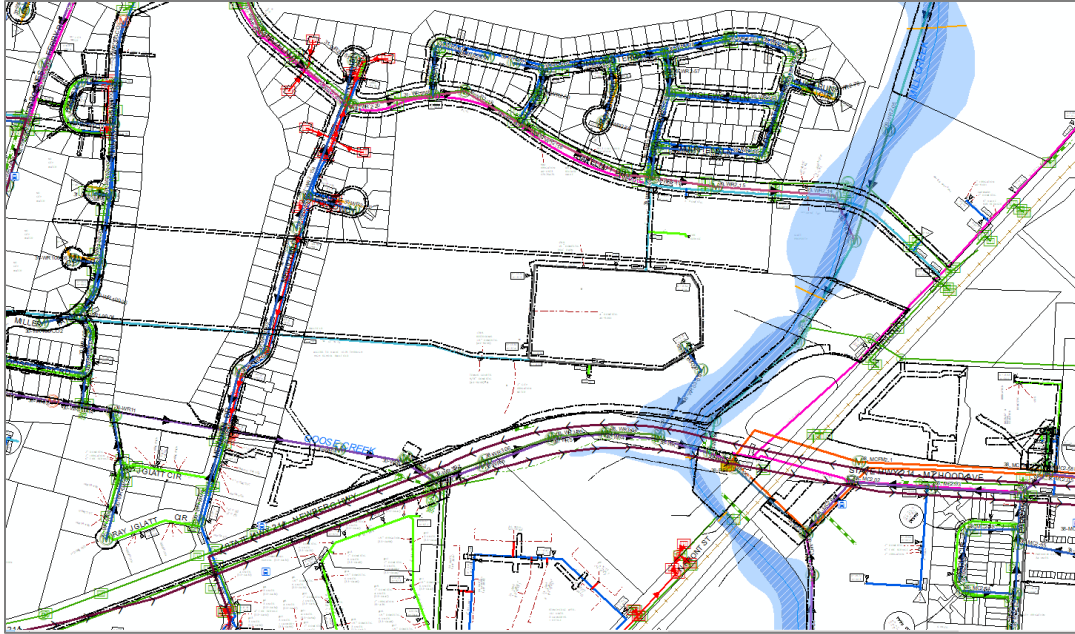
■ *Street Exception:* Staff addresses N. Front Street frontage improvements further below under both 3.03.02 “Special Setbacks” and the Street Exception Provisions section.

3.02 Utilities & Easements

3.02.01A. The Director shall require dedication of specific easements for the construction and maintenance of municipal water, sewerage and storm drainage facilities located on private property.

3.02.01C. As a condition of approval for development, including property line adjustments, partitions, subdivisions, design reviews, or Planned Unit Developments (PUDs), the Director may require dedication of public utility easements.

The basic utility easement, potable water, and sanitary sewer view from the City geographic information system (GIS) is below.



City GIS excerpt: Easements (black dashed bold line), Potable Water, and Sanitary Sewer

The City would address nonconformance, if any, as part of building permit and/or Public Works permits reviews. The appended “Public Works Comments” (February 18, 2020) from the City Engineer addresses easements and utilities. See Attachment 102A.

▲ In order to secure conformance with Public Works direction including regarding on-site PUEs, staff applies *Conditions G6 & D5*.

3.02.01B. A five-foot wide public utility easement shall be dedicated along each lot line abutting a public street.

Along both Hazelnut Drive and N. Front Street, the site plans correctly illustrate a 5-ft PUE along widened ROW.

Along the highway, a 10-ft PUE is required because WDO Figure 3.01B “Major Arterial” supersedes 3.02.01B. The site plans correctly illustrate a 10-ft PUE along widened ROW.

▲ In order to secure PUE dedications, staff applies *Condition Conditions G5-PW, G6, D2, EX1, & PP1b(ii)*.

3.02.03 Street Lighting A. Public Streets

The appended “Public Works Comments” (February 18, 2020) from the City Engineer through Attachment 102A addresses the topic through second Condition 2.

3.02.04 Underground Utilities. All permanent utility service to and within a development shall be underground, except where overhead high-voltage (35,000 volts or more) electric facilities exist.

The site plans have no indication that the applicant would fail to bury such.

✓ The provisions are met.

3.03 Setbacks and Open Space

3.03.02 Special Setbacks

As examined earlier for 3.01, three existing public streets are involved. However, because the proposal is dedicating all required ROWs, the Special Setbacks are moot.

Were ROW dedications to fail, the applicant would have to verify that the proposal conforms to the Special Setback along each of the three streets, particularly Hazelnut Drive.

⊖ None apply because ROW dedications make them moot.

3.03.03 Projections into the Setback Abutting a Street

3.03.04 Projections into the Side Setback

3.03.05 Projections into the Rear Setback

⊖ Because the proposal involves no such projections, the provisions are not applicable.

3.03.06 Vision Clearance Area; Figures 3.03A & B

The proposal includes no alterations of vision clearance area (VCA) at any of the driveways such that there would be encroachment, assuming normal pruning of shrubbery.

✓ The provision is met.

3.04 Vehicular Access

3.04.02 Drive-Throughs

Though the proposal for the existing public elementary school includes a new set of paired one-way lanes for site school bus drop-off / pick-up, because the use is not a conventional commercial use for which drive-throughs are common – such as for fast food restaurants – staff

does not consider the improvement as a “drive-through” and therefore the provisions are not applicable.

- ➡ Because the proposal involves no drive-through, the provisions are not applicable.

3.04.03 Driveway Guidelines and Standards

A. Number of Driveways

...

3. For nonresidential uses, the number of driveways should be minimized based on overall site design, including consideration of:

a. The function classification of abutting streets;

b. The on-site access pattern, including parking and circulation, joint access, turnarounds and building orientation;

c. The access needs of the use in terms of volume, intensity and duration characteristics of trip generation.

...

Access Requirements		
Table 3.04A		
		5 or More Dwelling or Living Units, School ...
Paved Width of Driveway (feet) <small>3, 4</small>	1-way	12 minimum 20 maximum
	2-way	24 minimum 30 maximum (Add 8' if a turn lane is provided)
Curb Flare Radius (feet)		25 minimum
Throat Length (feet) ⁵	Major Arterial, Minor Arterial, Service Collector	50 minimum
	Access or Local Street	20 minimum
Corner Clearance (feet) Guidelines ¹ (See Figure 3.04B)	Access or Local Street	30 minimum
	Service Collector	50 minimum
	Minor Arterial	245 minimum
	Major Arterial	300 minimum
Driveway Separation Guidelines (feet) ^{1, 2} (See Figure 3.04B)	Driveway on the same parcel	50 minimum
	Access or Local Street	none
	Service Collector	50 minimum
	Minor Arterial	245 minimum
	Major arterial	300 minimum
Turnarounds (See Figure 3.04C)	Access to a Major or Minor Arterial	Required
	Access to any other street	Requirements per the Woodburn Fire District
<ol style="list-style-type: none"> 1. The separation should be maximized. 2. Driveways on abutting lots need not be separated from each other, and may be combined into a single shared driveway. 		

Unlike as for CUs 2019-02, 5, & 6 driveway number is not an issue. The one driveway change is to add a driveway along Hazelnut Drive aligned with Graystone Drive and that conforms to the above cited standards.

Because of the opportunity of the conditional use to lessen interruption of Hazelnut Drive frontage, staff applies a maximum width of 26 ft to the driveway.

▲ Because of the opportunity of the conditional use, in order to lessen interruption of Hazelnut Drive frontage, staff applies a maximum width to the driveway through *Condition C15*.

3.04.03A. Unused driveways shall be closed.

— There are none.

3.04.03C. Interconnected Parking Facilities.

There are none. (The proposed additional driveway along Hazelnut Drive opens into a short drive aisle that terminates at a fire apparatus turnaround bulb, none of which has any off-street parking as the site plans illustrate.)

— The provision is not applicable.

3.04.04 Improvement Standards

The site plans illustrate conformance.

✓ The requirement is met.

3.04.05 Traffic Impact Analysis

A. A Traffic Impact Analysis (TIA) may be required by the Director prior to the approval of a City access permit when the Director estimates a development proposal may generate either 100 or more additional, peak hour trips, or 1,000 or more additional daily trips, within ten years of a development application.

The applicant's traffic letter (dated December 12, 2019 and submitted December 13, 2019; Attachment 106) states that because there is no student capacity increase, there is no trip generation increase.

Staff acknowledges this and the clear implication that the proposal does not exceed the two thresholds for a TIA.

✓ The requirement is met.

3.05 Off-Street Parking and Loading

3.05.02 General Provisions

The site plans illustrate that the proposal meets the general provisions.

✓ The general provisions are met.

3.05.03 Off-Street Parking

3.05.03 Off-Street Parking

A. Number of Required Off-Street Parking Spaces

1. Off-street vehicle parking spaces shall be provided in amounts not less than those set forth in this Section (Table 3.05A).
2. Off-street vehicle parking spaces shall not exceed two times the amount required in this Section (Table 3.05A).

B. Accessible parking shall be provided in amounts not less than those set forth in Table 3.05B. The number of accessible spaces shall be included as part of total required vehicle parking spaces.

C. A maximum of 20 percent of the required vehicle parking spaces may be satisfied by compact vehicle parking spaces.

D. Off-street vehicle parking spaces and drive aisles shall not be smaller than specified in this Section (Table 3.05C).

E. All uses that are required to provide 10 or more off-street parking spaces and residential structures with four or more dwelling or living units shall provide a bicycle rack within 50 feet of the main building entrance. The number of required rack spaces shall be one space per ten vehicle parking spaces

Off-Street Parking Ratio Standards Table 3.05A	
Use ¹	Parking Ratio - spaces per activity unit or square feet of gross floor area
COMMERCIAL / PUBLIC	
42. Elementary or middle school	2/ classroom

Off-Street Parking Ratio Standards Table 3.05A	
Use ¹	Parking Ratio - spaces per activity unit or square feet of gross floor area
1. The Director may authorize parking for any use not specifically listed in this table. The applicant shall submit an analysis that identifies the parking needs, and a description of how the proposed use is similar to other uses permitted in the zone. The Director may require additional information, as needed, to document the parking needs of the proposed use.	

Accessible Parking Ratio Standards Table 3.05B			
Total Spaces	Minimum Total Accessible Spaces ¹	Minimum Van Accessible Spaces	Minimum "Wheelchair User Only" Spaces
401 to 500	9	[0]	2
1. "Van Accessible Spaces" and "Wheelchair User Only" are included in "Total Accessible Spaces."			

The overall site plan, Sheet CU-A01, tabulates Phase 2 having a sitewide net result of 477 parking spaces and states that as applied to the site by end of Phase 2, minimum required parking is 377 spaces. The proposal exceeds the minimum.

✓ The requirement is met.

ADA

The proposal by end of Phase 2 provides at least 12 designated ADA / handicap (HC) stalls.

▲ To secure greater ADA parking compliance, staff applies *Condition C1a*.

Compact

The proposal converts 34 standard spaces into the only compact ones. This equals 9.0% of the minimum required parking of 377 spaces total. The 20% maximum of subsection C. is met.

Bicycle

The proposal triggers subsection E., which would require $(377 \text{ parking stalls minimum} / 10) = 37.7 \rightarrow 38$ bike stalls minimum; however, by conditional use and based on [Comprehensive Plan](#) Goal H-6 and Policies H-1.3, H-3.2, H-6.7, and K-4.4, and that the proposal involves a public high school, via *Condition C2* staff establishes a greater bicycle parking ratio equal to that of the off-street vehicle parking ratio: retention of the existing 24 bike stalls and addition of 2.0 per additional classroom, which for 15 additional classrooms totals $(24 + [15 \times 2]) = 54$ minimum.

The general WDO rates fits some land uses better than others, for example commercial office well, but a public high school poorly. The goal and policies are:

“Goal H-6. Coordinate with Marion County in planning for a safe and efficient county-wide transportation system by:

(a) Encouraging use of alternative modes of transportation including mass transit, bicycling, walking and carpooling; and

(b) Addressing transportation needs appropriate to both urban and rural areas throughout the county.

H-1.3 Develop a bikeway system that provides routes and facilities that allow bicyclists to travel from residential areas to schools, parks, places of employment, and commercial areas. Identify off-street facilities in City greenway and park areas. Ensure all new or improved collector and arterial streets are constructed with bicycle lanes.

H-3.2 Develop a plan for improving pedestrian and bicycle safety for travel to and from local schools, commercial areas, and major activity centers.

H-6.7 The Woodburn TSP shall include measures to improve the walking and biking environment by providing sidewalks in all new developments and by providing an interconnecting system of pedestrian connections. Designing for a comfortable and practical pedestrian environment is especially important in Downtown Woodburn and within the Nodal Development Overlay.

K-4.4 Efforts should continue to evaluate the feasibility of bicycle paths linking the CBD with City schools and parks.”

Staff cites also Transportation System Plan (TSP; 2005) Goal 5 Policy 1:

“Identify a range of potential Transportation Demand Management (TDM) strategies that can be used to improve the efficiency of the transportation system by shifting single-occupant vehicle trips to other modes and reducing automobile reliance at times of peak traffic volumes.”

The City Council on September 23, 2019 adopted the 2019 major update to the TSP that dated from 2005. (Per the City Charter, Ordinance 2575 takes effect 30 days later on October 23.) It retains much of existing goals and policies and adds:

“Goal 1 ... Objective 1C

Develop a comprehensive low stress network of bicycle lanes and routes that link major activity centers such as residential neighborhoods, schools, parks, commercial areas, and employment centers.

Goal 1 ... Objective 1D

Develop a comprehensive network of sidewalks and off-street pathways that improve pedestrian mobility within neighborhoods and link residential areas to schools, parks, commercial areas, and employment centers.

Mill Creek Greenway Master Plan

An additional factor is the [Mill Creek Greenway Master Plan \(2006-07\)](#), excerpted in Attachment 107. Along with TSP Figure 8 “Pedestrian Plan Elements” (Attachment 105) the subject property is planned for an off-street bicycle pedestrian trail extending from Mill Creek west along the highway and then along Goose Creek. The greenway plan map “Mill Creek Greenway: North Tributary: Goose Creek” notes a crossing of the on-site “Bulldog Drive” drive aisle and a potential on-site trailhead along Hazelnut Drive. Page 29 of the plan describes “safe route to school” connections to and from the schools site.

In short, the greenway master plan illustrates near and through the school site a public off-street bicycle/pedestrian improved trail that would attract pedestrians and cyclists. This reinforces the need to have more bicycle parking at Woodburn H.S. (Incidentally, the Assistant City Administrator, who oversees parks and recreation, had declined to require the applicant either to construct bicycle/pedestrian trail improvements based on the Mill Creek Greenway Master Plan or pay a fee in-lieu.)

Applicable Comprehensive Plan policies include:

“L-1.6 It is the policy of the City to implement the Mill Creek Greenway Master Plan, and to manage the Mill Creek, Goose Creek and Senecal Creek corridors as public greenways and pathways; multiple functions will include open space and habitat preservation, flood control, cycling and walking on all-weather pathways, nature recreation and education, and limited playground activities where there is a deficiency of neighborhood parks. The City will establish and enforce a healthy streams policy to ensure that Woodburn’s waterways are preserved and well-maintained.

L-1.7 To provide for a continuous public greenway and pathway system, it is the policy of the City to acquire privately-owned segments along Mill Creek, Goose Creek, and Senecal Creek and other stream corridors including the west tributary from Settlemier Park to Parr Road. It is the policy of the City to seek dedication of floodplains and creek corridors for natural areas, neighborhood recreation areas, open space and transportation.”

Example: Wilsonville

Lastly, looking to a nearby city of similar population and size, Wilsonville, its [development code](#) in [Section 4.155 Parking, Loading, and Bicycle Parking](#) through Table 5 requires 8 stalls per class (above the 2nd grade, with a lesser ratio for kindergarten through 2nd grade).

Example: Bend

As a second example, looking to a relatively more populous but also isolated city in Oregon, Bend, its [development code](#) in [Chapter 3.3 Vehicle Parking, Loading and Bicycle Parking](#) through Table 3.3.300 requires 1 stall per employee or 4 seats in the auditorium, whichever is greater. Without the numbers of Woodburn H.S. employees or auditorium seats, staff cannot apply this ratio to the quantity of classrooms in Lincoln; however, assuming that a typical classroom seats 30, then per classroom the auditorium rate becomes 7½ stalls per classroom, close to the Wilsonville standard and like Wilsonville well above 2 per classroom as Woodburn staff conditions.

If higher quantities work in Bend and Wilsonville, which rely heavily on privately-owned cars but also have sidewalks, bicycle/pedestrian paths, and local public transit, a higher quantity would work for Woodburn H.S..

Transit

Additionally, there is City bus service through both the local [Woodburn Transit Service](#) or [Woodburn Transit System \(WTS\)](#) and regional Salem-Keizer Cherriots 10X express bus route. Admittedly, though Route 10X passes along the highway, there is no nearby Cherriots stop. The closest stops are downtown Woodburn and along the highway near the intersection with N. Pacific Highway / U.S. 99E. However, the WTS has two stops, each centered along the north and south highway frontages and just east of N. 5th Street / Meridian Drive.

Lastly, the evidence is in support of the premise, “Build it, and they will come.”

The site plans note and illustrate a net result of retention of 24 stalls plus addition of 2 stalls per additional classroom for a minimum total of 54.

Bicycle Parking Maximum Distance

Sheet 2.00 shows an amount of bicycle parking within 50 ft of the east/main entrance of the high school, meeting the 50-ft maximum distance provision.

The applicant needs to provide additional bicycle parking. Staff applies a condition.

▲ To secure the minimum amount of bicycle parking, staff applies *Condition C2*.

Parking Space and Drive Aisle Dimensions Table 3.05C							
Parking Angle	Type of Space	Stall Width (feet)	Curb Length (feet)	Stripe Length (feet)	Stall to Curb (feet)	Drive Aisle Width (feet)	
						1-way	2-way
A		B	C	D	E	F	G
90°	Standard or Accessible	9.0	9.0	19.0	19.0	24.0	24.0
	Compact	7.5	7.5	15.0	15.0	22.0	
	Car Accessible Aisle	6.0	6.0	19.0	19.0	24.0	
	Van Accessible Aisle	8.0	8.0	19.0	19.0		

1. A parking space may occupy up to two feet of a landscaped area or walkway. At least four feet clear width of a walkway must be maintained.
2. Space width is measured from the midpoint of the double stripe.
3. Curb or wheel stops shall be utilized to prevent vehicles from encroaching on abutting properties or rights-of-way.
4. The access aisle must be located on the passenger side of the parking space, except that two adjacent parking spaces may share a common access aisle.
5. Where the angle of parking stalls differ across a drive aisle, the greater drive aisle width shall be provided.

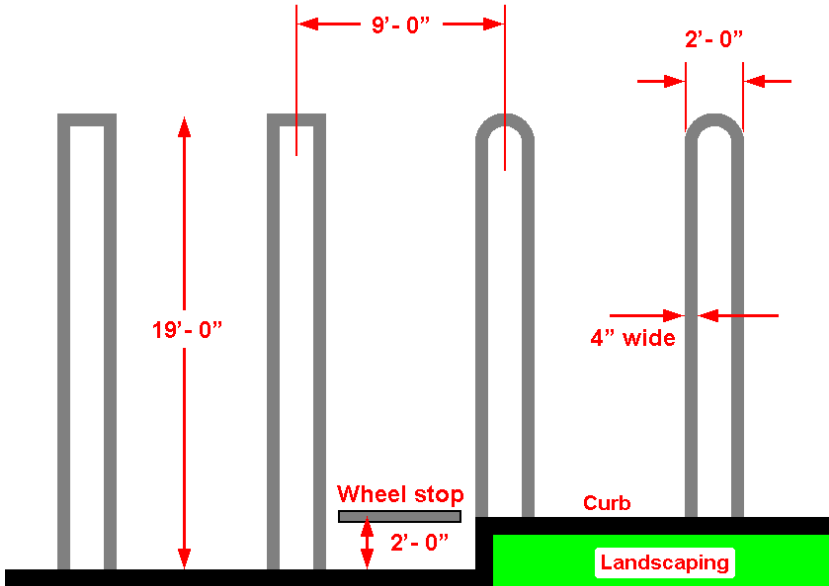


Figure 3.05C - Parking Space Striping

The applicant proposes all altered and additional stalls, which are all at 90°, to the drive aisle with dimensions, double-striping, and curbing that meet or exceed the minimum standards.

✓ The requirement is met.

3.05.04 Off-Street Loading

B. The off-street loading facilities shall be on the same lot, or site, as the use or structure they are intended to serve. Required loading spaces and required parking spaces shall be separate and distinct, except that if authorized through a land use decision, a parking area may be used for loading during those times when the vehicle parking area is not in use.

Loading Space Requirements Table 3.05D				
Use and Area (square feet)	Minimum Number of Spaces	Minimum Size of Space (feet)		
		Width	Length	Height
Office				
0 – 4,999	0	12	30	14
5,000 – 41,999	1			
42,000 or more	2			
Nonresidential uses, except office, in the CO, CG, and NNC zones				
0 – 9,999	1	12	30	14
10,000 – 41,999	2			
42,000 – 81,999	3			
82,000 or more	4			

Though the use is nonresidential, because the subject property is zoned P/SP, the table is not applicable.

⊖ The provisions are not applicable.

3.05.05 Shared Parking

The applicant proposes none.

⊖ The provisions are not applicable.

3.06 Landscaping

3.06.02 General Requirements

The landscape plan sheets (L1.02-2.06) illustrate and note that the general provisions are met, including irrigation, curbing, and avoidance of prohibited tree species.

✓ The requirement is met.

3.06.03 Landscaping Standards

A. Street Trees

Hazelnut Drive

The existing improved frontage appears to conform. Upon addition of the one driveway, the proposal will replace the displaced street tree. The proposal notes it will add the minimum number of additional street trees upon improvement of the frontage along Tax Lot 051W08BC00200.

✓ The requirement is met.

N. Front Street

The existing improved frontage fails to conform. With the exception (EXCP) request obviating a planter strip, the site plans illustrate include a number of additional east yard trees near widened ROW.

■ *Street Exception:* Staff addresses N. Front Street street trees further below under the Street Exception Provisions section.

Highway

The existing improved frontage appears to conform west of the driveway but not east. Along the easterly frontage, a planter strip exists. The proposal notes it will add the minimum number of additional street trees here.

✓ The requirement is met.

B. & Tables 3.06A & B

Planting Requirements Table 3.06A		
Location	Planting Density, Minimum	Area to be Landscaped, Minimum
Setbacks abutting a street	1 PU/15 square feet	Entire setback excluding driveways
Buffer yards	1 PU/20 square feet	Entire yard excluding off-street parking and loading areas abutting a wall

Planting Requirements Table 3.06A		
Location	Planting Density, Minimum	Area to be Landscaped, Minimum
Other yards	1 PU/50 square feet	Entire yard, excluding areas subject to more intensive landscaping requirements and off-street parking and loading areas
Off-street parking and loading areas	<ul style="list-style-type: none"> • 1 small tree per 10 parking spaces; or¹ • 1 medium tree per 15 parking spaces; or¹ • 1 large tree per 25 parking spaces¹ and <ul style="list-style-type: none"> • 1 PU/20 square feet excluding required trees² 	<ul style="list-style-type: none"> • RS, R1S, RSN, RM, RMN, P/SP, CO, CG and MUV zones: 20% of the paved surface area for off-street parking, loading and circulation • DDC, NNC, IP, IL, and SWIR zones: 10% of the paved surface area for off-street parking, loading and circulation • Landscaping shall be within or immediately adjacent to paved areas
Common areas, except those approved as natural common areas in a PUD	3 PU/50 square feet	Entire common area
<ol style="list-style-type: none"> 1. Trees shall be located within off-street parking facilities, in proportion to the distribution of the parking spaces. 2. Required landscaping within a setback abutting a street or an interior lot line that is within 20 feet of parking, loading and circulation facilities may also be counted in calculating landscaping for off-street parking, loading and circulation areas. 		

Plant Unit (PU) Value Table 3.06B		
Material	Plant Unit (PU) Value	Minimum Size
1. Significant tree ¹	15 PU each	24" Diameter
2. Large tree (60-120 feet high at maturity) ¹	10 PU each	10' Height or 2" Caliper
3. Medium tree (40-60 feet high at maturity) ¹	8 PU each	10' Height or 2" Caliper
4. Small tree (18-40 feet high at maturity) ¹	4 PU each	10' Height or 2" Caliper
5. Large shrub (at maturity over 4' wide x 4' high) ¹	2 PU each	3 gallon or balled

Plant Unit (PU) Value Table 3.06B		
Material	Plant Unit (PU) Value	Minimum Size
6. Small to medium shrub (at maturity maximum 4' wide x 4' high) ¹	1 PU each	1 gallon
7. Lawn or other living ground cover ¹	1 PU / 50 square feet	
8. Berm ²	1 PU / 20 lineal feet	Minimum 2 feet high
9. Ornamental fence ²	1 PU / 20 lineal feet	2½ - 4 feet high
10. Boulder ²	1 PU each	Minimum 2 feet high
11. Sundial, obelisk, gnomon, or gazing ball ²	2 PU each	Minimum 3 feet high
12. Fountain ²	3 PU each	Minimum 3 feet high
13. Bench or chair ²	0.5 PU / lineal foot	
14. Raised planting bed constructed of brick, stone or similar material except CMU ²	0.5 PU / lineal foot of greatest dimension	Minimum 1 foot high, minimum 1 foot wide in least interior dimension
15. Water feature incorporating stormwater detention ²	2 per 50 square feet	None
<p>1. Existing vegetation that is retained has the same plant unit value as planted vegetation.</p> <p>2. No more than twenty percent (20%) of the required plant units may be satisfied by items in lines 8 through 15.</p>		

The landscape plans (the L2 series of sheets) illustrate bringing the developed campus into greater conformance with minimum planting densities, landscaped areas, and trees for yards and parking and loading areas. (See above cited 1.04 regarding nonconformance.)

✓ The requirement is met.

Prohibited Trees Table 3.06C

The landscape plans notes that additional trees are not any that the table prohibits.

✓ The requirement is met.

3.06.05 Screening & Table 3.06D

A. Screening between zones and uses shall comply with Table 3.06D.

Because the property to be (re)developed is zoned P/SP, the provisions are not applicable. Staff opts not to exercise the discretion that the table grants to require one or more Architectural Wall or Walls along the west RS or CO zoning district boundaries or the northeast IL zoning district boundary.

⊖ The provisions are not applicable.

3.06.05B.

