



Staff Report

To: Planning Commission

Through: Chris Kerr, Community Development Director *C.K.*

From: Dan Handel, AICP, Associate Planner

Meeting Date: October 22, 2020 (Prepared October 15, 2020)

Item: DR 2020-05 & PLA 2020-03 “La Morenita Tortilla Bakery” at 2230 and 2400 N. Pacific Highway

Tax Lots: 051W08A004900, 5000, and 5100

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Issue before the Planning Commission

Action on a land use application, Design Review DR 2020-05 and Property Line Adjustment PLA 2020-03, for the construction of a new commercial tortilla bakery facility.

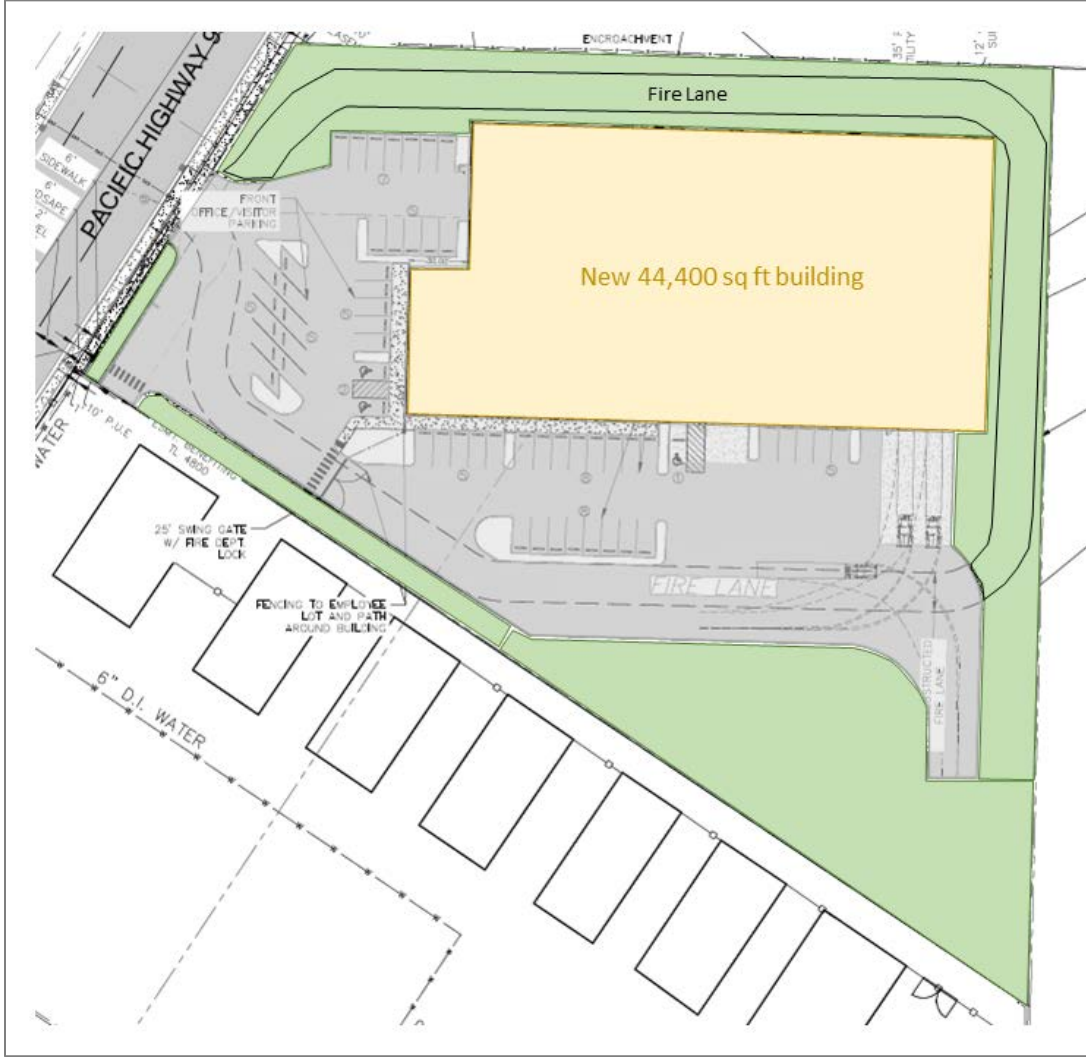
Executive Summary

The three subject properties are located at 2230 and 2400 N. Pacific Highway, northeast of the N. Pacific Highway / Mt Hood Avenue intersection. Existing land uses on the site include a recreational vehicle storage lot and a retail appliance business.

The proposal before the Planning Commission includes a consolidation of the three lots and construction of a 44,400 square foot commercial tortilla bakery facility with additional site improvements including parking lot, landscaping, and stormwater detention area. The project will also construct street frontage improvements, including landscape strip with street trees and new sidewalk, as well as extend the public 8-inch sewer line north within the highway right-of-way to the southern site boundary.



Vicinity Map (subject properties outlined in yellow)



Site Plan

Recommendation

Approval with conditions: Staff recommends that the Planning Commission consider the staff report and its attachments and approve the application with the conditions recommended by staff below. The conditions can also be found towards the end of the Analyses & Findings (Attachment 101).

Recommended Conditions of Approval

General

G-1. The applicant or successor shall develop the property in substantial conformance with the final plans submitted and approved with these applications, except as modified by these conditions of approval. Were the applicant to revise plans other than to meet conditions of approval or meet building code, even if Planning Division staff does not notice and signs off on building permit issuance, Division staff retains the right to obtain restoration of improvements as shown on an earlier land use review plan set in service of substantial conformance.

G-PW. Public Works: Follow the attached "Public Works Conditions October 15, 2020" (Attachment 101A).

Design Review

DR-1. Fencing: To meet WDO 2.06.02, the applicant shall demonstrate conformance with applicable requirements for the proposed fencing through the fence permit process of 5.01.03.

DR-2. N. Pacific Highway improvements:

- a. Per WDO 3.01.01A. and Figure 3.01B, dedicate right-of-way (ROW) along N. Pacific Highway to achieve 50 feet from centerline.
- b. Per WDO Figure 3.01B and 3.02.01C., dedicate a 10-foot public utility easement (PUE) along the widened ROW of N. Pacific Highway.
- c. Per WDO Figure 3.01B, remove the existing curbside sidewalk and construct a new 6-foot landscape strip (including curb) and 6-foot sidewalk, which shall transition to existing sidewalk along adjacent property frontages in a manner acceptable to the City Engineer.
- d. Per WDO Figure 3.01B and 3.06.06A, plant four large street trees (large tree per Table 3.06B and of a species not listed in Table 3.06C) within the new landscape strip.
- e. Subsections a. and b. are due prior to building permit issuance, subsections c. and d. are due prior to building permit final inspection.

DR-3. Additional on-site public utility easements (PUEs):

- a. Per WDO 3.02.01A., dedicate a 16-foot PUE centered along the looped 6-inch public water line.
- b. Per WDO 3.01.01C., 3.01.04A., 3.02.01C., and Transportation System Plan (TSP) Figure 6, dedicate a 35-foot PUE along the north property line of the resulting consolidated lot.
- c. Per WDO 3.04.03C.2., dedicate a 20-foot shared access easement benefitting 2200 N. Pacific Highway (Tax Lots 051W08A004700 and 4800).
- d. Subsections a., b., and c. are due prior to building permit issuance.

DR-4. Parking:

- a. Per WDO 3.05.02J., revise plans to illustrate painted directional markings on pavement or directional signage throughout the parking lot.
- b. Per WDO 