All parking areas, except those for single-family and duplex dwellings, abutting a street shall provide a 42-inch vertical visual screen from the abutting street grade. Acceptable design techniques to provide the screening include plant materials, berms, architectural walls, and depressed grade for the parking area. All screening shall comply with the clear vision standards of this ordinance (Section 3.03.06).

Hazelnut Drive

The east front visitor parking area would lie closer to Hazelnut Drive than the building closest to the road, and this would continue to be the case with the proposal, which illustrates shrubbery along the north side of the parking area. Because, the landscape plans do not indicate if the shrubbery is evergreen, at the minimum planting size per Table 3.06B above, and able to grow to at least 3½ feet at maturity, staff applies a condition.

✗ The proposal fails to meet the provision fully. Staff applies *Condition D3*.

N. Front Street

There are none.

⊖ The provisions are not applicable.

Highway

There are none.

⊖ The provisions are not applicable.

3.06.06 Architectural Walls

B. Design Standards and Guidelines

As examined earlier for 3.06.05A, the proposal requires no Architectural Wall; however, outdoor storage of recycling and trash does require enclosure through an Architectural Wall or Walls. The application materials indicated no upgrade to existing such storage, and based on 1.04 as cited earlier staff interprets the proposal to not necessitate additional such area, allowing the existing area to remain as is.

- ⊖ The provisions are not applicable.

C. Retaining walls should/shall meet the texture and color requirements of architectural walls in or abutting residential districts, where the texture and color requirements apply to the visible face of the retaining wall.

1.02

Abutting: Touching on the edge or on the line, including at a corner. It shall include the terms adjacent, adjoining and contiguous.

Adjacent: Near, close or bordering but not necessarily contiguous with; adjoining but separated by a right-of-way.

- ⊖ Because the proposal includes no retaining walls, the provisions are not applicable.

3.06.07 Significant Trees on Private Property

Because the site plans indicate no removal of such, the provisions are not applicable.

- ⊖ The provisions are not applicable.

3.07 Architectural Design

3.07.06 Standards for Non-Residential Structures in Residential, Commercial and Public/Semi Public Zones

A. The following design guidelines shall be applicable to all non-residential structures and buildings in the RS, RSN, R1S, RM, RMN, CO, CG, and P/SP zones.

B. Architectural Design Guidelines

1. Mass and Bulk Articulation Guidelines

a. Building facades visible from streets and public parking areas should be articulated, in order to avoid the appearance of box-like structures with unbroken wall surfaces.

b. The appearance of exterior walls should be enhanced by incorporating three-dimensional design features, including the following:

(1) Public doorways or passage ways through the building

(2) Wall offsets or projections

(3) Variation in building materials or textures

(4) Arcades, awnings, canopies or porches

2. Materials and Texture Guidelines

a. Building exteriors should exhibit finishes and textures that reduce the visual monotony of bulky structures and large structural spaces. Building exteriors should enhance visual interest of wall surfaces and harmonize with the structural design.

b. The appearance of exterior surfaces should be enhanced by incorporating the following:

(1) At least 30% of the wall surface abutting a street should be glass.

(2) All walls visible from a street or public parking area should be surfaced with wood, brick, stone, designer block, or stucco, or with siding that has the appearance of wood lap siding.

(3) The use of plain concrete, plain concrete block, corrugated metal, plywood, T-111 and sheet composite siding as exterior finish materials for walls visible from a street or parking area should be avoided.

(4) The color of at least 90 percent of the wall, roof and awning surface visible from a street or public parking area should be an "earth tone" color containing 10 parts, or more of brown or a "tinted" color, containing 10 parts or more white.

(5) Fluorescent, "day-glo," or any similar bright color shall not be used on the building exterior.

3. Multi-Planned Roof Guidelines

a. The roof line at the top of a structure should establish a distinctive top to the building.

b. The roof line should not be flat or hold the same roof line over extended distances. Rather, the roof line should incorporate variations, such as:

(1) Offsets or jogs in the plane of the roof;

(2) Changes in the height of the exterior wall for flat roof buildings, including parapet walls with variations in elevation or cornices

4. Roof-Mounted Equipment Guidelines

All roof-mounted equipment, except solar collectors, should be screened from view by:

a. Locating roof-mounted equipment below the highest vertical element of the building, or

b. Screening roof-mounted equipment using materials of the same character as the structure's basic materials

5. Weather Protection Guidelines

All building faces abutting a street or a public parking area should provide weather protection for pedestrians. Features to provide this protection should include:

a. A continuous walkway at least eight feet wide along the face of the building utilizing a roof overhang, arcade, awnings or canopies

b. Awnings and canopies that incorporate the following design features:

(1) Angled or curved surfaces facing a street or parking area

(2) A covering of fabric, or matte finish vinyl

(3) A constant color and pattern scheme for all buildings within the same development

(4) No internal back lighting

6. Solar Access Protection

Obstruction of existing solar collectors on abutting properties by site development should be minimized.

C. Building Location Guidelines

1. Within the prescribed setbacks, building location and orientation should compliment abutting uses and development patterns.

2. The maximum yard abutting a street should be 150 feet.

The high school building exterior remains unaltered except where the site plans propose a north addition/expansion. The proposal also adds a north outbuilding. The elevations show that the addition/expansion has largely what the guidelines describe, though the façade has

distracting randomly placed narrow bands of orange, yellow, and navy blue amid the steel blue of the main wall plane.

It would be better for at least the outbuilding north façade, which faces Hazelnut Drive, to have a calmer façade with bands of one accent color and limited to placement as window lintels.

✓ The provisions are largely met.

Conditional Use Provisions

The conditional use is a public K-12 school, specifically Woodburn High School (H.S.).

The existing site development and the high school it already has date from 1977, pre-dating City adoption of the WDO in 2002.

Conditional Use Criteria

5.03.01 Conditional Use

B. Criteria:

1. The proposed use shall be permitted as a conditional use within the zoning district.
2. The proposed use shall comply with the development standards of the zoning district.
3. The proposed use shall be compatible with the surrounding properties.

Relevant factors to be considered in determining whether the proposed use is compatible include:

- a. The suitability of the size, shape, location and topography of the site for the proposed use;
- b. The capacity of public water, sewerage, drainage, street and pedestrian facilities serving the proposed use;
- c. The impact of the proposed use on the quality of the living environment:
 - 1) Noise;
 - 2) Illumination;
 - 3) Hours of operation;
 - 4) Air quality;
 - 5) Aesthetics; and
 - 6) Vehicular traffic.
- d. The conformance of the proposed use with applicable Comprehensive Plan policies; and
- e. The suitability of proposed conditions of approval to ensure compatibility of the proposed use with other uses in the vicinity.

CU criteria and factors executive summary

The applicant addresses the criteria through application materials including drawings, a narrative, and responses to incompleteness letters from staff, copies of which are available upon request.

Staff concurs with the applicant's proposal.

Through the conditional use (CU) as opportunity, staff applies a light touch regarding upgrades of on-site nonconformities, specifically through improvements limited to encouraging staff carpooling/vanpooling, increasing parking and drive aisle area pedestrian crossing safety and ADA compliance as well as pedestrian comfort for staff, students, and visitors, and prevent exterior lighting nuisance. The applicant had revised site plans even before conditioning and largely in keeping with staff direction.

Staff applies a greater yet adept touch through both the CU and Design Review (DR) to identify and obtain frontage/public/street improvements that the WDO requires as is through the 1.02 definition of "development" and 3.01.

Capacity of public facilities

The City Engineer through Attachment 102A did not identify any deficiencies of or threats to public infrastructure in regards to factor b. of the third CU criterion – subsection B.3b – and the proposal indicates that through Phase 2 the applicant will have constructed required street improvements.

Illumination

A lighting condition addresses the illumination factor c. of the third CU criterion – subsection B.3c(2) – for altered and additional exterior fixtures in order to prevent light encroachment into ROWs and adjacent residentially zoned property as well as light pollution – glaring into others' eyesight. (WDO 3.05.02L and Ordinance No. 2338, Section 5A Light Trespass regulate aspects of exterior lighting, but fail to regulate fixture cut-offs or shielding as well as mounting heights. Regarding signage illumination, staff applies a *Condition C8c* to require that any monument signage that has electronic changing image to be off during certain evening and night hours to lessen light pollution and distracted driving. (The hours of 9:00 p.m. to 7:00 a.m. come from Ordinance No. 2338.)

Air quality

A carpool/vanpool (C/V) condition encourages carpooling in order to improve air quality and thereby addresses the air quality factor c. 4 of the third CU criterion – subsection B.3c(4).

Aesthetics

A condition for more north yard trees near Hazelnut Drive furthers aesthetics, addressing subsection B.3c(5).

Regarding the vehicular traffic factor of subsection B.3c(6), as examined above for 3.04.05, the applicant states in a traffic letter that there is no trip generation increase.

Overall factors support approval – with conditions, including through the factor of subsection B.3d: conformance with applicable Comprehensive Plan policies. Staff conditions certain improvements to further cited policies.

✓ The conditional use criteria are met. Staff recommends approval with conditions of the request.

Street Exception Provisions

5.03.03 Exception to Street Right of Way and Improvement Requirements

A. Purpose: The purpose of a Type III Exception is to allow a deviation from the development standard required for the functional classification of the street identified in the Transportation System Plan. Street exceptions are processed in conjunction with a development proposal that is a Type III application.

B. Criteria:

1. The estimated extent, on a quantitative basis, to which the rights of way and improvements will be used by persons served by the building or development, whether the use is for safety or convenience;
2. The estimated level, on a quantitative basis, of rights of way and improvements needed to meet the estimated extent of use by persons served by the building or development;
3. The estimated impact, on a quantitative basis, of the building or development on the public infrastructure system of which the rights of way and improvements will be a part; and
4. The estimated level, on a quantitative basis, of rights of way and improvements needed to mitigate the estimated impact on the public infrastructure system.

C. Proportionate Reduction in Standards: An exception to reduce a street right of way or cross section requirement below the functional classification standard may be approved when a lesser standard is justified based on the nature and extent of the impacts of the proposed development. No exception may be granted from applicable construction specifications.

D. Minimum Standards: To ensure a safe and functional street with capacity to meet current demands and to ensure safety for vehicles, bicyclists and pedestrians, as well as other forms of non-vehicular traffic, there are minimum standards for right of way and improvement that must be provided to meet the standards of this Ordinance (Section 3.01). Deviation from these minimum standards may only be considered by a variance procedure.

A Street Exception is akin to an adjustment or variance and specific to frontage/public/street improvements. In return for granting a Street Exception, the City applies conditions to secure public benefits.

The applicant addressed the EXCP criteria through the application materials and focusing on not majorly altering the N. Front Street frontage. Planning and Public Works staff concur with the applicant's proposal. Upgrades are limited to bringing existing sidewalk ramps at the "Bulldog Drive" driveway into ADA compliance.

ROW & PUEs

Staff considers these required in all instances, Street Exception or not, because providing ROW and a PUE to conform to the functional class and model/standard cross section necessitates no

improvements and so no construction expense. Staff memorializes dedications both to affirm that they remain required and to secure them.

N. Front Street: Street trees

Staff interprets the WDO such that though 3.01 and 3.06 are separate sets of provisions, the model/standard cross sections of 3.01 by illustrating street trees *de facto* include street trees as a type of required improvement. Therefore, staff can handle street trees through a Street Exception when an applicant applies for such. The applicant has not.

Staff accepts that approval of the N. Front Street EXCP means there will be no planter strip and so no conformance with street tree provisions, at least not regarding placements within ROW. The proposal includes additional east yard trees near the widened ROW.

Hazelnut Drive and the highway

The proposal will upgrade Hazelnut Drive unimproved frontage at the northeast along Tax Lot 051W08BC00200 through Phase 2.

Because the highway frontage appears to conform, except for a full amount of street trees that the proposal provides through Phase 1, EXCP is not relevant.

ADA

The City cannot except from ADA federal and state standards, and 5.03.03C states, “No exception may be granted from applicable construction specifications.” The applicant needs to upgrade to current such standards the highway and road sidewalks where they cross driveways regarding in particular maximum cross slope. Through incompleteness letters and responses to them, the applicant and staff had discussed this and the applicant expects a condition. Staff applies *Conditions C1b, EX1b(i)*, and “Public Works Comments” (October 16, 2019; Attachment 102A) *Condition 11*.

Phasing Plan Provisions

Phasing Plan request for a project of Phases 1 & 2 – including to defer to Phase 2 all frontage/public/street improvements that remain required. The application materials indicate that Phase 2 would be as late as 2027, and there is no condition requiring that the applicant do any of construct Phase 2, construct the public improvements, or pay a fee in-lieu.

The applicant submitted narrative text addressing the criteria.

Phasing Plan Criteria

5.03.05 Phasing Plan for a Subdivision, PUD, Manufactured Dwelling Park or any other Land Use Permit

- A. Purpose:** The purpose of a Type III Phasing Permit is to allow phased construction of development while meeting the standards of this ordinance (Sections 2 and 3), while providing fully functional phases that develop in compliance with the tentative approval for the development.
- B. Criteria:** The proposed phasing of development shall:
1. Ensure that individual phases will be properly coordinated with each other and can be designed to meet City development standards; and
 2. Ensure that the phases do not unreasonably impede future development of adjacent undeveloped properties;
 3. Ensure that access, circulation, and public utilities are sized for future development of the remainder of the site and adjacent undeveloped sites.

The proposal does not interfere with future high school campus redevelopment or expansion within the site and does not hinder any adjacent sites, most of which are already developed.

The proposal includes but defers to Phase 2 all frontage/public/street improvements that remain required. The application materials indicate that Phase 2 would be as late as 2027, and there is no condition requiring that the applicant do any of construct Phase 2, construct the public improvements, or pay a fee in-lieu.

However, were the applicant to not construct Phase 2, at the least, if the District were to return for City approval for further development or redevelopment, the City retains authority and ability through the WDO and public works standards to require public improvements in conformance with the version of the WDO and public works standards as they would exist at the time and to document this through another land use review and approval with conditions as well as Public Works Department permits.

Lastly, The City Engineer through Attachment 102A did not identify any deficiencies or threats to public infrastructure posed by the phasing plan.

The criteria are met with one condition to memorialize what improvements the proposal assigns to which of the two phases.

▲ The phasing plan criteria are met with *Condition PP1*.

Recommended Conditions of Approval

Staff recommends approval of the consolidated applications based on the findings in the staff report and attachments, which are incorporated by this reference, as well as applying the following conditions of approval:

General

G1. Either prior to or as part of building permit application, the applicant shall submit revised site plans meeting the conditions of approval to the attention of the Planning Division and obtain Division approval or permit sign-off.

G2. The applicant or successor shall develop the property in substantial conformance with the final plans submitted and approved with these applications, except as modified by these conditions of approval. Were the applicant to revise plans other than to meet conditions of approval or meet building code, even if Planning Division staff does not notice and signs off on building permit issuance, Division staff retains the right to obtain restoration of improvements as shown on an earlier land use review plan set in service of substantial conformance.

G3. References: The descriptions below define certain words, phrases, and assumptions in the context of the conditions of approval:

- “ADA” refers to the federal Americans with Disabilities Act of 1990.
- “Director” refers to the Community Development Director.
- “District” refers to the Woodburn School District.
- “Highway” refers to Oregon Highway 214 / Newberg Highway.
- “H.S.” refers to high school.
- “Public Works” refers to the Public Works Department, especially the Engineering Division.
- “PUE” means public utility easement.
- “ROW” means right-of-way.
- “WDO” refers to Woodburn Development Ordinance.

G4. Due date: Unless a condition specifies otherwise, meeting the conditions is due by building permit issuance for the implied phase.

G5-PW. Public Works: Follow the appended "Public Works Comments February 18, 2020" (Attachment 102A).

G6. Recordation(s): Dedications of ROWs, PUEs, and other easements involve recordation(s) with Marion County. The applicant shall:

- a. Prior to recordation with the County, follow City Public Works Department review and dedication process as directed by the City Engineer or higher authority, including the use of Public Works document templates;

- b. Submit a draft copy of the ROW dedications and easements, including text and drawing(s), to the attention of both the Director and City Engineer for their reviews and directions. Easement text shall contain references to “City of Woodburn Conditional Use CU 2019-07”, “Woodburn High School”, street address, and tax lot number. Submit through the Planning Division both Adobe PDF and written copies under a cover or transmittal letter providing project context, including land use / planning / zoning case file numbers, street address, tax lot number, and project name and an explanation of the purpose of the submittal that refers to this condition of approval;
- c. Record dedications prior to building permit final inspection;
- d. Upon recordation, submit proof to pass building permit final inspection; and
- e. Apply to Marion County for recordation(s) no later than six (6) months past the Planning Commission tentative decision date of February 27, 2020, i.e. by August 31, 2020, and complete recordation(s) no later than a year past, i.e. by February 28, 2021. The due date to complete recordation(s) shall not supersede when recordation(s) are due relative to the building permit stage.

G7. Inspection: The applicant shall construct, install, or plant all improvements, including landscaping, prior to City staff verification. At least three (3) City business days prior to a desired date of planning and zoning inspection of site improvements, the applicant shall request a land use / planning / zoning inspection through the [Accela / Oregon ePermitting](#) building permit record(s) for the project. This inspection is required and separate from and in addition to the usual building code and fire and life safety inspections. Note that Planning staff are not primarily inspectors, do not have the nearly immediate availability of building inspectors, and are not bound by any building inspector’s schedule or general contractor convenience. For help with ePermitting, call the Oregon Building Codes Division help line at (503) 373-7396.

Conditional Use 2019-07

C1. ADA:

- a. On-site: The applicant shall make ADA-compliant the walkway ramp transition at the location Sheets CU-A01 & 3 illustrate and note for Phase 1.
- b. Highway and road sidewalks: See Street Exception (EXCP) conditions.

C2. Bicycle parking:

- a. Ratio: Required off-street bicycle parking shall number a total of the existing amount of twenty-four (24) stalls plus at least two (2) stalls per additional classroom. (This would equal 24 existing stalls, plus 18 stalls for Phase 1 nine classrooms, and plus for Phase 2 six classrooms 12 stalls yielding 54 stalls total.)
- b. Distribution: The number shall be divided among at least four (4) areas: the main entrance with sixteen (16) stalls minimum, a southeast entrance with twelve (12) stalls

minimum, a west staff entrance with twelve (12) stalls minimum, and an addition/expansion or outbuilding entrance with fourteen (14) stalls minimum.

- c. Facility: Each stall shall adjoin a facility designed for the securing of one or more bicycles, such as a U-rack or wave rack.
- d. Stall size: The minimum size of additional or altered stalls shall be two (2) feet wide by six (6) feet long, and minimum vertical clearance shall be eight (8) feet. If a stall is wall-mounted, the minimum stall depth shall be five (5) feet. The minimum maneuvering area to access any stall shall be at least five (5) feet deep. The dimensional standards do not apply to the existing bicycle parking at the south and west.

This condition is due by building permit final inspection for each phase because additional bicycle parking is allocated between the two phases.

C3. Bond / bonding / performance guarantee: Were the applicant to bond any improvements as allowed by and pursuant to WDO 4.02.08, the amount shall be 200% instead of 120%.

C4. Carpool/vanpool (C/V) parking:

- a. Number: Within a staff-only parking area there shall be at least two C/V stalls located within ninety (90) feet of a building occupied by District staff and limited to carpools and vanpools.
- b. Striping: Each stall shall be striped "CARPOOL/VANPOOL ONLY" in lettering one (1) foot high minimum.
- c. Signage: Each stall shall have a wall-mounted or pole-mounted sign for "Carpool/Vanpool Only". Minimum sign face size shall be one (1) foot wide by one-and-a-half (1½) feet high. The top of a posted sign shall be between five-and-a-half (5½) and six-and-a-half (6½) feet above vehicular grade.
- d. If there are State of Oregon standards that conflict with b. and c., the applicant may go by state standards instead and provide a copy of them to staff.

This condition is due by building permit final inspection for each phase because the improvements manifest in Phase 1 and are displaced and reproduced in Phase 2.

C5. Driveway widths:

- a. Two-way: Any additional driveway or reconstructed driveway shall have a maximum width of either twenty-four (24) feet or, were the Oregon Fire Code (OFC) to apply such that a fire apparatus access road of minimum 26 feet would be required such as for a multi-story building, then twenty-six (26) feet. The maximum may increase by up to eight (8) feet were a driveway to include an outbound left-turn lane or pocket.
- b. One-way: Any additional driveway or reconstructed driveway shall have a maximum width of twelve (12) feet if one-lane and twenty (20) feet if two-lane.

C6. Fire access plan: Through the City building permit application and review process, the applicant shall submit for the Woodburn Fire District Fire Marshal a detailed plan sheet

illustrating and noting: fire apparatus access and lane routes, lane widths, lane inside and outside turning radii of the minimums per the Oregon Fire Code (OFC), lane height clearance, fire suppression water line, hydrants, a PUE for the fire suppression water line and hydrants, fire department connections (FDCs), and fire riser rooms.

C7. Greenway: The applicant shall by Phase 1 dedicate to the City an easement for public pedestrian and cyclist access along the likely alignment of the Mill Creek Greenway Trail based on the image on plan p. 17, "North Tributary: Goose Creek Mill Creek" (Attachment 107), and existing topography. The easement or easements shall extend at least (a) between Hazelnut Drive ROW and highway ROW and (b) between highway ROW along the Goose Creek corridor to the west property line. The easement may be continuous from Hazelnut Drive ROW to the west property line if it has a segment that parallels roughly the highway ROW and extends to or abuts the highway ROW. The minimum greenway trail easement width shall be twenty (20) feet.

C8. Lighting: This applies to additional or altered exterior lighting fixtures.

- a. Full cut-off: They shall be full cut-off or fully shielded models.
- b. Heights: As measured to the underside of the light fixture:
 - i. Wall: Wall-mounted fixtures shall be no higher than eight (8) feet above walkway finished grade. (This height limit is not applicable to emergency egress lighting and permanent wall signs allowed through WDO 3.10 were they to have interior illumination.)
 - ii. Parking pole: Exterior pole-mounted fixtures within five (5) feet of or in parking, loading, and vehicular circulation areas shall be no higher than fourteen (14) feet above vehicular finished grade.
 - iii. Other pole: Remaining exterior pole-mounted fixtures, excepting sports field lighting, shall be no higher than fifteen-and-a-half (15½) feet above grade.
- c. Monument signage: Any monument signage illuminated by lighting exterior to it shall be full cut-off or fully shielded, and any monument signage that has electronic changing image shall have it off from 9:00 p.m. to 7:00 a.m. Pacific time.

This condition is due by building permit final inspection.

C9. Modular buildings / modulares: Consistent with the land use review plan set submitted February 4, 2020, Sheets CU-A00 Overall Existing Site Plan and CU-A01 Overall Master Plan, the number of modular buildings shall decrease by at least ten (10) by Phase 1. The removal of modular buildings is due by building permit final inspection for Phase 1. (The City may later supersede this condition by taking action on a future land use application submittal as to allow one or more modular buildings.)

C10. Parking area improvements:

- a. East front student/visitor: As proposed, per Sheets CU-A01 & 3 and L01 & 2, the applicant shall modify the parking area to stripe a walkway at least six (6) feet wide across

the drive aisles and install and plant four landscaped islands with trees. The middle two islands shall each be at least fifty-two (52) square feet within inside of curbing with narrowest dimension of six-and-a-half (6½) feet minimum within inside of curbing. The trees shall conform to Table 3.06B. Additional or altered compact stalls shall be striped with “C”, “COMPACT”, or “COMPACT ONLY”.

- b. West rear staff: As proposed, per Sheets CU-A01 & 3 and L01 & 2, the applicant shall modify the parking area to stripe walkway crossings of the east drive aisle.

This condition is due by building permit final inspection for Phase 2.

C11. Signage: Based on WDO 3.10 and the needs of the Public Works Department:

- a. Permanent signage for the school campus shall not encroach within any PUE.
- b. Electronic changing image: Based on the hours in Ordinance No. 2338, Section 5A Light Trespass, any monument sign electronic changing image shall be off every evening or night during the hours of 9:00 p.m. to 7:00 a.m. Pacific time.
- c. The applicant shall through the sign permit process of 5.01.10 demonstrate that any new, relocated, or structurally altered permanent signage conforms to WDO 3.10.

C12. Discontinuance/Revocation: Because the WDO does not specify if and when a conditional use approval would expire were a use to cease, based on WDO 4.02.04B the approval shall expire if the use of public school ceases and three (3) years pass without the use recommencing.

C13. CU Modification: Because the WDO, including 5.03.01, does not specify how changes to an approved conditional use (CU) and related site improvements might trigger another CU or modification of a CU approval, for Director determination the following serve as criteria and – where noted – as factors:

- a. Significant expansion of the use, factors being an increase in any of: total gross floor area (GFA) by 25.0% or more or by an absolute value of ten thousand (10,000) square feet or more, the number of buildings (excluding modulars) by two (2) or more, the number of modular buildings by one (1) or more, and the number of classrooms by six (6) or more;
- b. Increase in off-street parking by twelve (12) or more stalls, even if the existing supply were in excess of the minimum required ratio(s);
- c. Net Increase in impervious surface totaling at least two-and-half (2½) acres;
- d. Development as defined in WDO 1.02 within twenty (20) feet of a property boundary and not already conditioned through the subject approval;
- e. Installation of additional or altered sports field lighting taller than twenty (20) feet and within twenty (20) feet of ROW or other property line;
- f. Any proposal necessitating a request for Exception to Street Right-of-Way and Improvement Requirements (“Street Exception”);
- g. Any proposal necessitating a request to vary from the WDO;
- h. Any proposal necessitating a Type III or IV land use application type;

- i. Any proposal increasing either student capacity or actual student population by ninety (90) or more students;
- j. Any proposal predicted based on the Institute of Transportation Engineers (ITE) *Trip Generation Manual* to increase peak hour vehicle trip generation such that per WDO 3.04.05A the City could require a traffic impact analysis (TIA);
- k. An application for Modification of Conditions pursuant to WDO 4.02.07;
- l. City adoption of a unified development ordinance replacing the WDO were to have intervened; and
- m. Proposal by the Woodburn School District for the City to approve a District-led long-range master plan for the Woodburn H.S. campus.

Modification of a specific condition of approval remains pursuant to WDO 4.02.07. Were the City to have amended the WDO to establish modification provisions for conditional uses, the Director may decide that the provisions supersede this condition of approval.

C14. Architecture: As proposed, the north outbuilding labeled as “classroom building” shall meet or exceed the window area percentage provision of WDO 3.07.06B.2b(1) on the north façade that faces Hazelnut Drive.

C15. Driveway: The additional driveway along Hazelnut Drive shall be aligned with a public street that intersects Hazelnut Drive and shall have a maximum width of either twenty-six (26) feet or, if later changed to have an outbound left-turn pocket between an inbound and an outbound lane, thirty (30) feet.

Design Review 2019-13

D1. Fences / fencing: To meet WDO 2.06.02C.1 (height maximum), 2.06.02D.2 (coating and slats), vision clearance area (3.03.06), and, the applicant shall through the fence permit process of 5.01.03 demonstrate the proposed fencing conforms.

- D2. ROWs & PUEs: To meet WDO 3.01, the applicant shall illustrate, note, and dedicate for:
- a. Hazelnut Drive, additional ROW of three (3) feet and a PUE of five (5) feet width;
 - b. N. Front Street, additional ROW of nineteen (19) feet and a PUE of five (5) feet width; and
 - c. the highway, additional ROW of five (5) feet and a PUE of ten (10) feet width.

D3. Screening of parking: To meet WDO 3.06.05B, the applicant shall screen surface parking in the north yard from Hazelnut Drive. The applicant shall revise Sheet CU-L02 to clarify that proposed shrubbery amid the row of trees would be evergreen shrubbery forming a screen hedge

or row as a vertical visual screen, growing to a minimum height at maturity of three-and-a-half (3½) feet from the abutting street grade.

D4. Screening of roof-mounted equipment: To meet WDO 3.07.06B.4 fully, the applicant shall revise the building elevation Sheets CU-A07 through 9 to clarify that all such equipment will be screened.

D5. On-site PUEs other than streetside ones: To meet WDO 3.02.01 & 3.02.02, were the City Engineer to identify later a nonconforming situation in need of correction regarding any on-site utilities and easements in order to protect public health, safety, and welfare, upon City Engineer written direction, the applicant shall record a document or documents that accomplish the directed correction.

Street Exception 2019-07

The street exception request is approved for the three frontages as modified by the following conditions:

EX1. ROWs & PUEs: Approval of the street exception request shall not exempt the proposal from dedications of required ROWs and PUEs.

EX2. Hazelnut Drive: The required improvements shall be per the land use review plan set submitted February 4, 2020, Sheets CU-A01 & 3 and L01.

EX3. N. Front Street: The required improvements shall be per the land use review plan set submitted February 4, 2020, Sheets CU-A01 & L01.

EX4. Highway: The required improvements shall be per the land use review plan set submitted February 4, 2020, Sheets CU-A01 & L01.

EX5. Street trees:

- a. Specifications: The applicant shall plant in conformance with WDO 3.06.03A and Tables 3.06B & C:
- b. Public Works Standard Specifications & Drawings: The applicant shall revise the site plans to reproduce [standard details](#) as Public Works directs and include [Drawing 1 Street Tree Planting New Construction](#) and [Detail No. 4150-8 Sidewalks](#).

EX6. ADA: The applicant shall upgrade to provide ADA compliance where it does not exist along sidewalks and pedestrian crossings of streets and do so by any of Phase 1 or 2.

EX7. Interpretation: Where there is uncertainty, contradiction, or conflict concerning what street improvements are or are not excepted, the Director shall determine upon guidance by the Public Works Department.


EX8. Future applications: Any future land use / planning / zoning application for the subject property that triggers street improvements supersedes this Street Exception 2019-07.

Phasing Plan 2019-03

PP1. Phasing:

- a. Number: The City acknowledges the phasing plan of two Phases 1 & 2.
- b. Improvements by phase:
 - i. Improvements are due by the phases with which they are associated as noted and delineated per the revised land use review sheets submitted February 4, 2020: CU-A01, 2, & 3 and CU-L02.
 - ii. Clarification: Though frontage/public/street improvements are deferred to Phase 2, the applicant shall record any and all required ROW, PUE, and Mill Creek Greenway Trail easement dedications prior to Phase 1 building permit final inspection.
- c. Timeframe: The City acknowledges the build-out timeframe per the applicant's letter of January 22, 2020 (p. 7) that was in response to the incompleteness letter of January 9, 2020: Phase 1 is through June 2021, and Phase 2 is through 2027, which through this condition staff interprets to mean through September 3, 2027. The applicant shall revise the site plans to indicate the phasing timeframe that the letter had indicated.

Applicant Identity

<i>Applicant</i>	 <p>Woodburn School District through Ivan Leigh, Operations Manager</p>
<i>Applicant's Representative</i>	<p>Deb France, Principal, Architect, Oh Planning+Design, Architecture; Caitlin McGehee, AIA, NCARB, Associate, Oh Planning+Design, Architecture</p>
<i>Landowner(s)</i>	<p>Woodburn School District 103</p>

Notes to the Applicant

The following are not planning / land use / zoning conditions of approval, but are notes for the applicant to be aware of and follow:

1. Records: Staff recommends that the applicant retain a copy of the approval of the conditional use.
2. Fences, fencing, & free-standing walls: The approval excludes any fences, fencing, & free-standing walls, which are subject to WDO 2.06 and the permit process of 5.01.03.
3. Signage: The approval excludes any signage, which is subject to WDO 3.10 and the permit process of 5.01.10.
4. Other Agencies: The applicant, not the City, is responsible for obtaining permits from any county, state and/or federal agencies, which may require approval or permit, and must obtain all applicable City and County permits for work prior to the start of work and that the work meets the satisfaction of the permit-issuing jurisdiction. The Oregon Department of Transportation (ODOT) might require highway access, storm drainage, and other right-of-way (ROW) permits. All work within the public ROW or easements within City jurisdiction must conform to plans approved by the Public Works Department and must comply with a Public Works Right-of-Way permit issued by said department. Marion County plumbing permits must be issued for all waterline, sanitary sewer, and storm sewer work installed beyond the Public Right-of-Way, on private property.
5. Inspection: The applicant shall construct, install, or plant all improvements, including landscaping, prior to City staff verification. At least three (3) City business days prior to a desired date of planning and zoning inspection of site improvements, the applicant shall

request a land use / planning / zoning inspection through the [Accela / Oregon ePermitting](#) building permit record(s) for the project. This inspection is required and separate from and in addition to the usual building code and fire and life safety inspections. Note that Planning staff are not primarily inspectors, do not have the nearly immediate availability of building inspectors, and are not bound by any building inspector's schedule or general contractor convenience. For help with ePermitting, call the Oregon Building Codes Division help line at (503) 373-7396.

6. Stormwater management: The storm sewer system and any required on-site detention for the development must comply with the City Storm Water Management Plan, Public Works storm water practices and the Storm Drainage Master Plan.
7. Public Works Review: Staff performs final review of the civil plans during the building permit stage. Public infrastructure must be constructed in accordance with plans approved by the City, as well as current [Public Works construction specifications, Standard Drawings, Standard Details](#), and General Conditions.
8. ROW:
 - a. Dedication: For a site development project that requires no and involves no re-plat, there is a process separate from land use / planning / zoning review to dedicate ROW and easements. Contact the Public Works Department to obtain paperwork and allow at least two months for Public Works staff to review the paperwork and schedule acceptance of dedications during a regularly scheduled City Council meeting.

ROW – and public utility easement (PUE) – dedications are due prior to building permit issuance.
 - b. Work: All work within the public ROWs or easements within City jurisdiction must require plan approval and permit issuance from the Public Works Department. All public improvements construction work must be performed in accordance with the plans stamped “approved” by the City, and comply with the City’s Standard Specifications and Standard drawings.
9. Franchises: The applicant provides for the installation of all franchised utilities and any required easements.
10. Water: All water mains and appurtenances must comply with Public Works, Building Division, and Woodburn Fire District requirements. Existing water services lines that are not going to be use with this new development must be abandoned at the main line. The City performs required abandonment of existing water facilities at the water main with payment by the property owner. All taps to existing water mains must be done by a “Hot Tap” method and by approved City of Woodburn Contractors. The applicant shall install the proper type of backflow preventer for all domestic, lawn irrigation and fire sprinkler services. The backflow devices and meters shall be located near the city water main within an easement, unless

approved otherwise by Public Works. Contact Byron Brooks, City of Woodburn Water Superintendent, for proper type and installation requirements of the backflow device at (503) 982-5380.

11. Wastewater Survey: The applicant must complete a City of Woodburn Nonresidential Wastewater Survey and comply with the conditions of the Wastewater Permit. Contact Carol Leimbach, Industrial Waste Coordinator, at (503) 982-5283.
12. Grease Interceptor/Trap: If applicable, a grease trap would need to be installed on the sanitary service, either as a central unit or in the communal kitchen/food preparation area. Contact Marion County Plumbing Department for permit and installation requirements, (503) 588-5147.
13. Fire: Fire protection requirements must comply with the Woodburn Fire District standards and requirements. Place fire hydrants within the public ROW or public utility easement and construct them in accordance with Public Works Department requirements, specifications, standards, and permit requirements. Fire protection access, fire hydrant locations and fire protection issues must comply with current fire codes and Woodburn Fire District standards. See City of Woodburn Standard Detail No. 5070-2 Fire Vault. The fire vault must be placed within the public right-of-way or public utility easement.
14. Street addressing: Were the District to desire or be prompted by a public agency to adjust street addresses, do so through the City. Assigning new addresses involves the City, the Fire District, and the U.S. Postal Service and needs completion prior to intake of building permits. For the Building Division to accept building permit applications for changed addresses, the applicant must have submitted an [Address Assignment Request](#) to the Community Development Department for and obtained adjusted street addresses with payment per the [Planning Division fee schedule](#). Expect 1-2 weeks for approval following application. The applicant prepares an exhibit with a site plan indicating the addresses as part of a street address application.
15. SDCs: The developer pays System Development Charges prior to building permit issuance. Staff will determine the water, sewer, storm and parks SDCs after the developer provides a complete Public Works Commercial/Industrial Development information sheet.



**Woodburn High School CTE & Classroom Addition,
CU2019-07,DR 2019-13, EXCP 2019-07, & VAR 2019-03
1785 N Front Street
Public Works Comments**

February 18, 2020

CONDITIONS OF LAND USE APPROVAL:

1. The Applicant, not the City, is responsible for obtaining permits from state, county and/or federal agencies that may require such permit or approval.
2. Final review of the Civil Plans will be done during the building permit application; a Professional Engineer shall stamp the plans. Public infrastructure will be constructed in accordance with plans approved by public works.
1. All work within the public rights-of-way or easement within city jurisdiction shall require plan approval and permit issuance from the Public Works Department. All work in the right-of-way or public utility easement shall be performed in accordance with plans stamped "Approved" by the City Public Works Department and in compliance with City's Standard Specifications and Standard Details and Drawings.
3. System Development fees shall be paid at the time of building permit issuance.

CONDITIONS TO BE ADDRESS DURING THE BUILDING PERMIT APPLICATION

1. All public utility easements and right-of-way dedications shall be properly recorded prior to building permit final inspection.
 - 5ft Public Utility Easement along Hazelnut Drive
 - 5ft Public Utility Easement along N Front Street
 - 10ft Public Utility Easement along Highway 214
 - 20ft easement along mill creek trails
 - 3ft right-of-way dedication along Hazelnut Drive
 - 19ft right-of-way dedication along N Front Street
 - 5ft right-of-way dedication along Highway 214
2. Street lighting shall comply with PGE and City of Woodburn Standards and Specifications.
3. Fire hydrants locations and fire protection requirements shall be as per the Woodburn Fire Districts and City of Woodburn requirements.
4. The applicant shall install the proper type of backflow preventer for all domestic, lawn irrigation and fire sprinkler services, RP devices are required if wells are located within the property. The backflow devices and meters shall be located near the city water main within an easement, unless approved otherwise by Public Works. Contact Byron Brooks, City of Woodburn Water Superintendent, for proper type and installation requirements of the

backflow device at 503-982-5380.

5. The applicant shall complete a City of Woodburn Nonresidential Wastewater Survey and comply with the conditions of the Wastewater Permit. Contact Carol Leimbach, City of Woodburn Industrial Waste Coordinator, at 503-982-5283.

Woodburn High School
**Career and Technical Education (CTE) &
Classroom Addition**

Woodburn, OR

An Application For:
Conditional Use Permit

Applicant:
Woodburn School District
1390 Meridian Dr.
Woodburn, OR 97071
503.981.9555

Prepared by:
Oh planning+design, architecture
115 NW 1st Ave., Suite 300
Portland, OR 97209
503.280.800

Rev. 1, 1/22/2020

TABLE OF CONTENTS

I. INTRODUCTION

GENERAL INFORMATION

EXISTING CONDIITIONS

SUMMARY OF WORK

PROJECT LIMITS AND SURROUNDING USES

II. CITY OF WOODBURN DEVELOPMENT ORDINANCE

SECTION 1 – NONCONFIRMING USES AND DEVELOPMENT

SECTION 2 – LAND USE ZONING AND STANDARDS

SECTION 3 – DEVELOPMENT GUIDELINES AND STANDARDS

SECTION 5 – APPLICATION REQUIREMENTS

III. EXHIBITS

TRAFFIC IMPACT LETTER

CU-A14, MILL CREEK ARIAL

I. INTRODUCTION

GENERAL INFORMATION

Applicant:	Woodburn School District 1390 Meridian Dr. Woodburn, OR 97071 Contact: Ivan Leigh E-mail: ileigh@woodburnsd.org Phone: 503.981.9555
Applicant's Representative:	Oh planning+design, architecture 115 NW 1 st Ave., Suite 300 Portland, OR 97209 Contact: Deb France, Principal E-mail: deb.france@ohpd.net Telephone: 503.280.800
Site Address:	Woodburn High School 1785 N. Front St. Woodburn, OR 97071
Tax Lot Information:	051W07A00600
Location:	1785 N. Front St. Woodburn, OR 97071
Zoning District:	P/SP - Public and Semi-Public
Project Site Area:	56.26 Acres

EXISTING CONDITIONS

In 2012, the Woodburn Academy of Arts, Science, and Technology (WAAST) lost approximately 11,900 square feet of classroom space due to a fire. The academy has since been housed throughout the rest of the high school building, within the other academies' zones, as well as in ten (10) portable classroom buildings on the site. This project will reconstruct the spaces lost during the fire and incorporate Career Technical Education (CTE) classroom space, makers shop, CADD Classrooms, and video production space. A separate building will include new classrooms to replace a portion of the existing portable classrooms. The project will be completed in two phases; the first phase will include the CTE and Classroom Buildings and minimal site improvements; the second phase of the Master Plan will include an addition to the classroom building, secure vestibules in the existing academies, and more extensive site improvements.



SUMMARY OF WORK

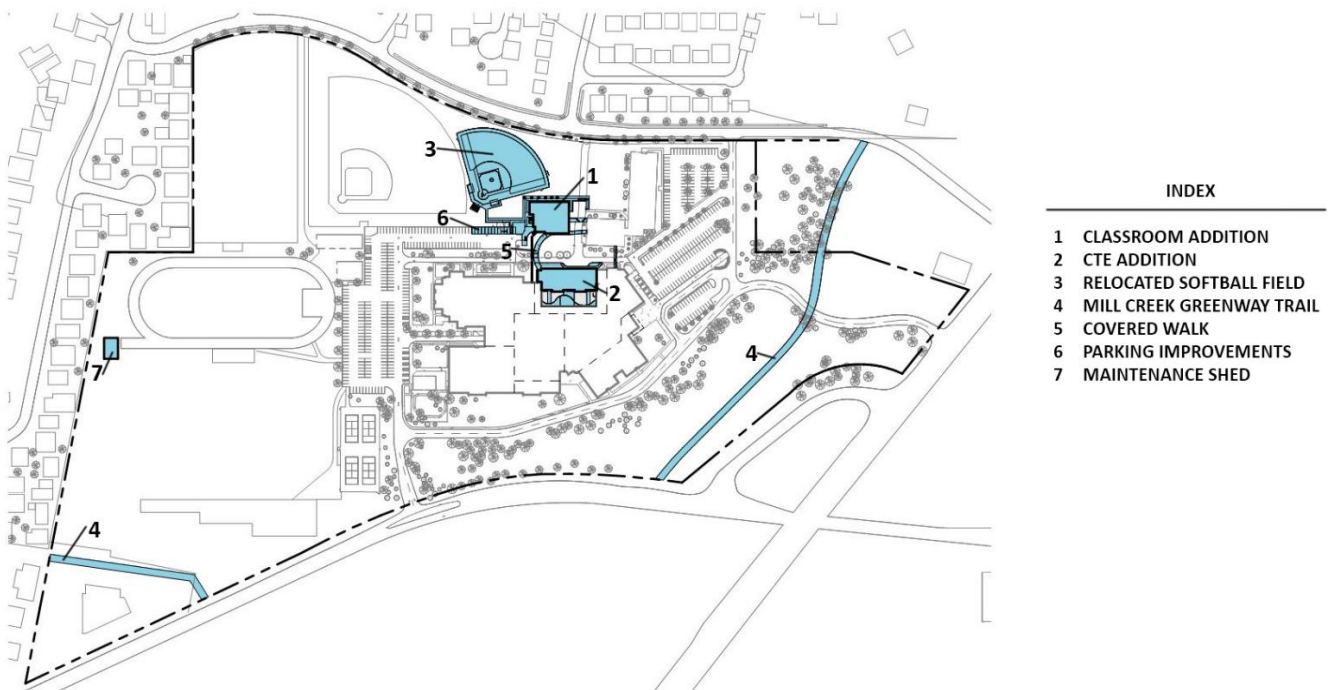
Master Plan – Phase 1

The Master Plan Phase 1 scope of work is the highest priority to Woodburn School District and is targeted for construction in Spring 2020. This scope will provide two new main components: first, an infill building at the existing courtyard for Career and Technical Educational (CTE) programs; second, a new, standalone classroom building to replace the 10 portables that were provided after the fire. Those portables will be removed after the new classroom building is completed.

A covered walkway will also be constructed to connect the CTE addition, the New Classroom Building, and the existing AIS building. The walkway will bridge over fire access lanes and provide a minimum of 14'-0" vertical clearance. Structural supports and footings will be sized and located so they are not within the required fire lane width or existing utility easements running through the site.

Both buildings will be steel-framed, type V-B Construction, fully sprinklered. The classroom building is freestanding, located approximately 48'-0" from the nearest structure. The CTE building will be connected to the existing high school by a new, fire-rated door on the east end. The west end of the building will be connected to the existing corridor, with a new opening. The total combined area of the existing, fire-separated building zone, and the new addition, will be 23,060 square feet, which is well under the allowable area of 38,000 SF for Type V-B construction.

Site preparation for this scope will include relocation of an existing greenhouse, maintenance shed, and softball field, and the temporary relocation of the portable restroom unit. Two (2) portables will be removed at the start of the project, and the remaining eight (8) will be removed from the site once the new Classroom Building is completed.



PHASE 1 SCOPE DIAGRAM – NOT TO SCALE

Master Plan – Phase 2

The Master Plan will be completed in Phase 2 and will consist primarily of sitework improvements, secure vestibule renovations, and an expansion to the new Classroom Building. The site scope of work will include a new bus drop off on south Bulldog drive, a new learning plaza between the CTE and Classroom buildings, and landscape improvements.

The plaza between the CTE and Classroom Buildings will involve reconfiguring a section of North Bulldog Drive, widening the drive to meet current Fire Code Requirements. The driveway to the east of the Classroom Building, connected to Hazelnut Drive, will be removed and replaced with a 96' diameter turnaround for fire access to the north side of the classroom building.

The new Classroom Building will be expanded by adding an additional 10,000 square feet of classroom space. Secure vestibules will be added to the existing entries at the Academy of International Studies (AIS) and the Wellness, Business, and Sports School (WeBSS).

Renovation to interior portions of the existing High School Building will also take place as part of the phase 2 portion of the master plan. This will include reconfigurations to existing classrooms, making more efficient use of the spaces to better meet the needs of the academies. This work will have no impact on the exterior of the building.

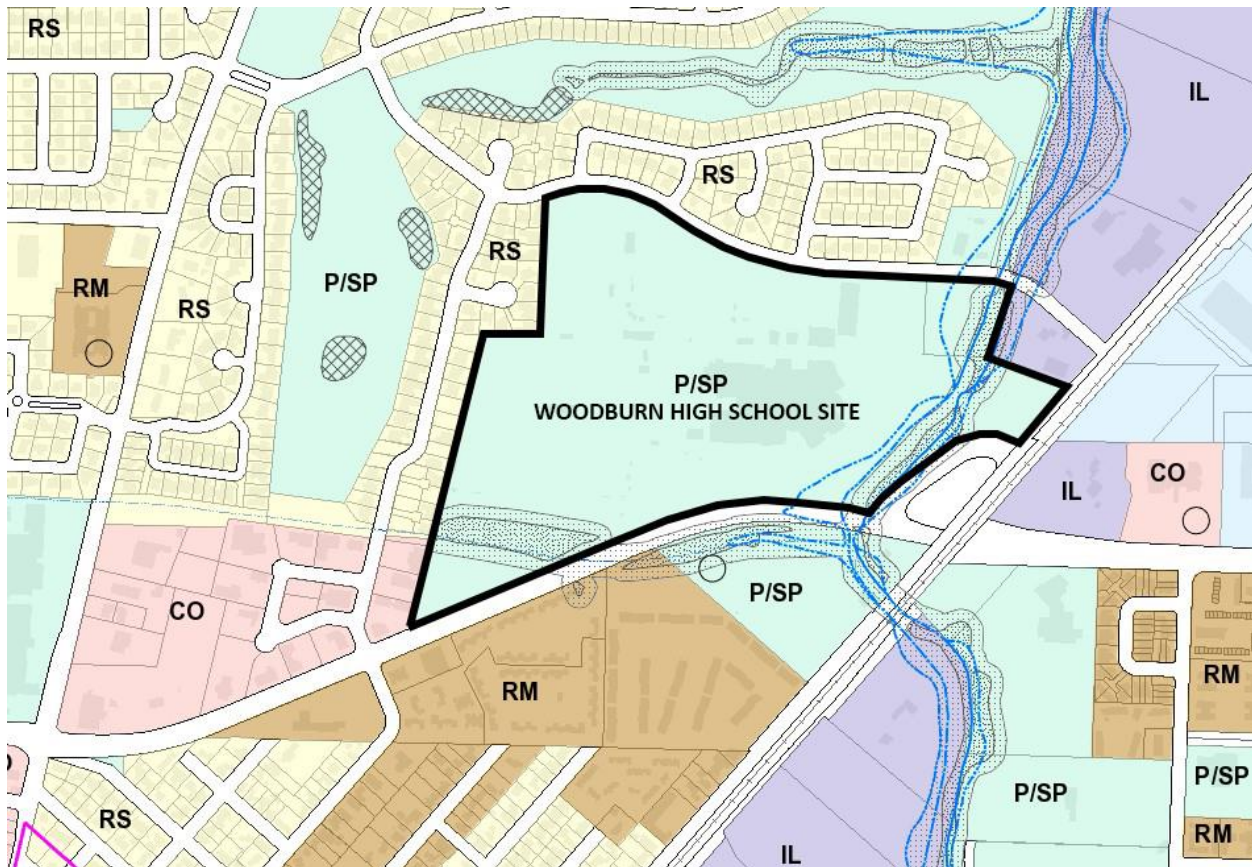


PHASE 2 SCOPE DIAGRAM – NOT TO SCALE

PROJECT LIMITS AND SURROUNDING USES

The following table identifies the project zoning designation and land uses of the surrounding properties.

LOCATION	ZONING DESIGNATION	LAND USE
North	RS, P/SP	Single Family Residential, Public/Semi-Public
East	IL, IP	Light Industrial, Industrial Park
South	RM, P/SP	Medium Density Residential, Public/Semi-Public
West	CO, RS	Commercial Office, Single Family Residential



ZONING MAP – NOT TO SCALE



VICINITY MAP – NOT TO SCALE

II. CITY OF WOODBURN DEVELOPMENT ORDINANCE

The applicable City of Woodburn Development Ordinance (WDO) sections are outlined in the following sections.

WDO SECTION 1 – ORGANIZATION AND STRUCTURE

1.04.02 – NONCONFORMING USES AND DEVELOPMENT

Change or Expansion of an Existing Use with Nonconforming Parking, Loading and/or Landscaping

Any additional parking, loading, landscaping, wall or refuse facility required by the WDO to accommodate a change in use, or expansion of an existing use shall be subject to the following:

- a. For applications where the change or expansion increases the required area for parking, loading, or landscaping by 25 percent or more, all parking, loading, landscaping, buffer walls and refuse facilities shall conform to the standards of the WDO.

- b. For applications where the change or expansion increases the required area for parking, loading, or landscaping by less than 25 percent, the parking, loading, landscaping, buffer walls and refuse facilities required for the expansion shall conform to the standards of the WDO. The property owner is encouraged, but not required, to bring more of the site into conformity.

RESPONSE:

Phase 1: *The Phase 1 scope of work will occupy a total area of 113,256 SF (2.6 acres). The scope of work is only 4.6% of the total 2,450,686 SF (56.26 acres) that make up Woodburn High School property.*

Parking: The site currently has 470 parking spaces. 21 spaces will be added in Phase 1. This is an increase of approximately 4.5%, which is well below the 25% threshold.

Loading: No new loading zones are provided, so the change in required area is 0%.

Landscaping: Landscape scope of work will occupy a total area of 61,432 SF (1.4 acres). This area is only 2.5 % of the total WHS property (56.26 acres).

With the changes to required areas for parking, loading, and landscaping being less than 25% of the existing areas, the Phase 1 is subject to WDO 1.04.02.B. All improvements within the area of work to follow the WDO development standards.

Phase 2: *The scope of work will occupy a total area of 191,664 SF (4.4 acres). This area is only 7.8% of the total 2,450,686 SF (56.26 acres) that make up Woodburn High School property.*

Parking: The site currently has 470 parking spaces. The Phase 2 scope of work will result in a net add of 7 spaces. This is an increase of approximately 1.5%, which is well below the 25% threshold.

Loading: No new loading zones are provided, so the change in required area is 0%.

Landscaping: Landscape scope of work will occupy a total area of 50,161 SF (1.2 acres). This area is only 2% of the total WHS property (56.26 acres).

With the changes to required areas for parking, loading, and landscaping being less than 25% of the existing areas, the Phase 1 is subject to WDO 1.04.02.B. All improvements within the area of work to follow the WDO development standards.

WDO SECTION 2 – LAND USE ZONING AND SPECIFIED USE STANDARDS

SECTION 2.04A – INDUSTRIAL AND PUBLIC ZONES

The project site is classified Public and Semi-Public (P/SP) zone, which is intended for public uses, parks, schools and cemeteries.

RESPONSE:

The CTE and Classroom Addition is an educational facility and is an approved use within the Public and Semi-Public (P/SP) zone.

SECTION 2.04D – PUBLIC/SEMI-PUBLIC (P/SP)

WDO 2.04 D outlines the Public/Semi-public development standards for the project site.

Public/Semi-Public (P/SP) - Site Development Standards Table 2.04D			
Lot Area, Minimum		No minimum	
Lot Width, Minimum		No minimum	
Lot Depth, Minimum		No minimum	
Street Frontage, Minimum		No minimum	
Front Setback and Setback Abutting a Street, Minimum (feet)		20 ¹	
Side or Rear Setback, Minimum (feet)	Abutting P/SP zone or a residential zone or use		20
	Abutting a commercial or industrial zone		0 or 5 ²
Setback to a Private Access Easement, Minimum (feet)		5	
Lot Coverage, Maximum		Not specified ³	
Building Height, Maximum (feet)	Primary or accessory structure	Outside Gateway subarea	35
		Gateway subarea	50
	Features not used for habitation		No minimum

WDO Table 2.04 D

RESPONSE:

Both Phase 1 and 2 of the Master Plan will meet the requirements listed within the P/SP site development standards above. See the proposed setbacks for each phase listed below. Please note, the setback dimensions are to the closest structure; existing or new. Building height is measured to the top of roof parapet, mechanical screening is not included.

Phase 1:

LOCATION	SETBACK	STRUCTURE
North	32'	Existing AIS Academy
East	280'	Existing AIS Academy
South	219'	Existing HS Main Building
West	34'	Relocated Maintenance Shed

BUILDING	MAX. HEIGHT
CTE	20'
Classroom Addition	30'

Phase 2:

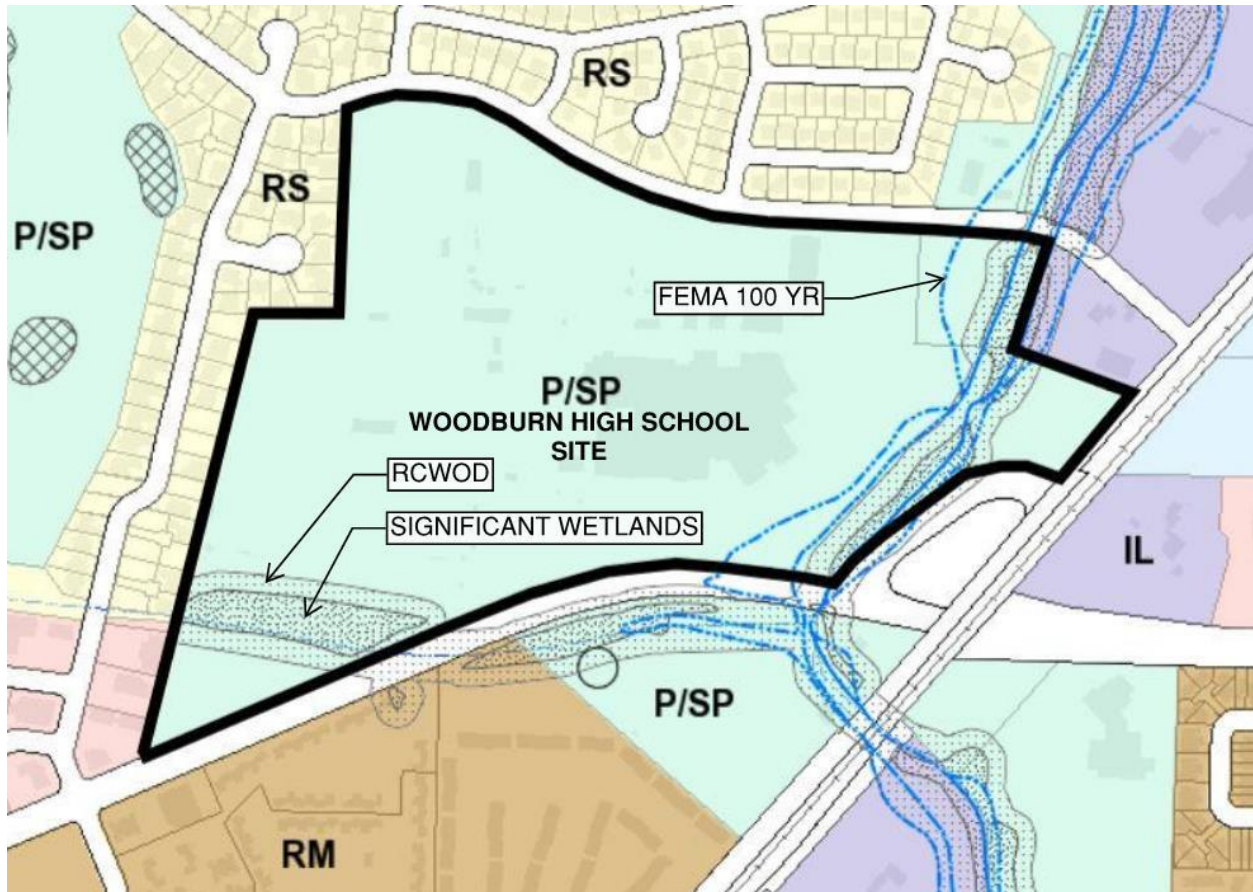
LOCATION	SETBACK	STRUCTURE
North	32'	Existing AIS Academy
East	280'	Existing AIS Academy
South	219'	Existing HS Main Building
West	34'	Relocated Maintenance Shed

BUILDING	MAX. HEIGHT
Classroom Addition	30'

SECTION 2.05 – OVERLAY DISTRICTS

WDO 2.05.05 – RIPARIAN CORRIDOR AND WETLANDS OVERLAY DISTRICT

The Riparian Corridor and Wetlands Overlay District (RCWOD) is intended to conserve protect and enhance significant riparian corridors, wetlands, and undeveloped floodplains in keeping with the goals and policies of the Comprehensive Plan.



WETLANDS MAP – NOT TO SCALE

RESPONSE:

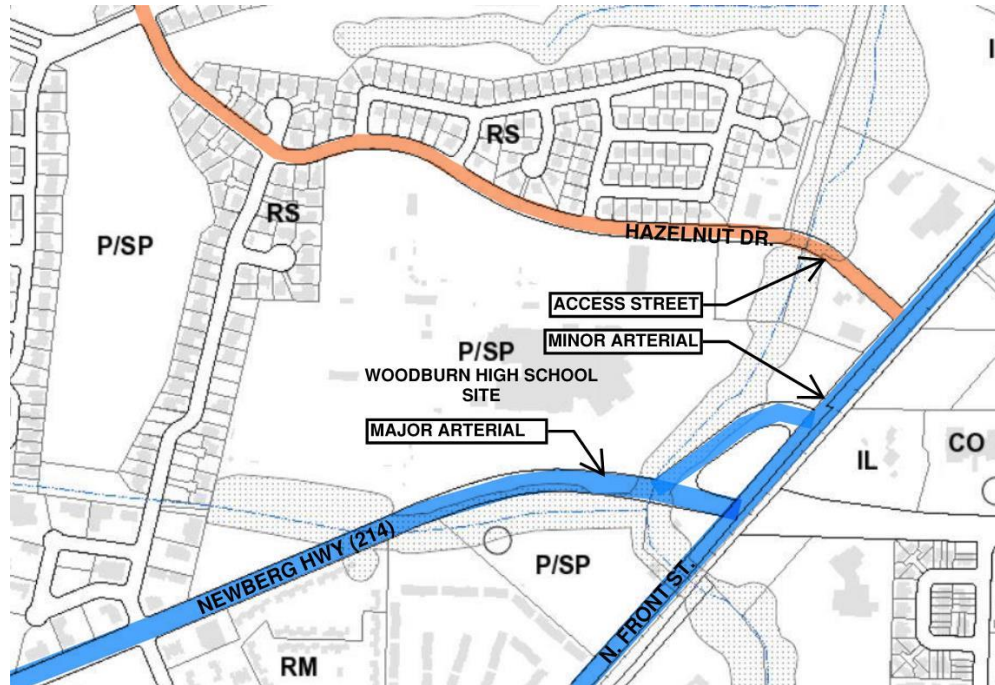
For both Phase 1 and 2 of the Master Plan, the significant wetlands and RCWOD will be protected during all construction activities of the CTE and Classroom Addition. Erosion and sediment control will be in place to protect the wetlands, and no additional stormwater runoff flows from the project will impact the wetlands.

Mill Creek Greenway:

Per 2.05.05, Phase 1 will dedicate a 20'-0" public easement within the RCWOD for the Mill Creek Greenway, per Mill Creek Greenway Master Plan standards. Please reference the exhibit CU-A14 Mill Creek Aerial for the placement of the greenway. The location of the greenway is per the City's courtesy review comments dated 11/25/19.

WDO SECTION 3 – DEVELOPMENT GUIDELINES AND STANDARDS

SECTION 3.01 – STREET IMPROVEMENTS



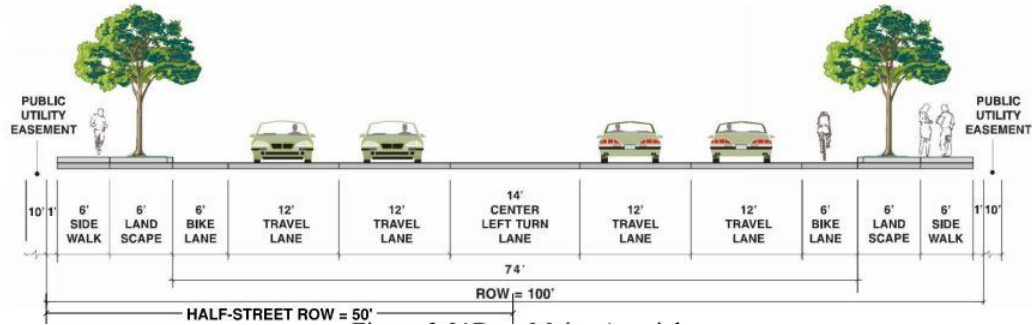
PERIMETER STREET DIAGRAM – NOT TO SCALE

RESPONSE: *The property abuts Newberg Hwy (OR 214), N. Front St., and Hazelnut Drive. The right-of-way (ROW) and public utility easements (PUE) will be adjusted to provide the required ROW and PUE per the WDO. Please reference CU-A01 for the proposed street improvements.*

Master Plan:

1. *Newberg Highway/OR 214*

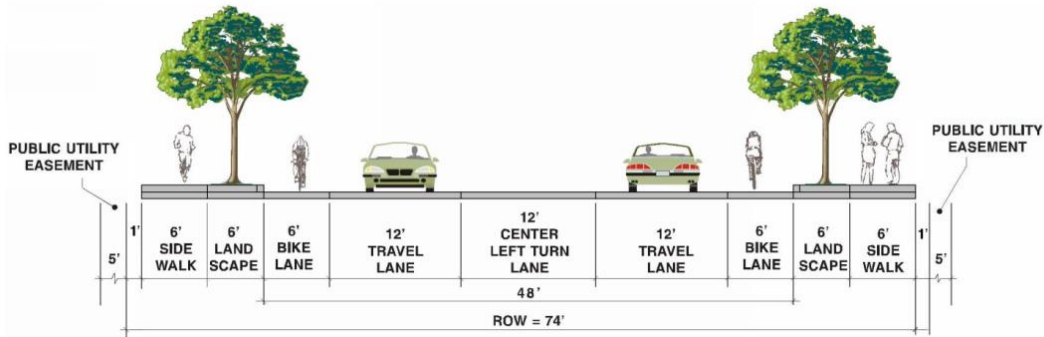
The existing Newberg Hwy. ROW is 90'-0", 10'-0" less than the WDO standard or 100'-0". The existing ROW to be increased by 5'-0" to meet the half-street ROW deficit per WDO standards for Major Arterial. A 10'-0" PUE will be placed within the 20'-0" building setback.



MAJOR ARTERIAL – WDO FIG. 3.01B

2. N. Front St.

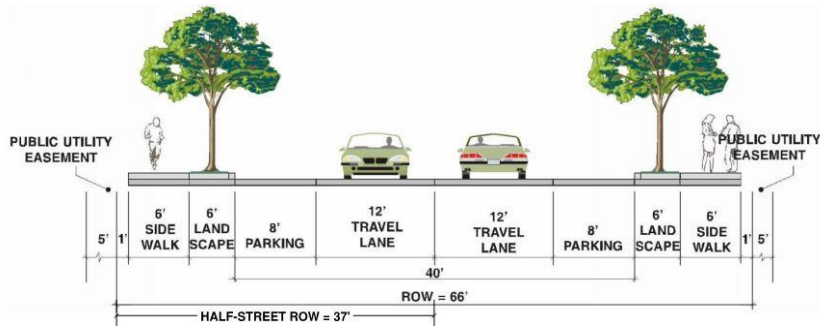
The existing N. Front St. ROW is 55'-0", 19'-0" less than the WDO standard of 74'-0". Per the City's courtesy review comments, the District will pursue a street exception to maintain the existing street improvements at N. Front St. Per request of the City, the existing ROW at N. Front St. will be increased by 19'-0" to meet the WDO standards for Minor Arterial. Due to the railroad on the east side of Front St., the entire 19'-0" deficit must be dedicated on the High School property. A 5'-0" PUE will be placed within the 20'-0" building setback.



MINOR ARTERIAL – WDO FIG. 3.01C

3. Hazelnut Dr.

The existing Hazelnut Dr. ROW is 60'-0", 6'-0" less than the WDO standard of 66'-0". The existing ROW to be increased by 3'-0" to meet the half-street ROW deficit per WDO standards for Access Street. Per City comments, a 5'-0" PUE will be placed within the 20'-0" building setback. The Hazelnut ROW and PUE improvements will extend to the District's property to the east, Lot 051W08BC00200.



ACCESS STREET – WDO FIG. 3.01E

SECTION 3.02 – UTILITIES AND EASEMENTS

The purpose of this section is to ensure that adequate easements for public utilities and drainage are provided for all developments, to establish standards for street lighting, and to require that new developments be served with underground utilities.

RESPONSE:

For both Phase 1 and 2 of the Master Plan, all public utilities are available to the City and shall be located within public utility easements. Please reference CU-C01 for the Phase 1 utility plans.

Exterior Lighting: Please reference CU-E01 for the site lighting photometric plan, fixture schedule and fixture placement. All site lighting to be full cut-off or fully shielded. All wall mounted fixtures to be placed 8'-0" maximum above finish grade. All pole mounted fixtures to be 14'-0" maximum above finish grade. No site lighting to be provided at the softball field improvements as games take place during daylight hours only.

SECTION 3.04.05 – TRAFFIC IMPACT ANALYSIS

A Traffic Impact Analysis (TIA) may be required by the Director prior to the approval of a City access permit when the Director estimates a development proposal may generate either 100 or more additional peak hour trips, or 1,000 or more additional daily trips, within ten years of a development application.

RESPONSE:

A traffic analysis is not required, because the capacity of the High School is not increasing. Please see the following outline of the existing and new High School capacity.

Prior to the 2012 fire, the existing High School had a capacity of 1,980 students. In 2012, the High School lost approximately 11,900 sq. ft. of program space due to a fire and included six (6) classrooms, which reduced the existing capacity by approximately 210 students. Resulting in an overall capacity of 1,770.

In 2012, the District added 10 portables to address the reduced classroom program. Each portable accommodates two (2) classrooms at 30 students each, or 60 students total. The 10 portables provide an increased capacity of 600 students, or 2,370 overall capacity.

The portables will be removed from the site once construction of Phase 1 CTE and Classroom Addition is complete. The program for the CTE and Classroom Addition is intended to replace a portion of the existing 10 portables by adding eight (8) new classrooms at the Classroom Addition and two (2) new classrooms at the CTE. Each classroom will accommodate 32 students, equaling a capacity of 320. The Wood/Metal Shop and Media Production Lab are shared academy programs and will not add capacity to the existing High School program. Following the completion of the Phase 1, the overall capacity of the High School will be 2,050; which is well below the 2012 capacity.

The Phase 2 Classroom Addition will add an additional capacity of 192 students; six (6) classrooms at 32 students each. The increase will result in an overall capacity of 2,242, which is 54 less than the 2012 capacity at 2,370.

SECTION 3.05 – OFF-STREET PARKING AND LOADING

Per Table 3.05A, the off-street parking ratio standards for high schools is one (1) parking stall per six (6) students. Please see the tables below for the parking calculations for each phase.

RESPONSE:

Per WDO 3.05.02, all new parking stalls to have wheel barriers and double parallel lines on each side. Directional markings and/or signs to be provided at areas of work to control vehicle movement.

Per WDO 3.05.03.E, Bicycle Parking: The project is to provide bicycle parking at a rate of 1 space per 10 vehicle spaces, with a maximum of 20 spaces required. The existing High School facility has 24 spaces, so this requirement is met, and no additional spaces are required. However, per request from the City, and as conditioned for the recently completed Elementary School projects, the bicycle count shall be 2 spaces per classroom. The existing High School has 65 classrooms. This means that a total of 130 bicycle spaces are required, or an additional 106 spaces. This increase was not anticipated per code requirements and is not cost-feasible for this project in Phase 1. To meet this request, the condition of 1 space per 2 classrooms will be applied to the new construction for Phase 1 and 2. For Phase 1, the scope of work adds 9 classrooms, thus 18 new bike spaces will be provided. (Note: the CTE CADD Lab, Media Room, and Shop are shared spaces that do not increase student capacity so are excluded from the classroom count). To help balance the site, 10 of these spaces will be located within 50' of the new Classroom Building main entrance, and 8 will be located within 50' of the existing High School main entrance. Two (2) existing clusters of racks are located within 50' of secondary entrances, providing a total of four (4) bicycle parking areas. Phase 2 of this project will add 6 more classrooms; and therefore, 12 more bicycle spaces.

All bicycle spaces will be 2' wide by 6' long, with a vertical clearance of 8'. A 5' deep maneuvering.

Per WDO 3.05.04, no new loading facilities will be added to the project. All deliveries, trash pickup, and other related activities will remain centralized at the existing High School Building.

Phase 1:

PARKING STALL TYPE	REQUIRED	PROPOSED
REGULAR	342	439
ADA	9	20
COMPACT	20% Max.	30
CARPOOL	2	2
TOTAL	353	491

BIKE PARKING	20	42
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The Classroom and CTE Addition will have a capacity of 320 students, combined with the existing WHS building (1,770 students) the total student capacity for Phase 1 will be 2,050 students. Per WDO Table 3.05A, a minimum of 342 parking stalls are required. The Phase 1 parking count (491) exceeds this requirement by 149 stalls.

Based on the Phase 1 parking count and WDO Table 3.05B, a minimum of 9 Accessible parking stalls and 2 "Wheelchair User Only" spaces are required. Both requirements are met in the Phase 1 Parking Count.

Per the Comprehensive Plan Goal H-1 at least two (2) carpool/vanpool stalls located convenient to staff entrances are required. Two (2) carpool/vanpool stalls have been added during Phase 1. The spaces will be striped with 1' tall lettering stating "CARPOOL/VANPOOL ONLY". Pole mounted signage will be provided at each space that is 1' wide by 1-1/2' tall and 6-1/2' above the vehicular grade.

Per WDO 3.05.03E, a maximum of 20 bike spaces are required for the existing high school, and per request from the City, 18 spaces are requested for the new addition. A total of 24 bike spaces are provided at the existing Woodburn High School. 18 more bike stalls will be added.

Phase 2:

PARKING STALL TYPE	REQUIRED	PROPOSED
REGULAR	374	390
ADA	9	20
COMPACT	20% Max.	65
CARPOOL	2	2
TOTAL	385	477

BIKE PARKING	20	54
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The phase 2 Classroom Addition will have a capacity of 192 students. Combined with phase 1 and existing WHS building (2,050 students), the total student capacity for Phase 2 will be 2,242 students. Based on WDO Table 3.05A, a minimum of 374 parking stalls are required. The Phase 2 parking count (477) exceeds this requirement by 87 stalls.

Based on the Phase 2 parking count and WDO Table 3.05B, a minimum of 9 Accessible spaces and two (2) "Wheelchair User Only" spaces are required. Both requirements are met in the Phase 2 Parking Count.

Per the Comprehensive Plan Goal H-1 at least two (2) carpool/vanpool stalls located convenient to staff entrances is required. Two (2) carpool/vanpool stalls have been designated by the Classroom Addition for Phase 2.

Per WDO 3.05.03E, a maximum of 20 bike spaces are required for the existing high school, and per request from the City, 12 spaces are requested for the Phase 2 addition. A total of 24 bike spaces are provided at the existing Woodburn High School. 12 more bike stalls will be added in Phase 2, in addition to the 18 new spaces included in Phase 1.

WDO SECTION 5 – APPLICATION REQUIREMENTS

SECTION 5.03.01 – TYPE III (QUASI-JUDICIAL) DECISIONS

Per the Conditional Use Pre-Application on 10/23/19, the City's Planning Division provided direction that the project shall pursue a Type III decision and will require a Conditional Use (CU), Design Review (DR), and Phasing Plan (PP).

- A. **PURPOSE:** A conditional use is an activity which is permitted in a zone but which, because of some characteristics, is not entirely compatible with other uses allowed in the zone and cannot be permitted outright. A public hearing is held by the Planning Commission and conditions may be imposed to offset impacts and make the use as compatible as practical with surrounding uses. Conditions can also be imposed to make the use conform to the requirements of this Ordinance and with other applicable criteria and standards. Conditions that decrease the minimum standards of a development standard require variance approval.

B. CRITERIA:

1. The proposed use shall be permitted as a conditional use within the zoning district.

RESPONSE:

Per Table 2.04A, public schools are a conditional use within the Public/Semi-Public Zones.

2. The proposed use shall comply with the development standards of the zoning district

RESPONSE:

All of the applicable development standards are to be met within Phase 1 and the Master Plan. Please reference section 2.04D of the narrative for the dimensions to the nearest structure; new or existing. Section 3.01 details the revisions to the property setbacks to align with the special setback provisions.

3. The proposed use shall be compatible with the surrounding properties.
 - a. The suitability of the size, shape, location and topography of the site for the proposed use.

RESPONSE:

The size of the site is expansive, at just over 56 acres, which is more than adequate for the proposed development. The area proposed for development in Phase 1 is only 2.6 acres, which includes the new classroom building, CTE infill building, and softball field relocation. The overall area for Phase 2 is 4.4 acres, and includes the additional expansion to the classroom building, site improvements around the new buildings, and improvements to the existing parking lots. The new buildings that are part of the development are located 195' from Hazelnut Dr, which is a substantial increase to the setback from the existing portables on the site that will be removed. The development location takes advantage of proximity to the existing facilities at the high school, including adequate parking and circulation facilities. The proposed development area is relatively flat, with only minor, localized re-grading required. Therefore, the use and development proposed are compatible with surrounding properties considering the suitability of the site.

- b. The capacity of public water, sewerage, drainage, street and pedestrian facilities serving the proposed use.

RESPONSE:

The development is a replacement of existing portables and a re-building of the portion of the existing high school that was destroyed by fire. The net increase in building area is minimal, so the added demand

on public utilities will be minor. The site is currently adequately served by water, sewer, and transportation facilities. The capacity of these utilities to serve the site is understood to be adequate but will be reviewed and approved by City staff as part of this applications. No substantial new facilities will need to be constructed as part of this development. Adequate drainage will be provided though the existing on-site storm water system, with eventual outfall into the storm water detention facility. The site is currently served by Hazelnut Drive to the north, N. Front Street to the east, and State Highway 214 to the south. The ROW on these streets will be revised as part of this development to meet current requirements of the WDO. As part of Phase 2, the existing driveway off Hazelnut Drive will be reconfigured to align with Graystone Drive, and a new crosswalk and ADA compliant curb cuts will be added. Additionally, a 20'-0" wide easement will be established to allow for the future development of the Mill Creek Trail, allowing eventual pedestrian movement through the site.

Accessible routes will be improved in the area of work for the Phase 1, including a new accessible path connecting the existing High School to the Classroom Addition and AIS Academy, as well as the public right-of-way. For Phase 2, improvements will be made to repair deficiencies at the existing pedestrian routes to provide accessible pathways per 2009 ICC A117.1 standard.

- c. The impact of the proposed use on the quality of the living environment.

RESPONSE:

The levels of noise associated with the school will mostly be associated with normal, passive student and teacher activities. Normal hours of operation will be between 7am to 6pm, with occasional evening or weekend meeting or event. A new dust collector and metal collector for the CTE shop space will be installed in the new courtyard. To mitigate sound disturbances to students and neighbors, these pieces of equipment will be located inside an acoustical enclosure with high sound transmission class (STC) rating. In Phase 1, a new courtyard space will be provided for outdoor teaching opportunities. The space is surrounded by the existing high school building and the new CTE addition and is located far from adjacent properties. Phase 2 will provide additional outdoor learning plazas. Sound disturbances from these spaces will occur only during school hours and will be shielded from neighboring properties by the new classroom building and landscaping. The softball field that is being relocated in Phase 1 is a replacement of an existing field, so there will be no new sound impacts. The field will not have lighting, so will be used during daylight hours only. All on-site lighting will include illumination at levels below the Code-required foot candle

requirement and will be full cutoff. No air emissions are anticipated beyond those associated with standard vehicle, bus, and delivery traffic. There is no anticipated increase in traffic on the site. The bulk, height, and design of the proposed buildings will follow the WDO Architectural Design Guidelines and Standards and will be compatible with the residential developments adjacent to the site.

SECTION 5.03.03 – EXCEPTION TO STREET RIGHT OF WAY AND IMPROVEMENT REQUIREMENTS

Per the Conditional Use Courtesy Review 1 on 11/25/19, the City's Planning Division provided direction that the Staff will support an Exception to Street Right of Way and Improvement Requirements on Front Street.

- A. PURPOSE: The purpose of a Type III Exception is to allow a deviation from the development standard required for the functional classification of the street identified in the Transportation System Plan. Street exceptions are processed in conjunction with a development proposal that is a Type III application.
- C. PROPORTIONATE REDUCTION IN STANDARDS: An exception to reduce a street right of way or cross section requirements below the functional classification standard may be approved when a lesser standard is justified based on the nature and extent of the impacts of the proposed development. No exception may be granted from applicable construction specifications.

RESPONSE: A Reduction in Standards Exception for the portion of Front street along the High School property is being sought. The existing Front Street Right of Way is 55'-0" wide, which is a deficit of 19'-0" from the required 74'-0" ROW for Minor Arterials. Due to the existing railroad tracks on the East side of Front Street, the entire Deficit must be dedicated on the High School Property. The exception request is to maintain the street improvements and sidewalks as they exist today. The overall street ROW of 74'-0" will be dedicated on the High School property. All new landscaping in Phase II along Front St. will be located outside the widened ROW to allow for potential street improvements to occur at a future date, separate from this project. The curb cuts at the Bulldog Dr. entrance will be improved to current ADA standards in Phase II of this project.

The section of the High School Property along Front St. is approximately 310' long. This represents only 6% of the +/- 5,200' of Front St. between Hwy. 214 to the south and the Woodburn City Limits to the North. This remainder of Front St. would remain unimproved. Additionally, the cost to relocate the sidewalks and other improvements would be prohibitive to the project, and the connections to adjacent properties would be problematic.

SECTION 5.03.05 – PHASING PLAN FOR SUBDIVISION, PUD, MANUFACTURED DWELLING PARK OR ANY OTHER LAND USE PERMIT

- A. PURPOSE: The Purpose of a Type III Phasing Permit is to allow phased construction of development while meeting the standards of this ordinance (Sections 2 and 3), while providing fully functional phases that develop in compliance with the tentative approval for the development.
- B. Criteria: The proposed phasing of development shall:
- a. Ensure that individual phases will be properly coordinated with each other and can be designed to meet City development standards; and

RESPONSE: See detailed description of phasing of work in the Summary section of this document. The work for Phase II has been planned in conjunction with Phase I so as to not conflict with each other. All work in both phases of the project have been designed to comply with the City development standards.

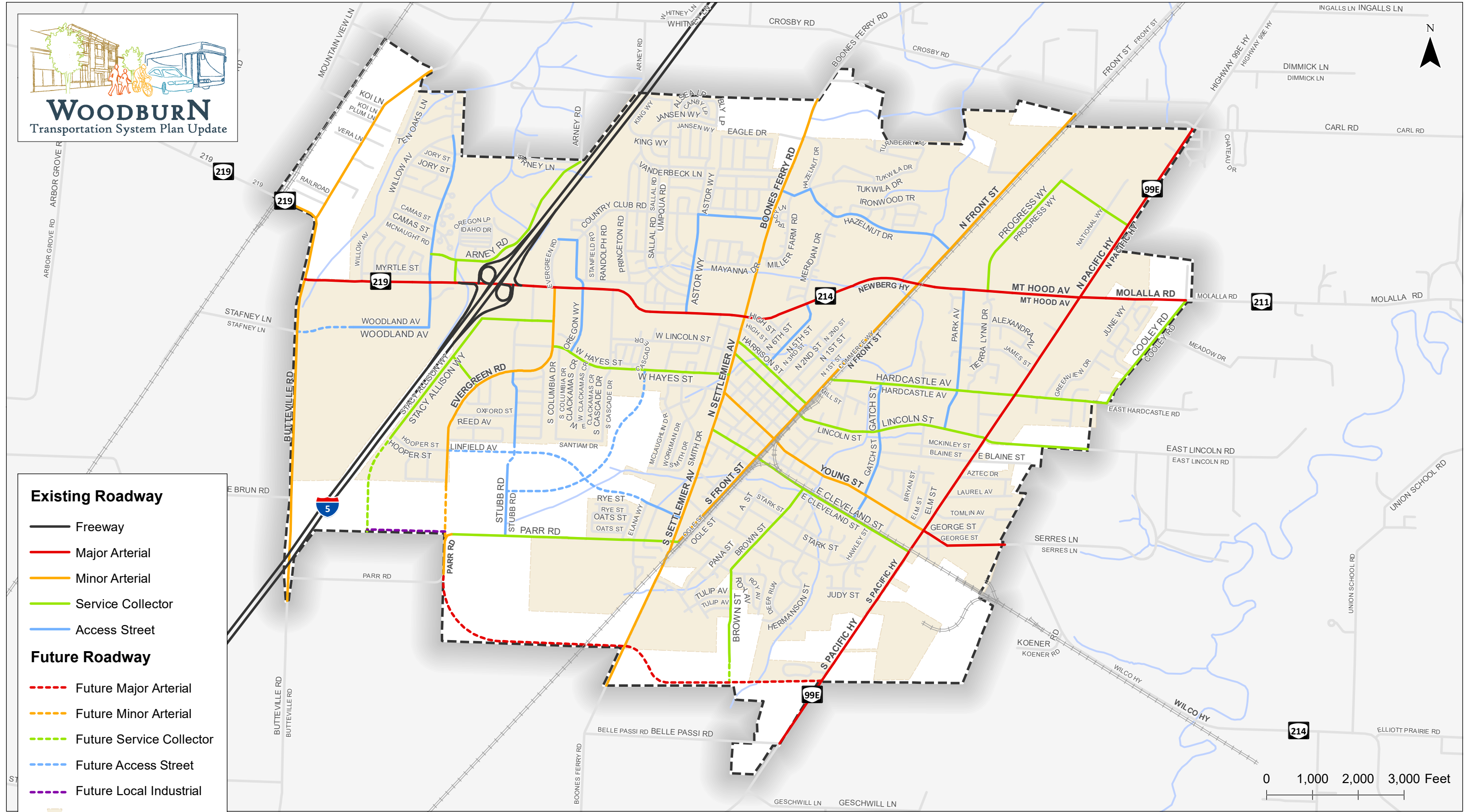
- b. Ensure that the phases do not unreasonably impede future development of adjacent undeveloped properties;

RESPONSE: All adjacent properties are currently developed. The only exceptions are segments of property along Mill creek, which are Significant Wetlands and Riparian Corridor & Wetlands Overlay District (RCWOD). A lot east of Front street is also undeveloped, but access to this site must be from Progress Way to the east, due to the Railroad tracks.

- c. Ensure that access, circulation, and public utilities are sized for future development of the remainder of the site and adjacent undeveloped areas.

RESPONSE: The new and improved utilities on the site have been sized to accommodate the Phase I and Phase II additions to the High School.

The District anticipates Phase 2 to be targeted within 5-7 years; however, the improvements are dependent on future City developments. The Phase 2 improvements are intended to meet the future student growth expected within the District, which is contingent on future City development.



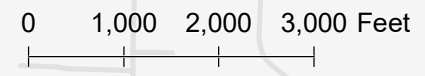
Existing Roadway

- Freeway
- Major Arterial
- Minor Arterial
- Service Collector
- Access Street

Future Roadway

- - - Future Major Arterial
- - - Future Minor Arterial
- - - Future Service Collector
- - - Future Access Street
- - - Future Local Industrial

City Boundary
 Urban Growth Boundary

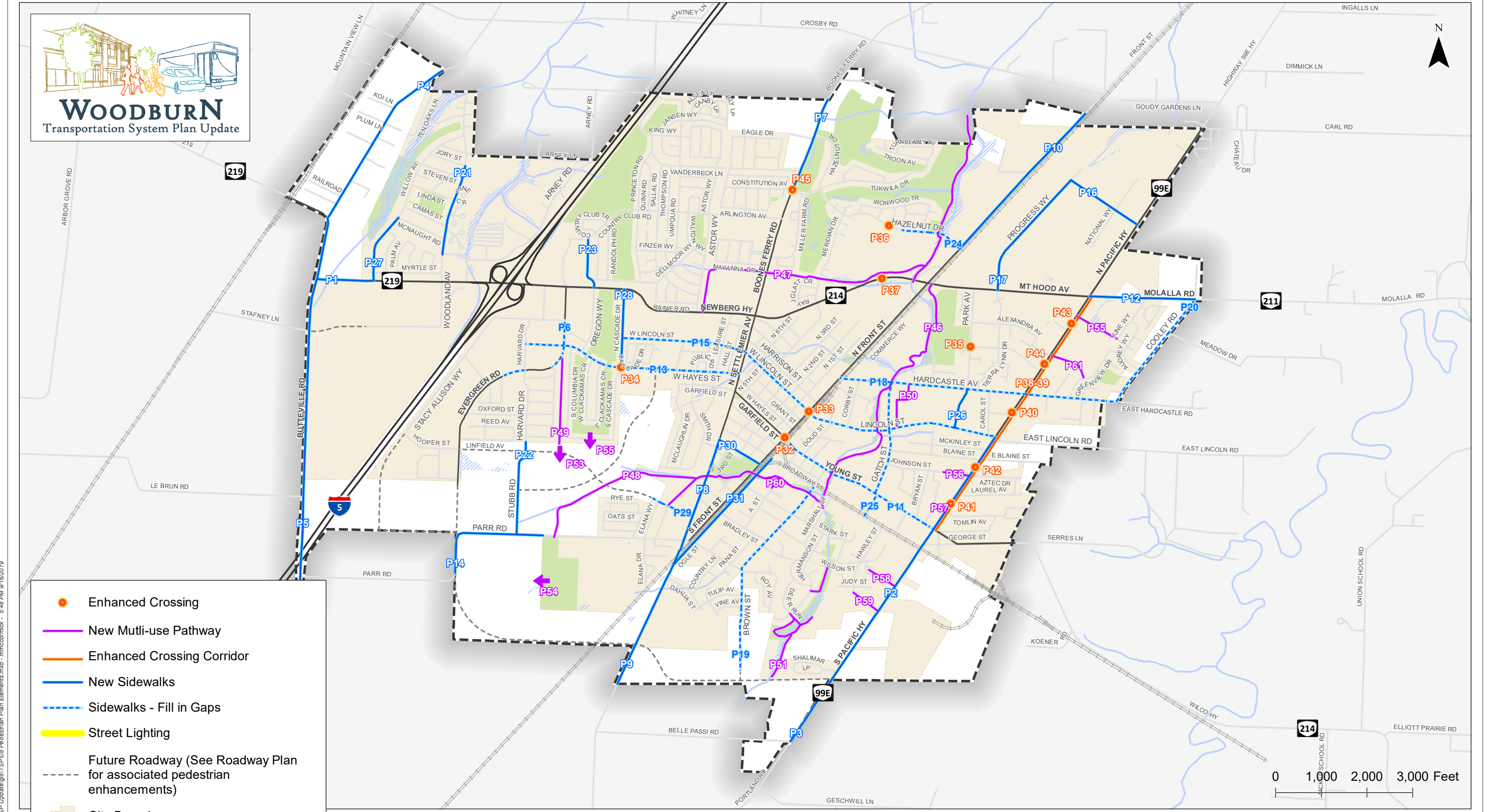
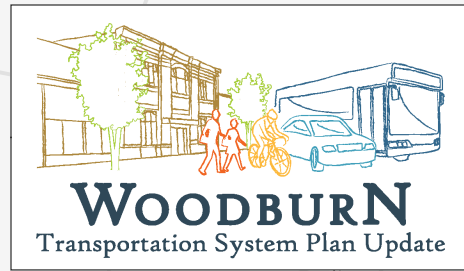


**Functional Roadway Classification
Woodburn, Oregon**

**Figure
2**

Note: Future roadway alignments are approximate and subject to further refinement.

H:\1212\1071 - Woodburn TSP Update\GIS\TSP02 Functional Roadway Classification.mxd - mmcormick - 5:25 PM 9/18/2019



- Enhanced Crossing
- New Multi-use Pathway
- Enhanced Crossing Corridor
- New Sidewalks
- - - Sidewalks - Fill in Gaps
- Street Lighting
- - - Future Roadway (See Roadway Plan for associated pedestrian enhancements)
- City Boundary
- Urban Growth Boundary

**Pedestrian Plan Elements
Woodburn, Oregon**

**Figure
8**

Note: Future roadway alignments are approximate and subject to further refinement.

H:\21121071 - Woodburn TSP Update\GIS\TSP08 Pedestrian Plan Elements.mxd - mmccormick - 5:48 PM 9/16/2019



December 12, 2019

Mr. Dan Handel

City of Woodburn, Planning Department

270 Montgomery St.

Woodburn, OR 97071

Subject: Woodburn High School, Career and Technical Education (CTE) &
Classroom Addition Traffic Impact

Dear Dan,

The purpose of this letter is to review the existing Woodburn High School capacity and expected traffic impacts for the new Woodburn High School Career and Technical Education (CTE) and Classroom Addition for both Phase 1 and Master Plan.

Prior to the 2012 fire, the existing High School had a capacity of 1,980 students. In 2012, the High School lost approximately 11,900 sq. ft. of program space due to a fire; six (6) classrooms, which reduced the existing capacity by approximately 210 students. Resulting in an overall capacity of 1,770.

In 2012, the District added 10 portables to address the reduced classroom program. Each portable accommodates two (2) classrooms at 30 students each, or 60 students total. The 10 portables provide an increased capacity of 600 students, or 2,370 overall capacity.

The portables will be removed from the site, once construction of Phase 1 CTE and Classroom Addition is complete. The program for the CTE and Classroom Addition is intended to replace a portion of the existing 10 portables; through eight (8) new classrooms at the Classroom Addition and two (2) new classrooms at the CTE. Each classroom will accommodate 32 students, equaling a capacity of 320. The Wood/Metal Shop and Media Production Lab are shared academy programs and will not add capacity to the existing High School program. Following the completion of the Phase 1, the overall capacity of the High School will be 2,050; which is well below the 2012 capacity.

The Master Plan Classroom Addition will add an additional capacity of 192 students; six (6) classrooms at 32 students each. This increase will result in an overall capacity of 2,242, which is 54 less than the 2012 capacity at 2,370.

Because the capacity of the high school will not increase, we conclude there will be no impact on traffic and a traffic study would not be necessary.

Sincerely,

Dr. William Rhoades,
Superintendent





Attachment 106











Mill Creek Greenway
North Tributary: Goose Creek





Existing Conditions and Proposed Options

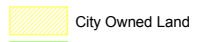
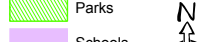

(Results of June '04 Workshop)
 Note: Map Not to Scale

-  Proposed Access-No Parking
-  Proposed Access-Parking
-  Notable Landmark
-  Proposed Play-Picnic Equipment

-  Proposed Crossings
-  Notable Landmark
-  Proposed Play-Picnic Equipment

- Legend**
-  Existing on-street bike routes and paths
 -  Proposed on-street bike route
 -  Existing off-street paths
 -  Proposed off-street paths
 -  Proposed Multi-Use Interim Route

-  Future Road
-  City Limits
-  Mill Creek & Tributaries
-  Mill Creek Floodplain

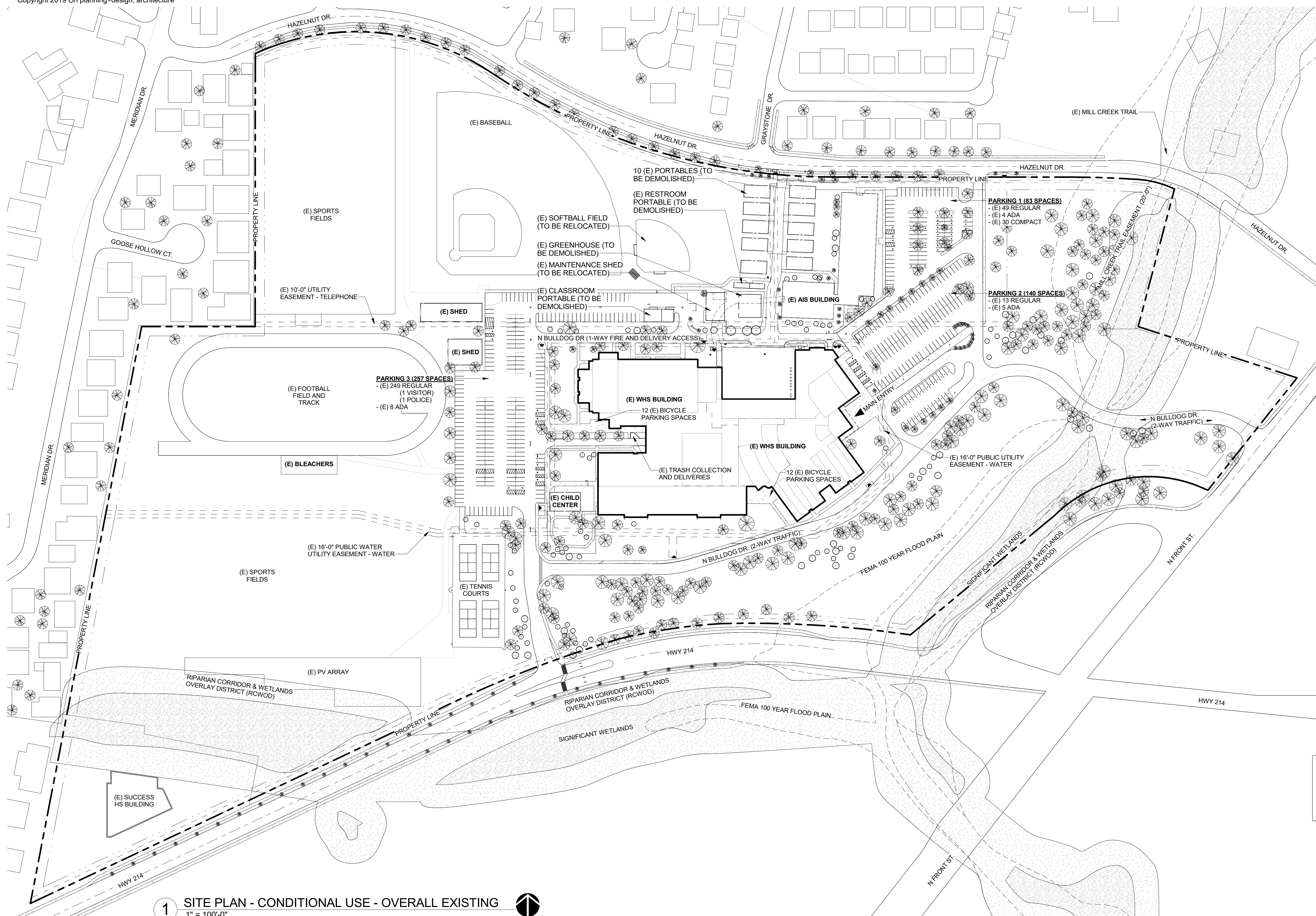
-  City Owned Land
-  Parks
-  Schools



- A. Refer to Landscape for planting count, tree protection plan and additional information.
- B. Refer to Electrical for site lighting and additional information.
- C. Refer to Civil for utilities and additional information.
- D. All fire hose lines are under 150 Feet maximum.
- E. Master Plan to include site improvements where required to ensure that pathways to buildings are ADA compliant.

SITE PLAN LEGEND
CONDITIONAL USE

- (E) EXISTING
- BUILDING OUTLINE
- (E) EASEMENT
- (E) SETBACKS
- BUILDING OVERHANGS AND CANOPIES ABOVE
- MAIN ENTRY
- FIRE HYDRANT
- FIRE DEPARTMENT CONNECTION



1 SITE PLAN - CONDITIONAL USE - OVERALL EXISTING
1" = 100'-0"

WSD HS CTE & CLASSROOM ADDITION

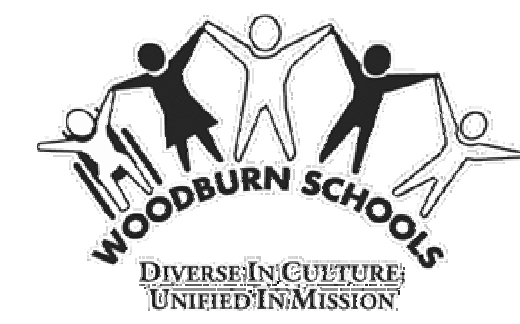
1785 N. FRONT ST. WOODBURN, OR 97071

CONDITIONAL USE

DATE: 02/04/2020

CU-A00

OVERALL EXISTING SITE PLAN



CU SITE PLAN SHEET NOTES

- A. Refer to Landscape for planting count, tree protection plan and additional information.
- B. Refer to Electrical for site lighting and additional information.
- C. Refer to Civil for utilities and additional information.
- D. All fire hose lines are under 150 Feet maximum.
- E. Master Plan to include site improvements where required to ensure that pathways to buildings are ADA compliant.

SITE PLAN LEGEND
CONDITIONAL USE

- (E) EXISTING
- NEW BUILDING OUTLINE
- (E) BUILDING OUTLINE
- PROPERTY LINE
- PHASE 1 AREA OF WORK
- (E) RIGHT-OF-WAY (ROW)
- WIDENED RIGHT-OF-WAY (ROW)
- EASEMENT
- BUILDING SETBACKS
- BUILDING OVERHANGS AND CANOPIES ABOVE
- FLS FIRE HOSE REACH
- FIRE ACCESS PATH
- ADA ACCESSIBLE PATH OF TRAVEL. PATHWAY TO COMPLY WITH ICC a117.1 STANDARDS.
- AREA OF NEW CONSTRUCTION (PHASE 1)
- AREA OF NEW CONSTRUCTION (PHASE 2)
- MAIN ENTRY
- FIRE HYDRANT
- FDC FIRE DEPARTMENT CONNECTION
- FIRE ACCESS ZONE
- NO PARKING FIRE LANE WITH RED PAINTED CURB

WDO Off-Street Parking Ratio Standards. Table 3.05A

Phase 2 Parking Count	
Regular:	390
ADA:	20
Compact:	65
Carpool:	2
Total:	477
Regular:	390
ADA:	20
Compact:	65
Carpool:	2
Total:	477
Bike Parking:	54

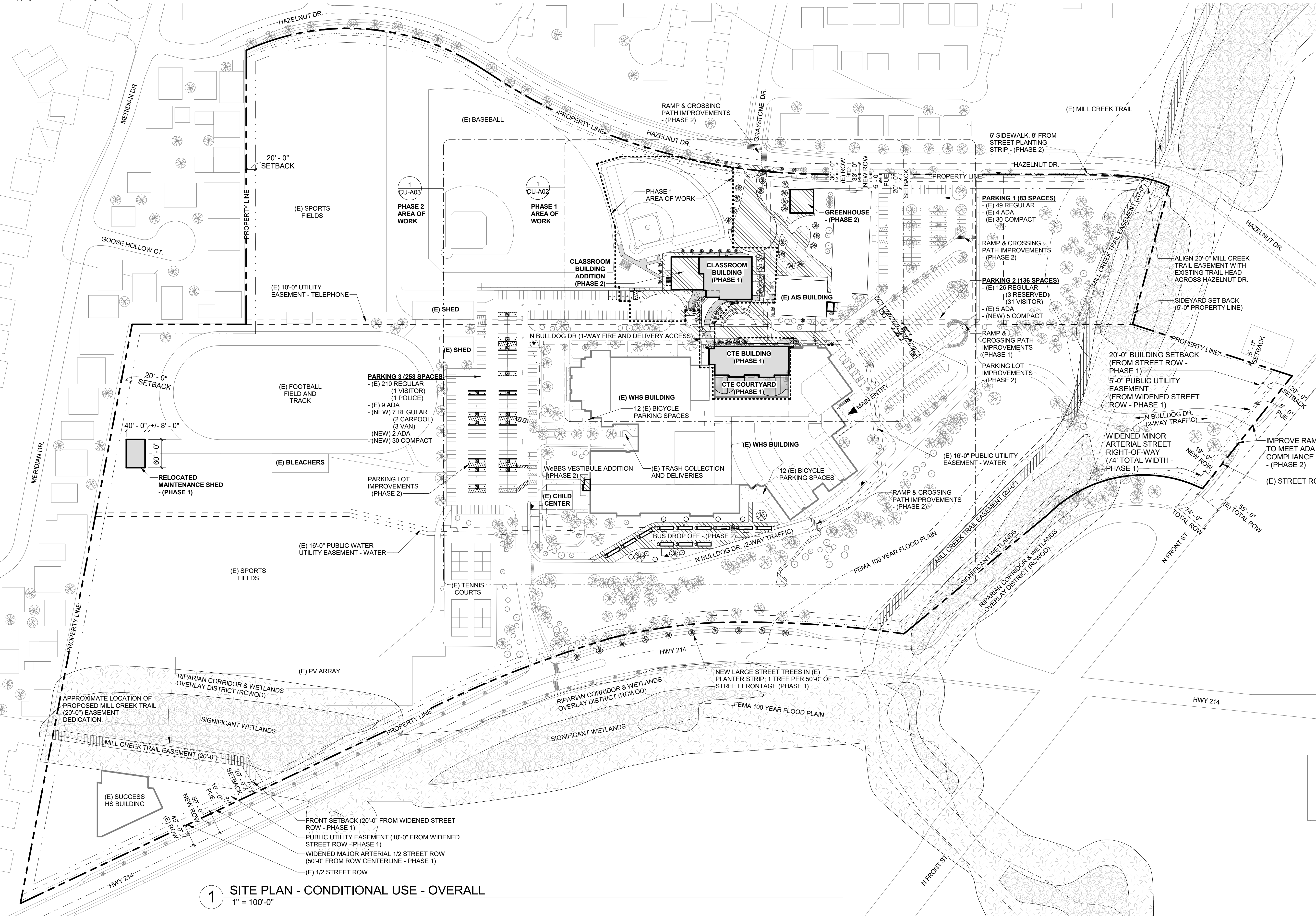
The Phase 2 Classroom and CTE Addition will have a student capacity of 568 students. Combined with the existing WHS building (1,770 students), the total student capacity for Phase 2 will be 2,242 students.

Based on WDO Table 3.05A, a minimum of 377 parking stalls are required. The Phase 2 parking count (477) exceeds this requirement by 103 stalls.

Based on the Phase 2 parking count, and WDO Table 3.05B, a minimum of 9 Accessible spaces and two (2) "Wheelchair User Only" spaces are required. Both requirements are met in the Phase 2 Parking Count.

Per WDO 3.05.03E, a maximum of 20 bike racks are required for the existing high school. 24 are currently provided. The City is requesting 2 additional spaces per classroom for the addition scope of work. 18 new bike stalls are being added in Phase 1. Phase 2 includes 12 additional bike stalls to meet the increased classroom program. These new racks will be located within 50' of the Classroom Building addition and the existing High School main entrance.

Based on Comprehensive Plan Goal H-1 at least two (2) carpool/vanpool stalls located convenient to staff entrances is required. Two (2) carpool/vanpool stalls have been designated by the Classroom Addition during Phase 2.



1 SITE PLAN - CONDITIONAL USE - OVERALL
1" = 100'-0"

WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CONDITIONAL USE

DATE: 02/04/2020

CU-A01

OVERALL MASTER PLAN

Project # 90049



OH PLANNING+DESIGN, ARCHITECTURE
115 NW 1st Ave, Ste. 300
Portland, OR 97209
1.503.280.8000
1.503.224.5442

- A. Refer to Landscape for planting count, tree protection plan and additional information.
- B. Refer to Electrical for site lighting and additional information.
- C. Refer to Civil for utilities and additional information.
- D. All fire hose lines are under 150 Feet maximum.
- E. Master Plan to include site improvements where required to ensure that pathways to buildings are ADA compliant.

SITE PLAN LEGEND
CONDITIONAL USE

- (E) EXISTING
- NEW BUILDING OUTLINE
- (E) BUILDING OUTLINE
- PROPERTY LINE
- PHASE 1 AREA OF WORK
- (E) RIGHT-OF-WAY (ROW)
- WIDENED RIGHT-OF-WAY (ROW)
- EASEMENT
- BUILDING SETBACKS
- BUILDING OVERHANGS AND CANOPIES ABOVE
- FLS FIRE HOSE REACH
- FIRE ACCESS PATH
- ADA ACCESSIBLE PATH OF TRAVEL. PATHWAY TO COMPLY WITH ICC a117.1 STANDARDS.
- AREA OF NEW CONSTRUCTION (PHASE 1)
- AREA OF NEW CONSTRUCTION (PHASE 2)
- MAIN ENTRY
- FIRE HYDRANT
- FDC FIRE DEPARTMENT CONNECTION
- FIRE ACCESS ZONE
- 'NO PARKING FIRE LANE' WITH RED PAINTED CURB

WDO Off-Street Parking Ratio Standards. Table 3.05A

Phase 1 Parking Count

Regular: 439
ADA: 20
Compact: 30
Carpool: 2
Total: 491

Bike Parking: 42

BICYCLE PARKING	SPACES PER PHASE	TOTAL SPACES
EXISTING	24 (20 MAX. PER WDO 3.05.03.E)	24
PHASE 1	9 CLASSROOMS X2 = 18	42

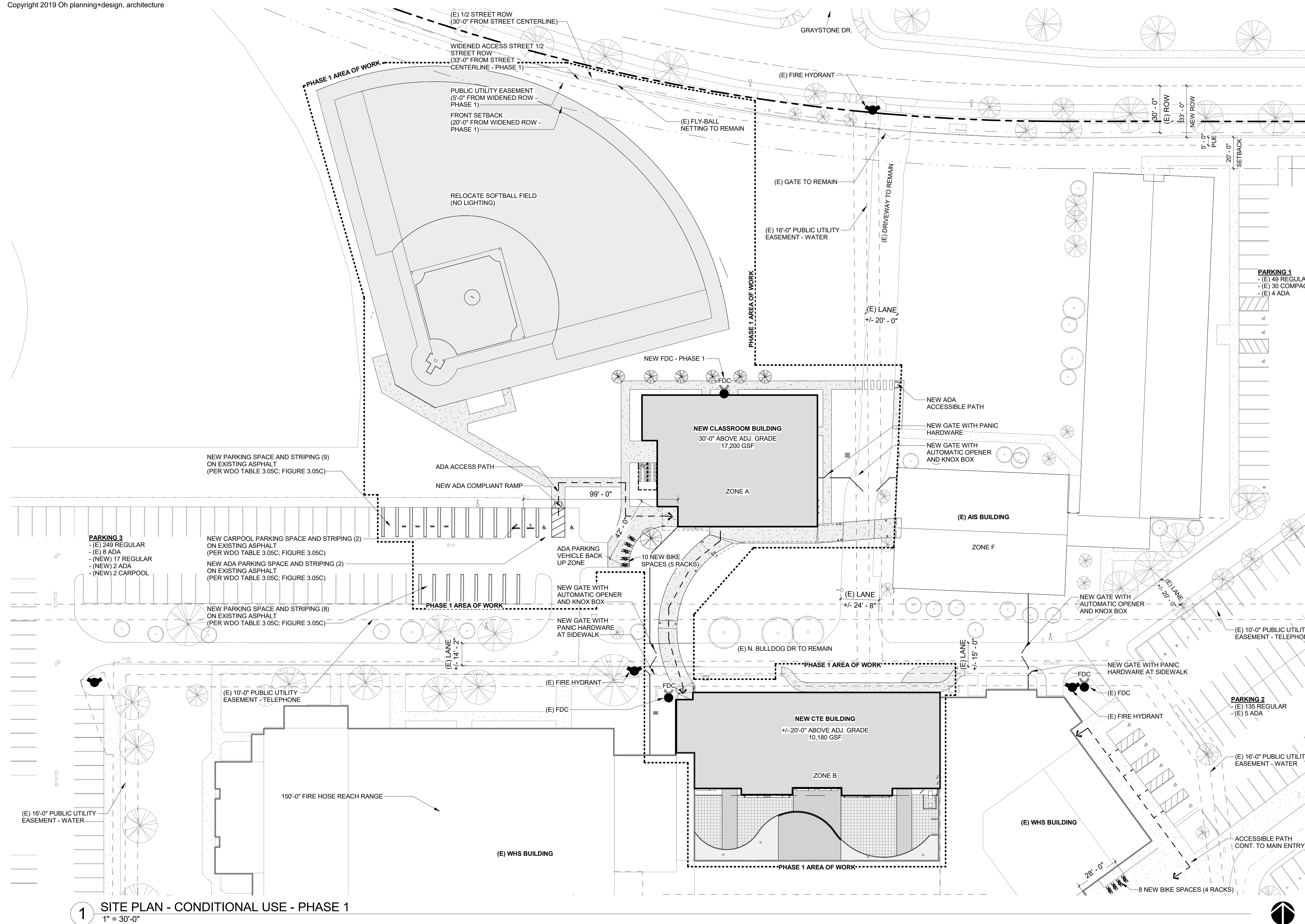
Phase 1 Classroom and CTE Addition will have a student capacity of 376 students, combined with the existing WHS building (1,770 students) the total student capacity for Phase 1 will be 2,050 students.

Based on WDO Table 3.05A, a minimum of 342 parking stalls are required. The Phase 1 parking count (491) exceeds this requirement by 149 stalls.

Based on the Phase 1 parking count, and WDO Table 3.05B, a minimum of 9 Accessible spaces and 2 "Wheelchair User Only" spaces are required. Both requirements are met in the Phase 1 Parking Count.

Per WDO 3.05.03E, a maximum of 20 bike rack are required for the existing high school. 24 are currently provided. The City is requesting 2 additional space per classroom for the addition scope of work. 18 new bike stalls are being added in Phase 1. These new racks will be located within 50' of the new Classroom Building addition (4 racks) and the existing High School main entrance (5 racks).

Based on Comprehensive Plan Goal H-1 at least two (2) carpool/vanpool stalls located convenient to staff entrances is required. two (2) carpool/vanpool stalls have been added during Phase 1.



1 SITE PLAN - CONDITIONAL USE - PHASE 1
1" = 30'-0"

WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CONDITIONAL USE

DATE: 02/04/2020

(PHASE 1) ENLARGED SITE PLAN

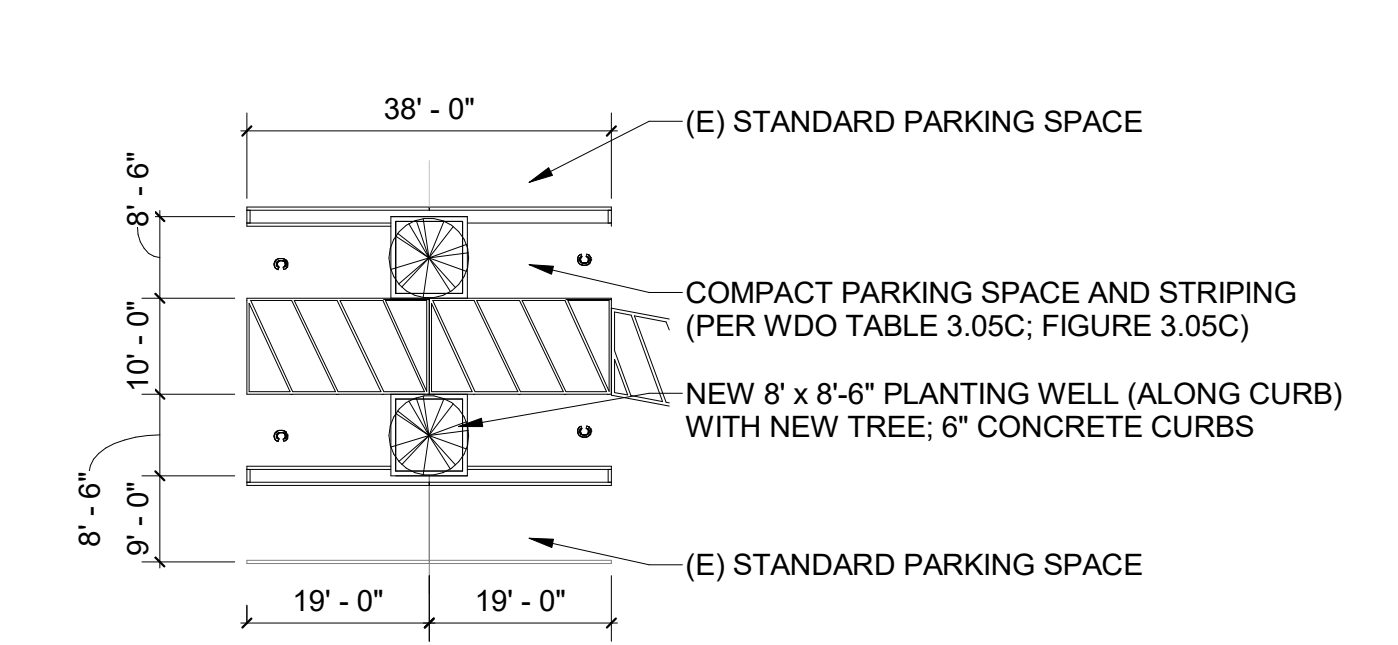
CU-A02

Project # 90049

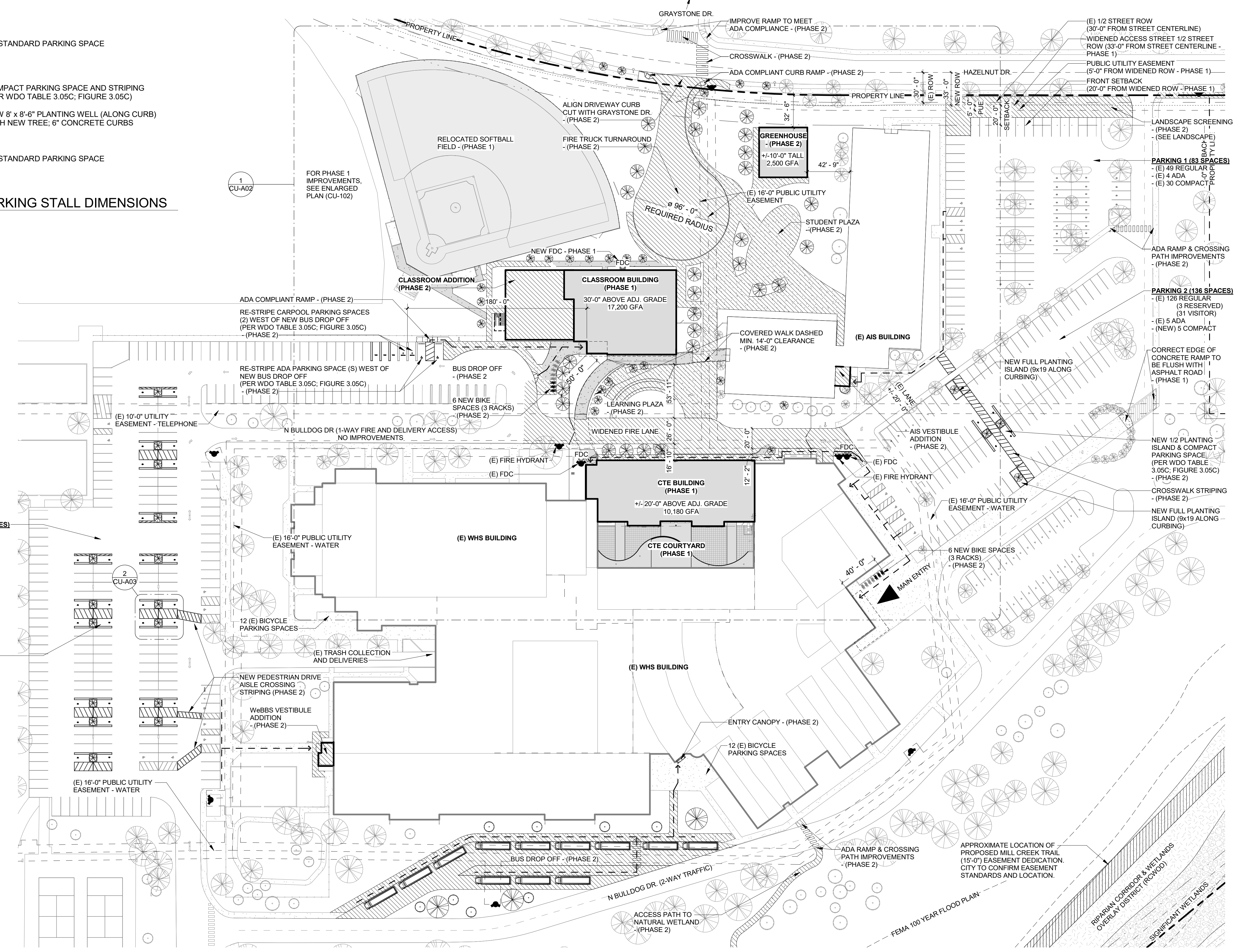


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Portland, OR 97209
1.503.280.8000
1.503.224.5442





2 SITE PLAN - TYPICAL PARKING STALL DIMENSIONS
1" = 20'-0"



1 SITE PLAN - CONDITIONAL USE - MASTER PLAN
1" = 50'-0"

- A. Refer to Landscape for planting count, tree protection plan and additional information.
- B. Refer to Electrical for site lighting and additional information.
- C. Refer to Civil for utilities and additional information.
- D. All fire hose lines are under 150 Feet maximum.
- E. Master Plan to include site improvements where required to ensure that pathways to buildings are ADA compliant.

SITE PLAN LEGEND
CONDITIONAL USE

- (E) EXISTING
- NEW BUILDING OUTLINE
- (E) BUILDING OUTLINE
- PROPERTY LINE
- PHASE 1 AREA OF WORK
- (E) RIGHT-OF-WAY (ROW)
- WIDENED RIGHT-OF-WAY (ROW)
- EASEMENT
- BUILDING SETBACKS
- BUILDING OVERHANGS AND CANOPIES ABOVE
- FLS FIRE HOSE REACH
- FIRE ACCESS PATH
- ADA ACCESSIBLE PATH OF TRAVEL PATHWAY TO COMPLY WITH ICC a117.1 STANDARDS.
- AREA OF NEW CONSTRUCTION (PHASE 1)
- AREA OF NEW CONSTRUCTION (PHASE 2)
- MAIN ENTRY
- FIRE HYDRANT
- FDC FIRE DEPARTMENT CONNECTION
- FIRE ACCESS ZONE
- 'NO PARKING FIRE LANE' WITH RED PAINTED CURB

WDO Off-Street Parking Ratio Standards. Table 3.05A

Phase 2 Parking Count

Regular:	390
ADA:	20
Compact:	65
Carpool:	2
Total:	477

Regular:	390
ADA:	20
Compact:	65
Carpool:	2
Total:	477

Bike Parking: 54

BICYCLE PARKING	SPACES PER PHASE	TOTAL SPACES
EXISTING	24 (20 MAX. PER WDO 3.05.03.E)	24
PHASE 1	9 CLASSROOMS X 2 = 18	42
PHASE 2	6 CLASSROOMS X 2 = 12	54

The Phase 2 Classroom and CTE Addition will have a student capacity of 568 students. Combined with the existing WHS building (1,770 students), the total student capacity for Phase 2 will be 2,242 students.

Based on WDO Table 3.05A, a minimum of 377 parking stalls are required. The Phase 2 parking count (477) exceeds this requirement by 103 stalls.

Based on the Phase 2 parking count, and WDO Table 3.05B, a minimum of 9 Accessible spaces and two (2) "Wheelchair User Only" spaces are required. Both requirements are met in the Phase 2 Parking Count.

Per WDO 3.05.03E, a maximum of 20 bike racks are required for the existing high school. 24 are currently provided. The City is requesting 2 additional spaces per classroom for the addition scope of work. 18 new bike stalls are being added in Phase 1. Phase 2 includes 12 additional bike stalls to meet the increased classroom program. These new racks will be located within 50' of the Classroom Building addition and the existing High School main entrance.

Based on Comprehensive Plan Goal H-1 at least two (2) carpool/vanpool stalls located convenient to staff entrances is required. Two (2) carpool/vanpool stalls have been designated by the Classroom Addition during Phase 2.



WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

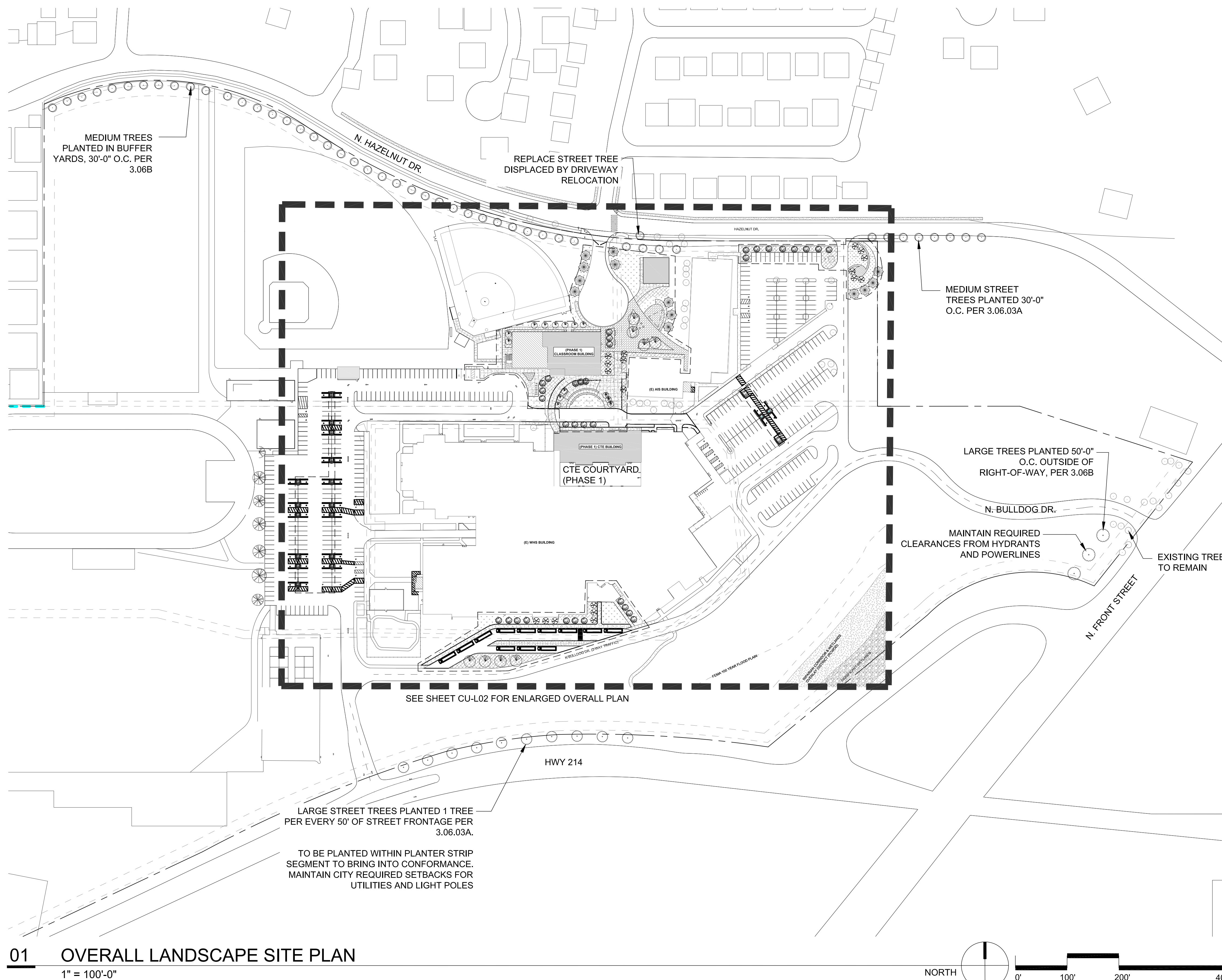
CONDITIONAL USE

DATE: 02/04/2020

(PHASE 2) ENLARGED SITE PLAN

CU-A03

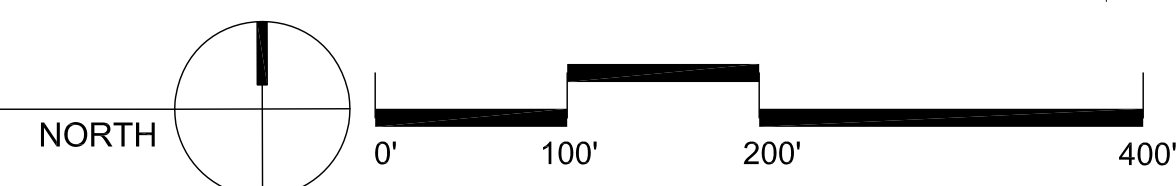
Project # 90049



SITE MATERIAL LEGEND	
	SEEDED LAWN AREAS
	PLANTING AREAS (SHRUBS / GROUNDCOVER)
	NATIVE PLANTING / RAIN GARDEN
	NEW CONCRETE PAVERS
	NEW SCORED CONCRETE PAVING
	DECOMPOSED GRANITE
	CAFE TABLES
	LOUNGE CHAIRS
	BENCHES
	PROPERTY LINE
	LIMIT OF WORK
	PUBLIC UTILITY EASEMENT
	PROPOSED TREES
	EXISTING TREES TO REMAIN

- LANDSCAPE NOTES**
- FOR PARKING LOT AREAS, DRIVE ISLES, SEE CIVIL PLANS.
 - FOR BUILDING AND OVERALL SITE INFORMATION, SEE ARCHITECTS PLANS.
 - IRRIGATION WILL BE PROVIDED FOR ALL PLANTING AREAS PER CITY REQUIREMENTS.
 - ALL SHRUBS AND GROUND COVER WILL BE OF A SIZE UPON INSTALLATION SO AS TO ATTAIN 80% GROUND COVERAGE WITHIN (3) YEARS.
 - ALL BARE SOIL WITHIN PLANTING BEDS TO BE COVERED WITH A 2" LAYER OF BARK MULCH.
 - PLANT MATERIALS SHALL BE APPROPRIATE TO CLIMATE AND ENVIRONMENT OF WOODBURN.
 - ALL LANDSCAPING TO MEET CITY OF WOODBURN PLANTING DENSITY REQUIREMENTS PER WDO SECTION 3.06.
 - PER CITY OF WOODBURN STANDARDS, ONE TREE OF APPROPRIATE SIZE PER (50) FEET OF STREET FRONTAGE SHALL BE PLANTED ALONG HAZELNUT DRIVE AND N. BULLDOG DRIVE WHERE EXISTING TREES FAIL TO MEET REQUIREMENT.
 - A MINIMUM OF 20% OF PAVED SURFACE AREA OF NEW OFF STREET PARKING AND LOADING AREAS TO BE LANDSCAPED PER CODE REQUIREMENTS.
 - EXISTING STREET TREES ALONG HAZELNUT DRIVE TO REMAIN. TREES NOT INCLUDED IN SURVEY.
 - LAWN AREAS TO BE RESEEDED WHERE DEVELOPMENT DISTURBS EXISTING VEGETATION.
 - NO TREE SPECIES FROM THE WDO PROHIBITED TREE LIST SHALL BE PLANTED.

01 OVERALL LANDSCAPE SITE PLAN
1" = 100'-0"



WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CONDITIONAL USE DOCUMENTS

DATE: 02/04/2020

CU-L01

OVERALL LANDSCAPE SITE PLAN - CONDITIONAL USE

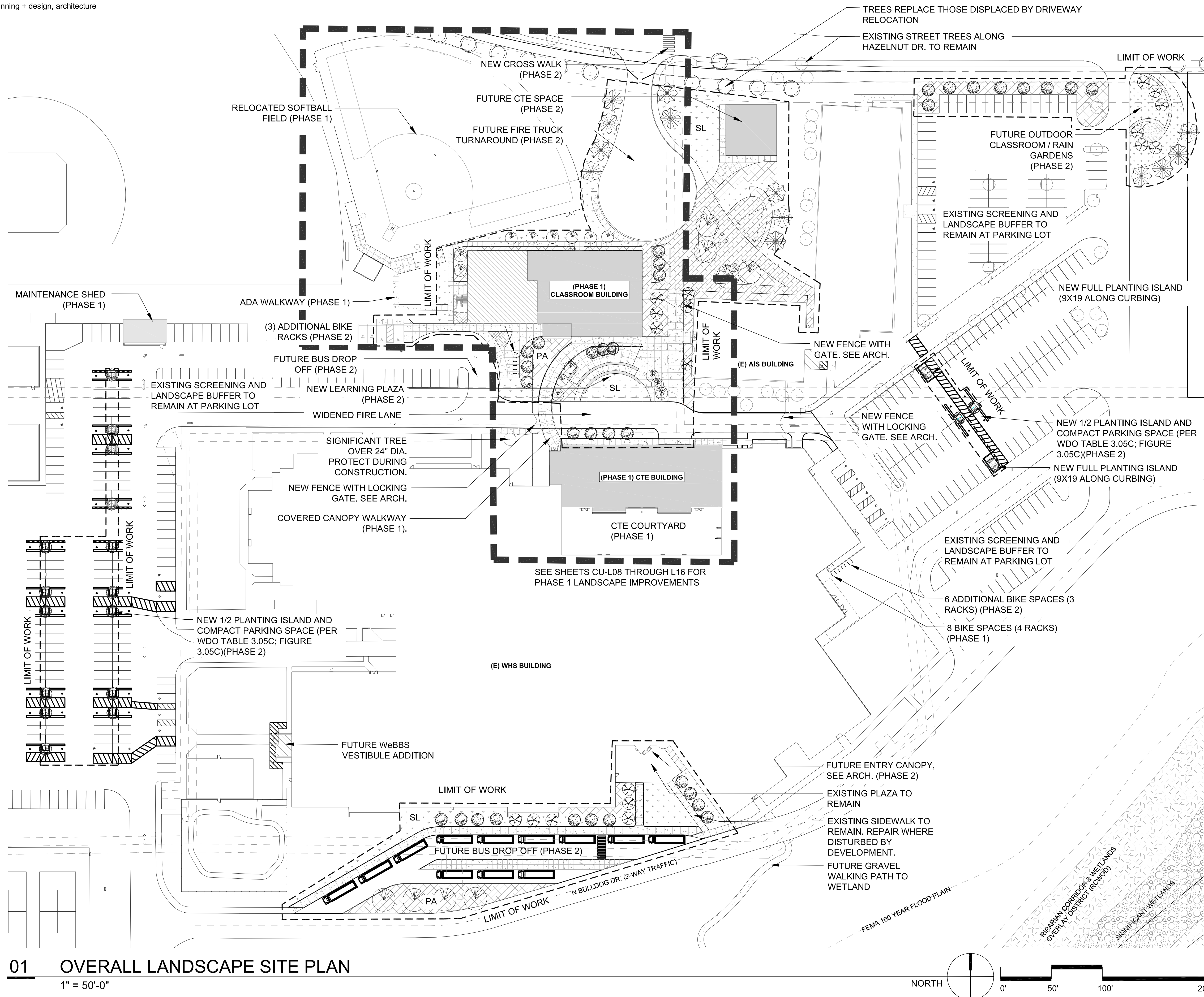
Project # 90049



OHPLANNING+DESIGN,
ARCHITECTURE
115 NW 1st Ave, Ste. 300
Portland, OR 97209
t 503.280.8000
f 503.224.5442



ecotone
ENVIRONMENTAL
522 N. THOMPSON ST., SUITE 4 - PORTLAND, OREGON - 97227
PHONE: 503.252.5235 EMAIL: INFO@ECOTONE-ENV.COM



SITE MATERIAL LEGEND	
SL	SEEDED LAWN AREAS
PA	PLANTING AREAS (SHRUBS / GROUNDCOVER)
	NATIVE PLANTING / RAIN GARDEN
	NEW CONCRETE PAVERS
	NEW SCORED CONCRETE PAVING
	DECOMPOSED GRANITE
	CAFE TABLES
	LOUNGE CHAIRS
	BENCHES
	PROPERTY LINE
	LIMIT OF WORK
	PUBLIC UTILITY EASEMENT
	PROPOSED TREES
	EXISTING TREES TO REMAIN

- LANDSCAPE NOTES**
- FOR PARKING LOT AREAS, DRIVE ISLES, SEE CIVIL PLANS.
 - FOR BUILDING AND OVERALL SITE INFORMATION, SEE ARCHITECTS PLANS.
 - IRRIGATION WILL BE PROVIDED FOR ALL PLANTING AREAS PER CITY REQUIREMENTS.
 - ALL SHRUBS AND GROUND COVER WILL BE OF A SIZE UPON INSTALLATION SO AS TO ATTAIN 80% GROUND COVERAGE WITHIN (3) YEARS.
 - ALL BARE SOIL WITHIN PLANTING BEDS TO BE COVERED WITH A 2" LAYER OF BARK MULCH.
 - PLANT MATERIALS SHALL BE APPROPRIATE TO CLIMATE AND ENVIRONMENT OF WOODBURN.
 - ALL LANDSCAPING TO MEET CITY OF WOODBURN PLANTING DENSITY REQUIREMENTS PER WDO SECTION 3.06.
 - PER CITY OF WOODBURN STANDARDS, ONE TREE OF APPROPRIATE SIZE PER (50) FEET OF STREET FRONTAGE SHALL BE PLANTED ALONG HAZELNUT DRIVE AND N. BULLDOG DRIVE WHERE EXISTING TREES FAIL TO MEET REQUIREMENT.
 - A MINIMUM OF 20% OF PAVED SURFACE AREA OF NEW OFF STREET PARKING AND LOADING AREAS TO BE LANDSCAPED PER CODE REQUIREMENTS.
 - EXISTING STREET TREES ALONG HAZELNUT DRIVE TO REMAIN. TREES NOT INCLUDED IN SURVEY.
 - LAWN AREAS TO BE RESEEDED WHERE DEVELOPMENT DISTURBS EXISTING VEGETATION.
 - NO TREE SPECIES FROM THE WDO PROHIBITED TREE LIST SHALL BE PLANTED.

01 OVERALL LANDSCAPE SITE PLAN
1" = 50'-0"

WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CONDITIONAL USE DOCUMENTS

DATE: 02/04/2020

CU-L02

OVERALL LANDSCAPE SITE PLAN - CONDITIONAL USE

Project # 90049



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t 503.280.8000
f 503.224.5442



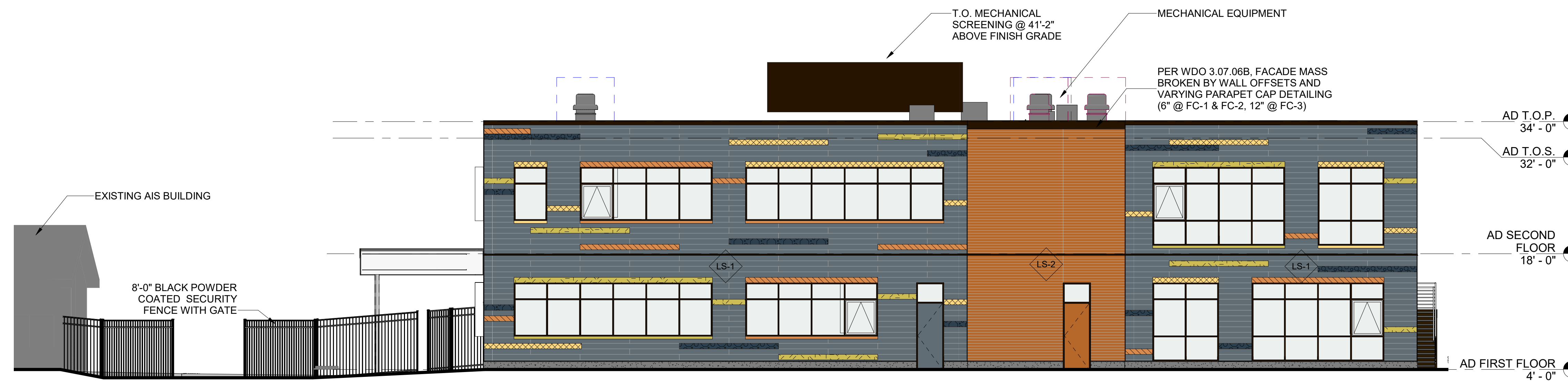
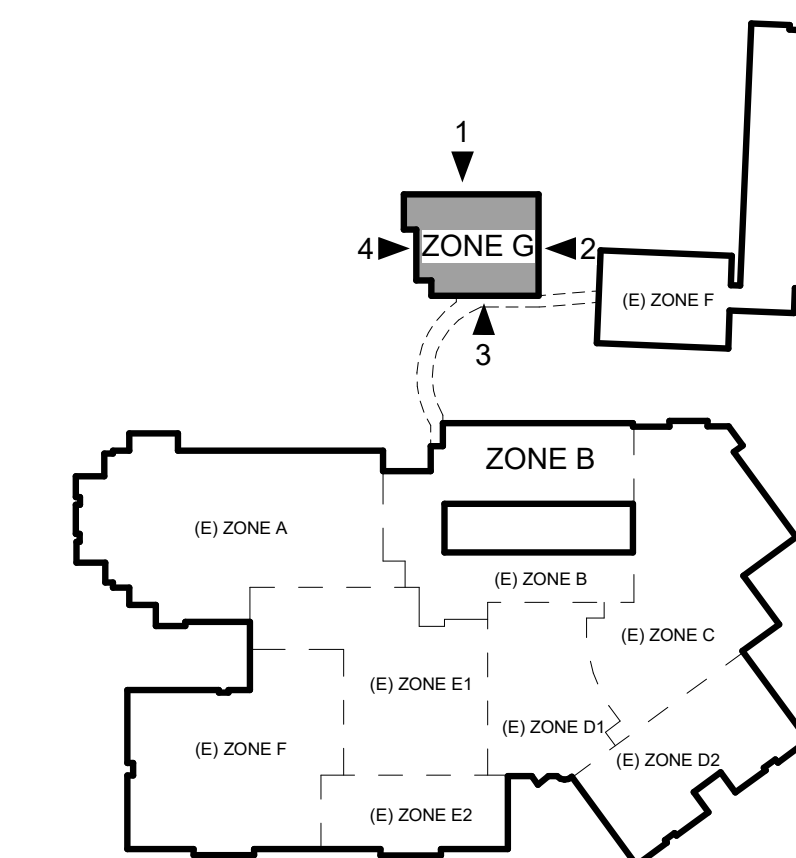
522 N. THOMPSON ST., SUITE 4, PORTLAND, OREGON - 97227
PHONE: 503.252.5235 EMAIL: INFO@ECOTONE-ENV.COM

EXTERIOR ELEVATIONS LEGEND

NOTE: COLORS ARE SHOWN AS AN APPROXIMATION TO TRUE MATERIAL COLOR ONLY.

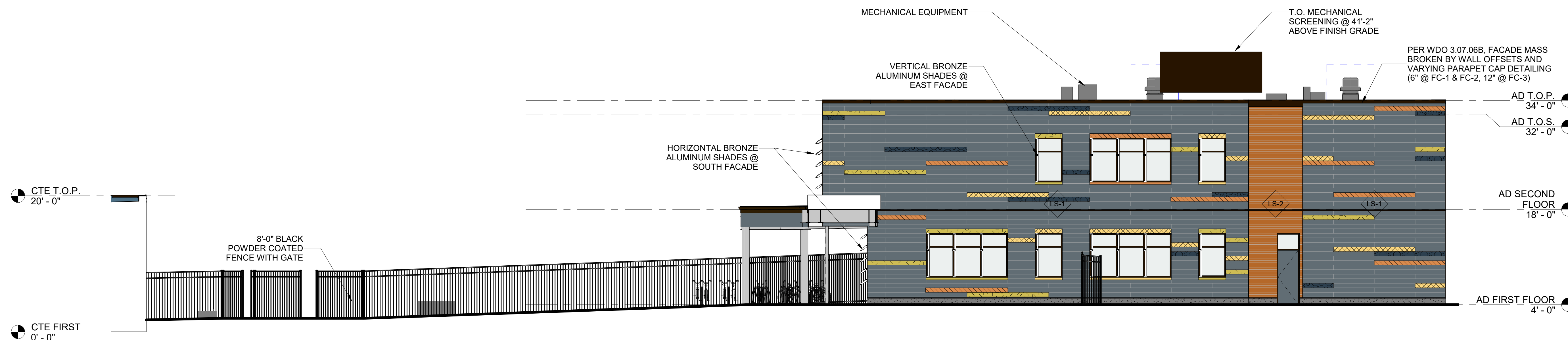
- LS-1** FIBER CEMENT - ALLURA
COLOR: PACIFIC BLUE W/ 15% CUSTOM (BELOW)
SMOOTH LAP SIDING - 8" LAP EXPOSURE
 - LS-2** FIBER CEMENT - ALLURA
COLOR: MAHOGANY
GRAIN LAP SIDING - 4" LAP EXPOSURE
 - PS-1** FIBER CEMENT - ALLURA
COLOR: CAVALRY
SMOOTH PANEL SIDING - 4'X8' PANEL W/
ALUMINUM REVEL
- CUSTOM COLORS:**
-

KEY PLAN



1 NORTH ELEVATION - ZONE A - CONDITIONAL USE
1/8" = 1'-0"

31% GLAZED SURFACE ON NORTH FACADE FACING HAZELNUT DR.
(PER WDO 3.07.06)



2 EAST ELEVATION - ZONE A - CONDITIONAL USE
1/8" = 1'-0"

15% GLAZED SURFACE ON EAST FACADE
(DOES NOT FACE PUBLIC STREET)

WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CU-A07

CONDITIONAL USE

DATE: 01/20/2020

EXTERIOR ELEVATIONS - ZONE A

Project # 90049


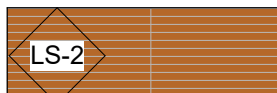



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ARCHITECTURE
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1.503.280.8000
1.503.224.5442

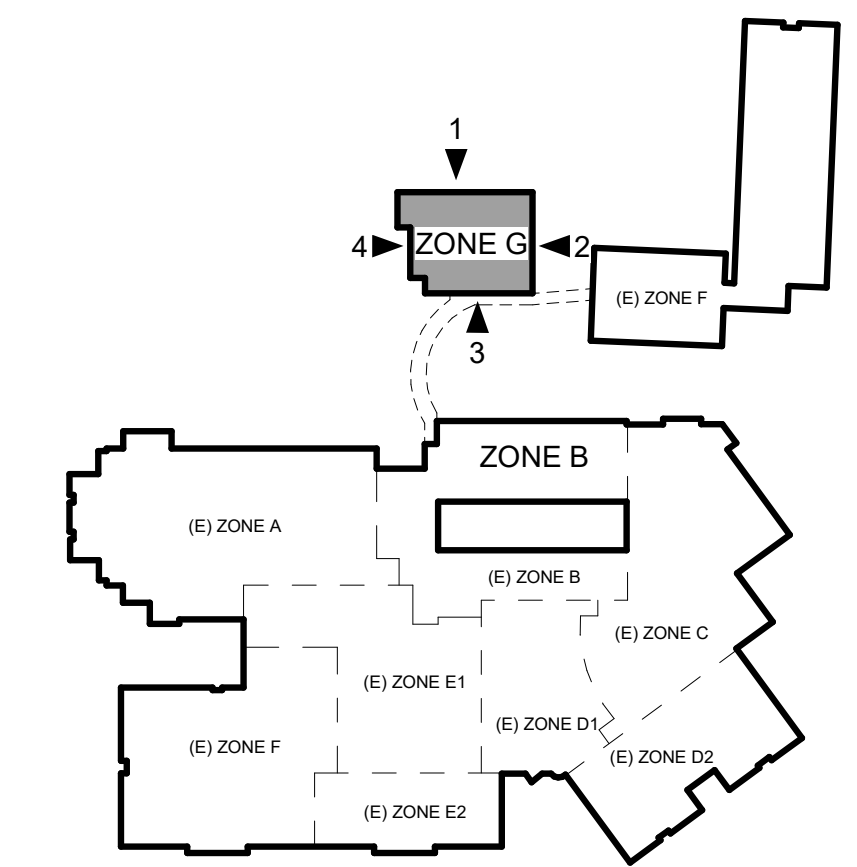


EXTERIOR ELEVATIONS LEGEND

NOTE: COLORS ARE SHOWN AS AN APPROXIMATION TO TRUE MATERIAL COLOR ONLY.

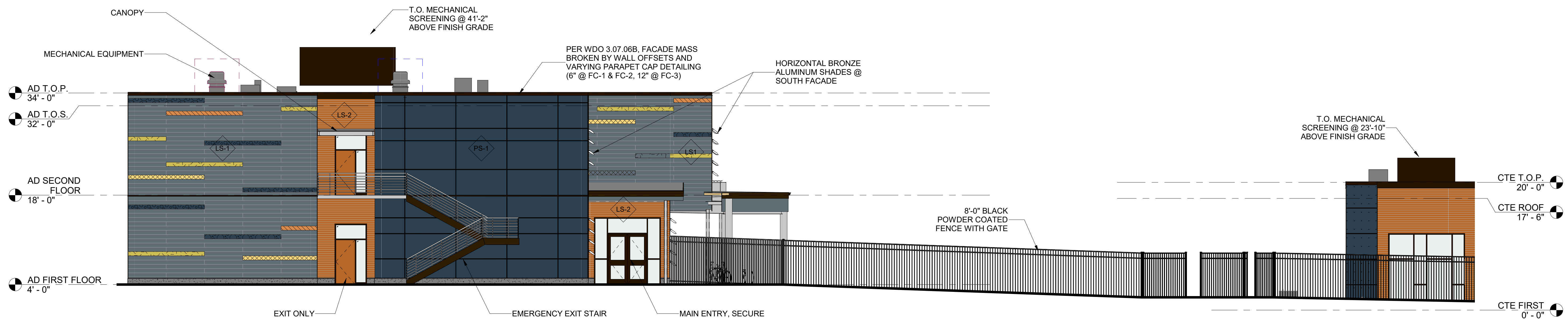
- 
LS-1 FIBER CEMENT - ALLURA
 COLOR: PACIFIC BLUE W/ 15% CUSTOM (BELOW)
 SMOOTH LAP SIDING - 8" LAP EXPOSURE
- 
LS-2 FIBER CEMENT - ALLURA
 COLOR: MAHOGANY
 GRAIN LAP SIDING - 4" LAP EXPOSURE
- 
PS-1 FIBER CEMENT - ALLURA
 COLOR: CAVALRY
 SMOOTH PANEL SIDING - 4'X8' PANEL W/
 ALUMINUM REVEL

KEY PLAN



3 SOUTH ELEVATION - ZONE A - CONDITIONAL USE
 1/8" = 1'-0"

28% GLAZED SURFACE ON SOUTH FACADE.
 (DOES NOT FACE PUBLIC STREET))



4 WEST ELEVATION - ZONE A - CONDITIONAL USE
 1/8" = 1'-0"

5% GLAZED SURFACE ON WEST FACADE
 (DOES NOT FACE PUBLIC STREET))

WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CU-A08

CONDITIONAL USE

DATE: 01/20/2020

EXTERIOR ELEVATIONS - ZONE A

Project # 90049



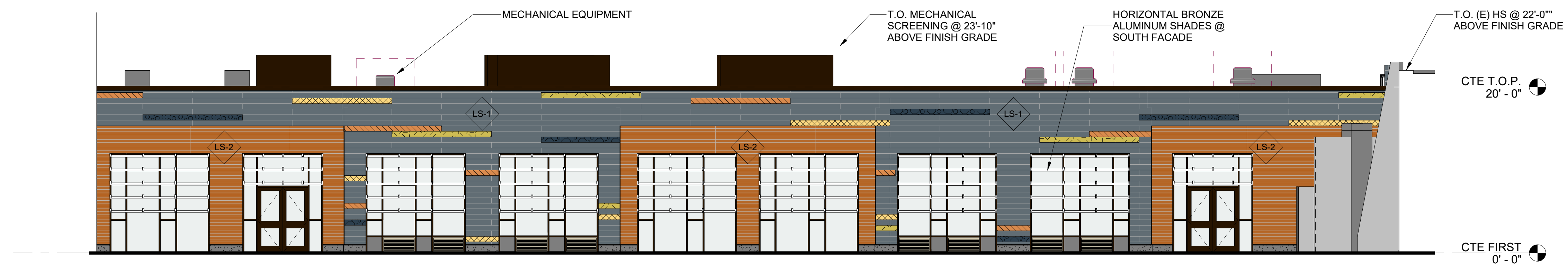
OH PLANNING+DESIGN,
 ARCHITECTURE
 115 NW 1st Ave, Ste. 300
 Portland, OR 97209
 1.503.280.8000
 1.503.224.5442





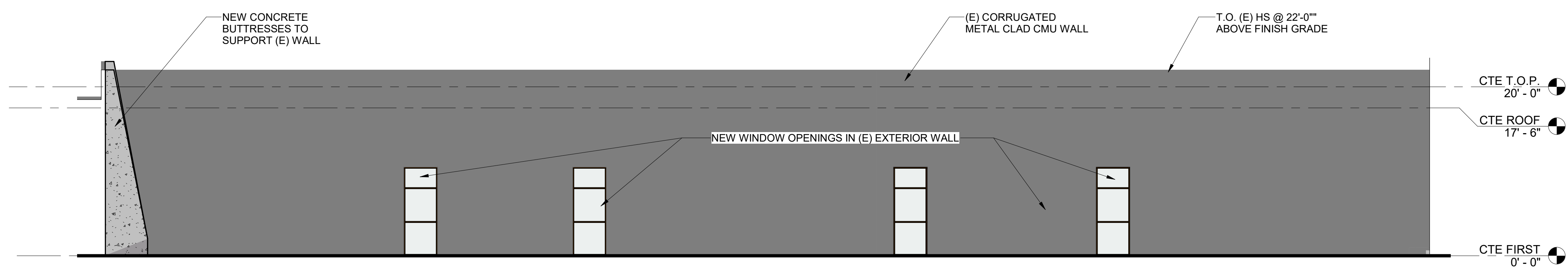
1 NORTH ELEVATION - ZONE B - CONDITIONAL USE
1/8" = 1'-0"

36% GLAZED SURFACE ON NORTH FACADE FACING N. BULLDOG DR.
(PER WDO 3.07.06)

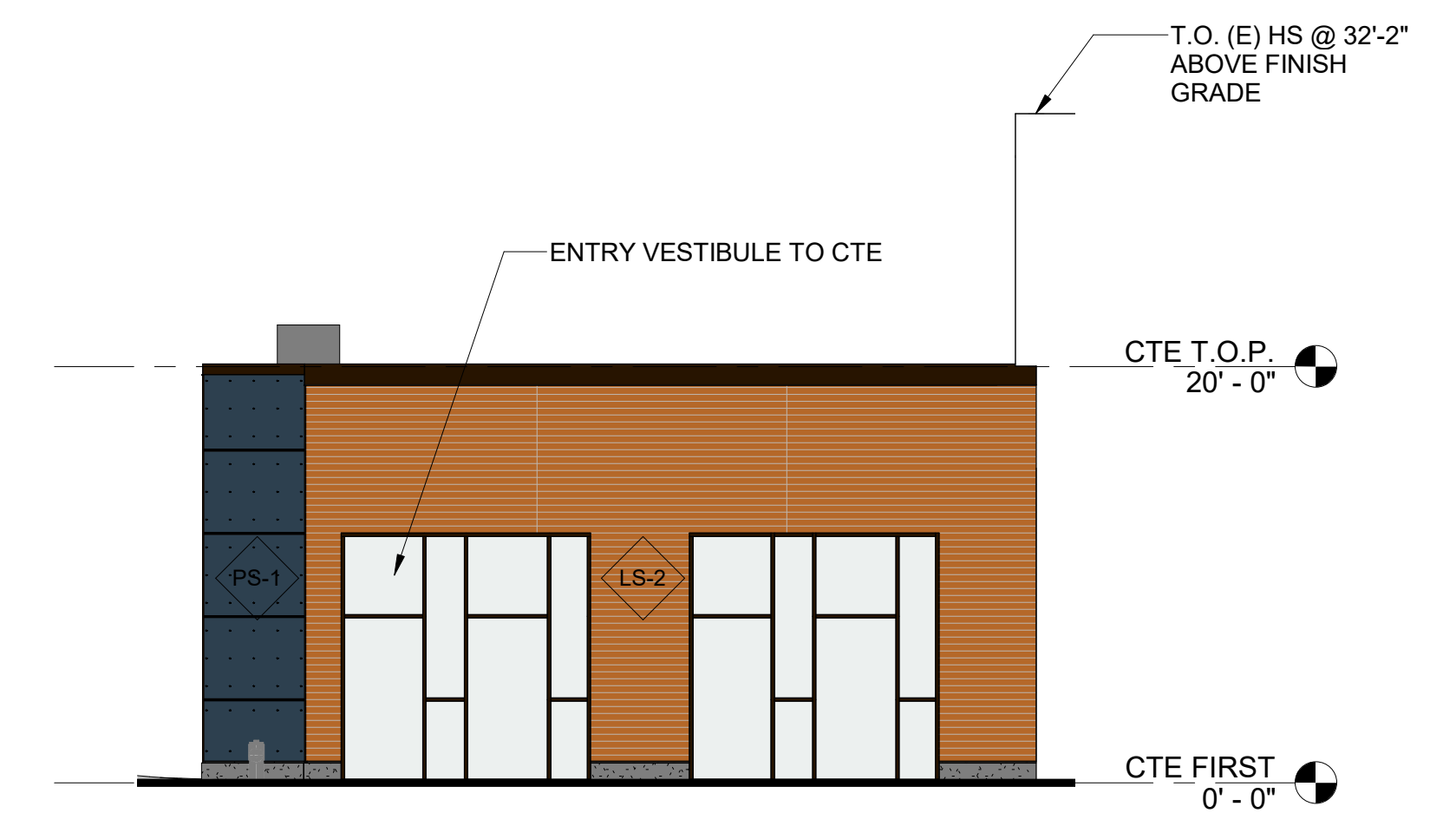


2 SOUTH ELEVATION - ZONE B - CONDITIONAL USE
1/8" = 1'-0"

30% GLAZED SURFACE ON SOUTH FACADE FACING STUDENT COURTYARD
(DOES NOT FACE PUBLIC STREET)




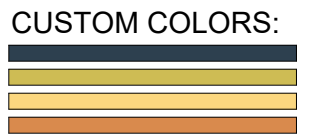


4 NORTH ELEVATION - ZONE B COURTYARD - CONDITIONAL USE
1/8" = 1'-0"



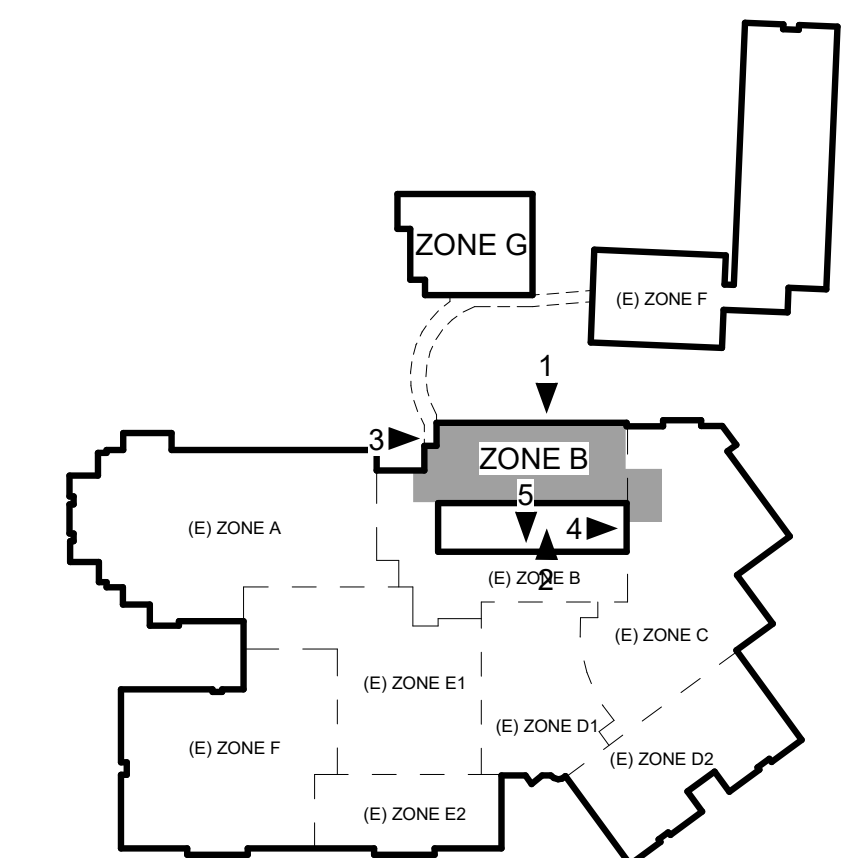
3 WEST ELEVATION - ZONE B - CONDITIONAL USE
1/8" = 1'-0"

EXTERIOR ELEVATIONS LEGEND

NOTE: COLORS ARE SHOWN AS AN APPROXIMATION TO TRUE MATERIAL COLOR ONLY.

- 
LS-1 FIBER CEMENT - ALLURA
COLOR: PACIFIC BLUE W/ 15% CUSTOM (BELOW)
SMOOTH LAP SIDING - 8" LAP EXPOSURE
- CUSTOM COLORS:**

- 
LS-2 FIBER CEMENT - ALLURA
COLOR: MAHOGANY
GRAIN LAP SIDING - 4" LAP EXPOSURE
- 
PS-1 FIBER CEMENT - ALLURA
COLOR: CAVALRY
SMOOTH PANEL SIDING - 4'X8' PANEL W/
ALUMINUM REVEL

KEY PLAN



OH PLANNING+DESIGN,
ARCHITECTURE
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1.503.280.8000
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WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

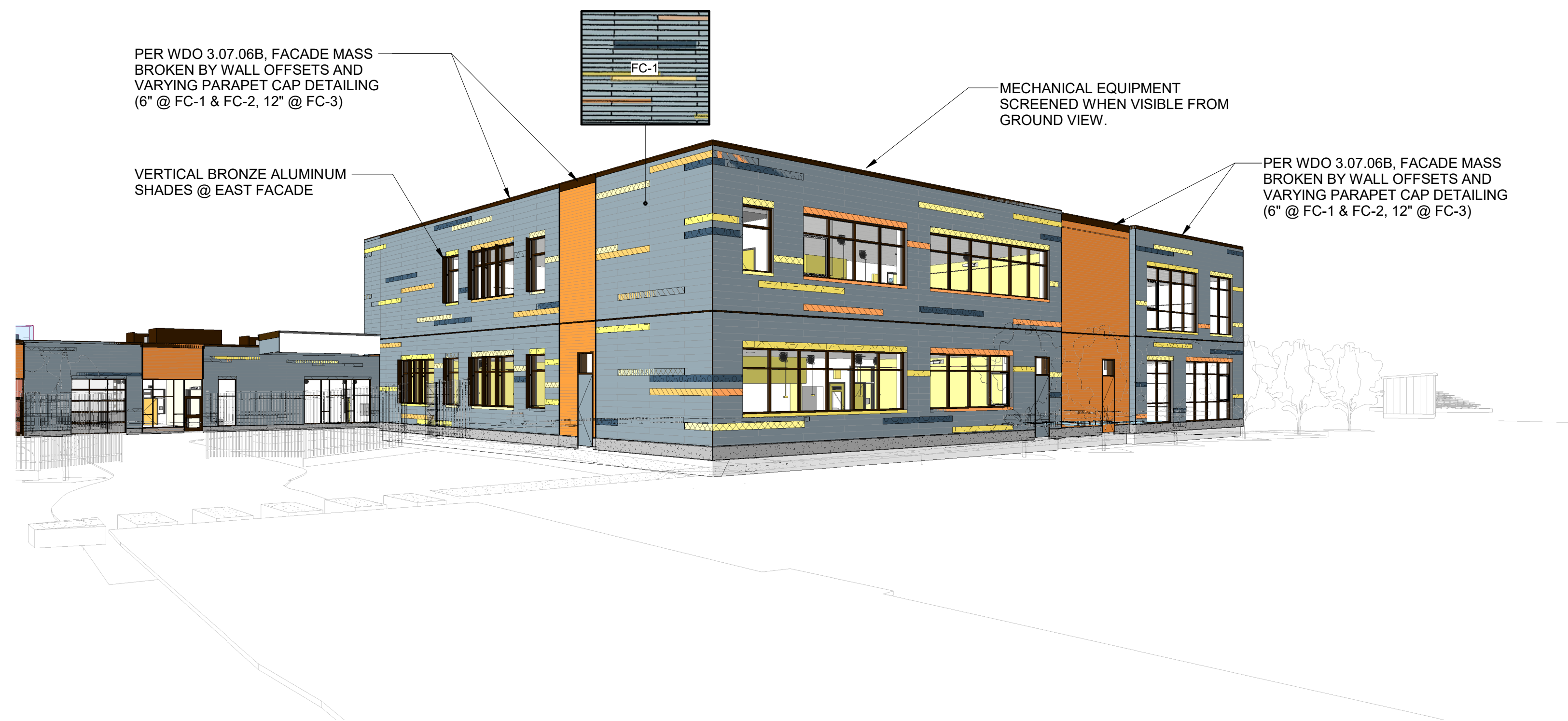
CONDITIONAL USE

DATE: 01/20/2020

EXTERIOR ELEVATIONS - ZONE B

CU-A09

Project # 90049




2 CLASSROOM ADDITION - NORTHEAST CORNER

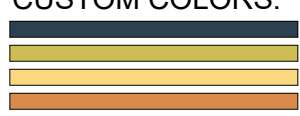
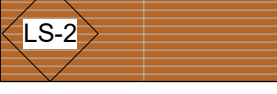



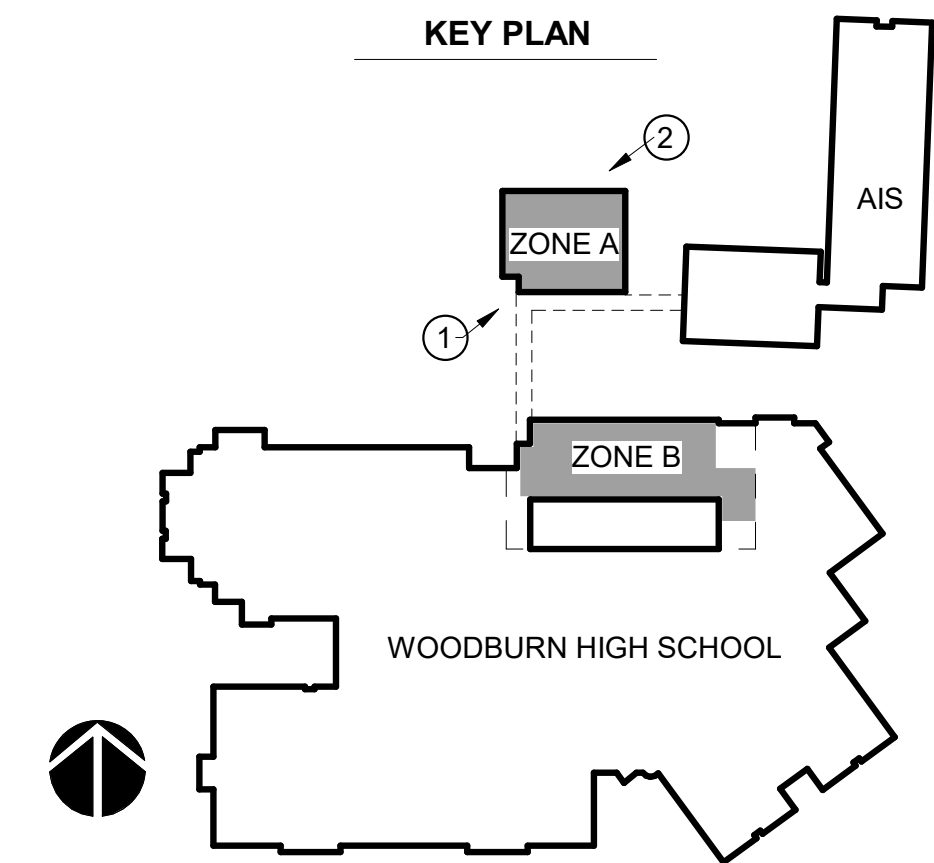
1 CLASSROOM ADDITION - MAIN ENTRY / SOUTHWEST CORNER

EXTERIOR ELEVATIONS LEGEND

NOTE: COLORS ARE SHOWN AS AN APPROXIMATION TO TRUE MATERIAL COLOR ONLY.

- 
LS-1 FIBER CEMENT - ALLURA
 COLOR: PACIFIC BLUE W/ 15% CUSTOM (BELOW)
 SMOOTH LAP SIDING - 8" LAP EXPOSURE
- CUSTOM COLORS:


- 
LS-2 FIBER CEMENT - ALLURA
 COLOR: MAHOGANY
 GRAIN LAP SIDING - 4" LAP EXPOSURE
- 
PS-1 FIBER CEMENT - ALLURA
 COLOR: CAVALRY
 SMOOTH PANEL SIDING - 4'X8' PANEL W/
 ALUMINUM REVEL



WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CONDITIONAL USE

DATE: 01/20/2020

EXTERIOR - CLASSROOM ADDITION

CU-A10

Project # 90049



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2 CTE ADDITION LOOKING NE (CTE COURTYARD)

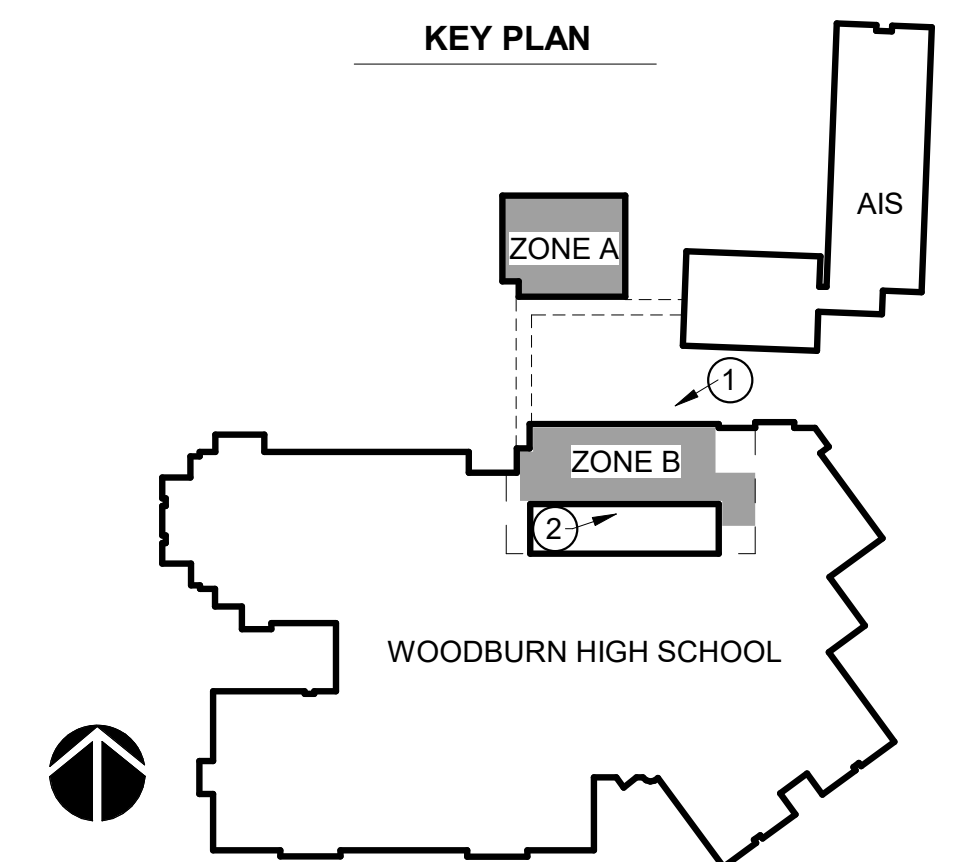


1 CTE ADDITION LOOKING SW

EXTERIOR ELEVATIONS LEGEND

NOTE: COLORS ARE SHOWN AS AN APPROXIMATION TO TRUE MATERIAL COLOR ONLY.

- LS-1 FIBER CEMENT - ALLURA
COLOR: PACIFIC BLUE W/ 15% CUSTOM (BELOW)
SMOOTH LAP SIDING - 8" LAP EXPOSURE
- CUSTOM COLORS:
- LS-2 FIBER CEMENT - ALLURA
COLOR: MAHOGANY
GRAIN LAP SIDING - 4" LAP EXPOSURE
- PS-1 FIBER CEMENT - ALLURA
COLOR: CAVALRY
SMOOTH PANEL SIDING - 4'X8' PANEL W/
ALUMINUM REVEL



WSD HS CTE & CLASSROOM ADDITION

1785 N. FRONT ST. WOODBURN, OR 97071

CONDITIONAL USE

DATE: 01/20/2020

CU-A11

EXTERIOR - CTE ADDITION

Project # 90049




LIGHTOLIER
by @ignify

Downlighting

Calculte LED 6" gen 3

C6RDL Round Downlight



Calculte LED 6" generation 3 features industry leading visual comfort, excellent uniform illumination over time, and patented installation flexibility.

Complete luminaire + Frame + Engine + Trim + Accessories (optional)

Frame example: GRNIP

Series	Installation	Voltage / Options
GR 6" Non-IC Round	N New construction	— Universal 120V/277V (specify for Power Over Ethernet) EM Emergency (see page 2 for details and limitations) IP Interact Pro ready (for 0-10V engine only)
	R Remodeler	— Universal 120V/277V (specify for Power Over Ethernet) IP Interact Pro ready (for 0-10V engine only)

Engine example: CBL1S835NZ10U

Series	Lumens	CRI	CCT	Beam	Dimming	Voltage
C6L Calculte LED 6"	10 1000lm 15 1500lm 20 2000lm 25 2500lm 30 3000lm 35 3500lm 40 4000lm	8 80CRI 9 90CRI	27 2700K 30 3000K 35 3500K 40 4000K	N Narrow (40°) M Medium (56°) W Wide (76°)	Z10 0-10V 1% SOL EIGRE LED Solo 0-10V 0.1% D Dim 0.1% L Lutron LDE1 EcoSystem (fade-to-black) DMX Digital Multiplexing	U Universal 120V/277V/347V E ELV (for up to 2000lm only) P Power over Ethernet (PoE) E Ethernet 48V DC (Only compatible with 1000-100 to 2000 lumens configurations)

Trim example: C6RDLCCP

Series	Aperture	Style	Finish	Flange
C6 Calculte LED 6"	R Round	DL Downlight	BL Black (anodized) CL Specular clear WH White (matte) WH White (matte)	W White (matte) P Polished (matches aperture) F Flangeless (requires CABRFT)


Accessories

- CABRFT** Mud-in-ring for use in flangeless trim installations (ordered with a flangeless trim)
- CAEM** Field installable EM pack (for use with new construction frame only)
- CSRVFWH** IP65 Rated vandal proof matte white accessory mounts onto flangeless trims (C4RDLF) and new construction frame only
- AMS** ActiLume multi-sensor (optional accessory for PoE configurations)
- SWZDT** SpaceWise wireless controller with dwell time functionality (compatible with all 0-10V options, see SWZDT spec sheet)
- SRAINT** Interact Office Accessory (for use with Ligholier Uniform 0-10V products)

1. Emergency (EM) frame includes emergency battery with cutting and reflector mountable test switch (see page 2 for details and limitations)
2. The 347V frame is not compatible with lumen packages above 3000lm for lensed wall wash trims.
3. Non-conductive flush mount lens with pre-installed gasket (matte white non-conductive flange with diffuse lens that is flush with the flange)

UL LISTED, IESNA, ROHS COMPLIANT, INTERACT READY

CalculteLEDgen3_6in_Downlight_C6RDL_06/19 page 1 of 7

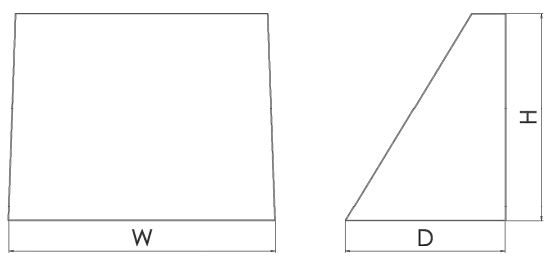


WDGE2 LED
Architectural Wall Sconce

Introduction
The WDGE2 LED family is designed to meet specifier's every wall-mounted lighting need in a widely accepted shape that blends with any architecture. The clean rectangular design comes in four sizes with lumen packages ranging from 1,200 to 25,000 lumens, providing a true site-wide solution. Embedded with nLight® AIR wireless controls, the WDGE2 family provides additional energy savings and code compliance. WDGE2 delivers up to 6,000 lumens with a soft, non-pixelated light source, creating a visually comfortable environment. When combined with multiple integrated emergency battery backup options, including an 18W cold temperature option, the WDGE2 becomes the ideal wall-mounted lighting solution for pedestrian scale applications in any environment.

Specifications

Depth: 7"
Height: 9"
Width: 11.5"
Weight (without options): 13.5 lbs



WDGE LED Family Overview

Luminaire	Standard EM, PFC	Cold EM, -20°C	Sensor	Lumens (4000K)					
				P1	P2	P3	P4	P5	P6
WDGE1 LED	4W	--	--	1,200	2,000	--	--	--	--
WDGE2 LED	10W	18W	Standalone / nLight	1,200	2,000	3,000	4,500	6,000	--
WDGE3 LED	15W	18W	Standalone / nLight	7,500	8,500	10,000	12,000	--	--
WDGE4 LED	--	--	Standalone / nLight	12,000	16,000	18,000	20,000	22,000	25,000

Ordering Information

EXAMPLE: WDGE2 LED P3 40K 80CRI VF MVOLT DDBXD

Series	Package	Color Temperature	CRI	Distribution	Voltage	Mounting	Shipped included	Shipped separately
WDGE2 LED	P1*	P1SW	27K 2700K	80CRI	VF	Visual comfort forward throw	MVOLT 347*	BBW Surface-mounted back box
	P2*	P2SW	30K 3000K	90CRI	VV	Visual comfort wide	480*	PBBW Premium surface-mounted back box (top, left, right, conduit entry)
	P4*	P4SW	35K 3500K	90CRI	VV	Visual comfort wide	480*	BBW Surface-mounted back box
	P5*	P5SW	40K 4000K	90CRI	VV	Visual comfort wide	480*	PBBW Premium surface-mounted back box (top, left, right, conduit entry)
	P6*	P6SW	50K 5000K	90CRI	VV	Visual comfort wide	480*	PBBW Premium surface-mounted back box (top, left, right, conduit entry)

Options

Options	Finish
E4WH Emergency battery backup, CEC compliant (40k, 50k)	DBRDK Dark bronze
E10WH Emergency battery backup, CEC compliant (10W, 50k)	DBLXD Black
E20WC Emergency battery backup, CEC compliant (15W, 20k)	DNALD Natural aluminum
PE* Photocell, Button Type	DWROD White
DS* Dual switching	DWROD Textured dark bronze
DMS* External 0-10V dimming (no controls)	DLBBD Textured black
DMS* Premium back box (PBBW) with bottom conduit entry (total of 4 entry points)	DNATD Textured natural aluminum
	DWNGD Textured white
	DSSTO Textured sandstone


Standalone Sensors/Controls

- PIR Motion/ambient sensor for 8-15' mounting heights
- PIRW Motion/ambient sensor for 15-30' mounting heights
- PIRIFCV Motion/ambient sensor for 8-15' mounting heights, pre-programmed to 1k and 35% light output
- PIRIFCV Motion/ambient sensor for 15-40' mounting heights, pre-programmed to 1k and 35% light output

Networked Sensors/Controls

- NLARB2 PIR nLightAIR Wireless enabled motion/ambient sensor for 8-15' mounting heights
- NLARB2 PIR nLightAIR Wireless enabled motion/ambient sensor for 15-30' mounting heights

LITHONIA LIGHTING COMMERCIAL OUTDOOR One Lithonia Way • Conyers, Georgia 30012 • Phone: 800-705-SERV (3738) • www.lithonia.com WDGE2 LED Rev. 11/13/19 © 2019 Acuity Brands Lighting, Inc. All rights reserved.

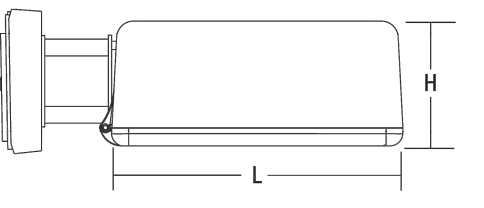


KAD LED
LED Area Luminaire

Introduction
This item is an A+ capable luminaire, which has been designed and tested to provide consistent color appearance and system-level interoperability. This luminaire is A+ Certified when ordered with DTL controls marked by a shaded background. DTL DLL equipped luminaires meet the A+ specification for luminaire to photcontrol interoperability. This luminaire is part of an A+ Certified solution for ROAM/F2 or XPaint™ Wireless control networks, providing out-of-the-box control compatibility with simple commissioning, when ordered with drivers and control options marked by a shaded background. To learn more about A+, visit www.acuitybrands.com/aplus. 1. See ordering tree for details. 2. A+ Certified Solutions for ROAM require the order of one ROAM node per luminaire. Sold Separately. Link to Roam; Link to DTL DLL.

Specifications

EPA: 1.2 ft² (0.11 m²)
Length: 17.1/2" (44.5 cm)
Width: 17.1/2" (44.5 cm)
Height: 7.1/8" (18.1 cm)
Weight (max): 3.6 lbs. (16.4 kg)



A+ Capable options indicated by this color background.

Ordering Information


EXAMPLE: KAD LED 40C 1000 40K R5 MVOLT SPD04 DDBXD

Series	LEDs	Drive current	CCT	Distribution	Voltage	Mounting	Shipped included	Shipped separately
KAD LED	20C 20 LEDs	530 530mA	30K 3000K	R2 Type II	MVOLT 277*	277*	SPBMSK Square pole universal mounting adaptor	04 4" arm
	30C 30 LEDs	700 700mA	40K 4000K	R3 Type III	120**	347**	RPMBSAK Routed pole universal mounting adaptor	06 6" arm
	40C 40 LEDs	1000 1000mA	50K 5000K	R4 Type IV	208**	480**	SPO Square pole	09 9" arm
	60C 60 LEDs	1500 1500mA	50K 5000K	R5 Type V	240**	480**	RPO Round pole	12 12" arm

Options

Options	Finish	Shipped separately
PER5 NEMA twist-lock line-wire receptacle only (no controls)	DBRDK Dark bronze	DBRDK Textured dark bronze
PER7 Seven wire receptacle only (no controls)	DBLXD Black	DBLXD Textured black
PIRIFCV Bi-level, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 1k, 35% light output	DNALD Natural aluminum	DNATD Textured natural aluminum
PIRIFCV Bi-level, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1k, 35% light output	DWROD White	DWROD Textured white
DF Double fuse (120, 277, 347V)	DLBBD Textured black	
SP Single fuse (120, 277, 347V)	DNATD Textured natural aluminum	
SR Bi-level, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 1k, 35% light output	DWNGD Textured white	
PIR Bi-level, motion/ambient sensor, 8-15' mounting height, ambient sensor enabled at 1k, 35% light output	BLSD Bi-level switched dimming, 100% light	PNMTD3 Part night dim 7/15 LED
PIRW Bi-level, motion/ambient sensor, 15-30' mounting height, ambient sensor enabled at 1k, 35% light output	BLSD Bi-level switched dimming, 50% light	PNMTD3 Part night dim 7/15 LED
		HS House-side shield*

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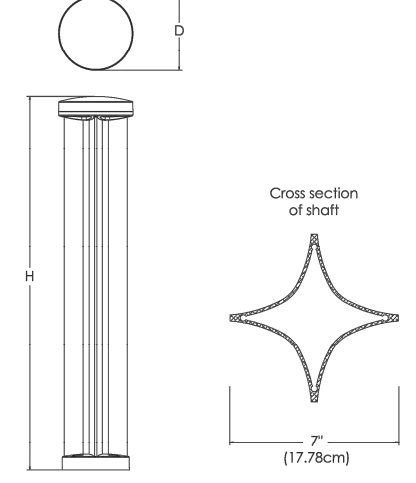


RADEAN Bollard
LED Site Luminaire

Introduction
The Radean LED Bollard is an award-winning, energy-saving, long-life solution designed to perform the way a bollard should. The Radean LED Bollard's rugged construction, durable finish and long-lasting LEDs will provide years of maintenance-free service.

Specifications

Diameter: D = 8.25" (20.95cm)
Height: H = 41.5" Standard (105.41cm)
Weight: 20lbs (9.07kg)
Max: 20lbs (9.07kg)



Ordering Information

EXAMPLE: RADB LED P4 30K SYM MVOLT BTS BCCDNATXD DBLXD

Series	Performance Package	Color temperature	Distribution	Voltage	Control options	Bollard top	Tall top
RADB LED	P1	27K 2700K	ASY Asymmetric	MVOLT 120	Shipped installed	SLM Top	TT Top
	P2	30K 3000K	SYM Symmetric	208*	PE Photocell, call button type**	BTS Slim top, painted to match shaft**	BTB Tall top, painted to match shaft**
	P3	35K 3500K	SYM Symmetric	240*	DMS 0-10V dimming (see page 2 for details)	BTSWHD Slim top, white**	BTBBLK Tall top, black**
	P4	40K 4000K	SYM Symmetric	277	EPWH Emergency battery backup, certified (ULC, ETL, IEC, NEMA, UL)	BTSWHD Slim top, white**	BTBBLK Tall top, black**
	P5	50K 5000K	SYM Symmetric	347	FAO Field adjustable output Motion sensor Bi-level***	BTSWHD Slim top, white**	BTBBLK Tall top, black**

Other options

Options	Finish
BCC Deep crown, painted to match shaft*	DBRDK Dark bronze
BCCWHD Deep crown, white*	DBLXD Black
BCCBLK Deep crown, black*	DNALD Natural aluminum
BCCDBRDK Deep crown, dark bronze textured*	DWROD White
BCCDBLXD Deep crown, black textured*	DWROD Textured dark bronze
BCCDNATXD Deep crown, natural aluminum textured*	DLBBD Textured black
BCCDNATXD Deep crown, natural aluminum textured*	DNATD Textured natural aluminum
BCCDNATXD Deep crown, natural aluminum textured*	DWNGD Textured white

Accessories

- RABAB II Anchor bolts (4)
- RABAB DBRDK II Replacement anchor bolt covers (specify finish) (4)
- RETRAB BCC1 SIBSU U Base cover with bolt caps
- RETRAB BCC1 SIBSU U Emergency test styles

Notes

- PE only available in 5M distribution.
- ASY has only two illuminated quadrants driven at higher drive currents to generate similar output as the 5M-4 quadrant product.
- PIR not available with 208V or 240V.
- FE only available with AS5.
- PE, PIR and FAO not available with BTS.
- EPWH and PIR only available in full height. Not available with 105, 105, 105.
- PIR not available with EPWH.
- Architectural and custom colors available (additional leadtimes and cost may apply).
- 42" Height is standard. H42, H30 and H36 have longer leadtimes.

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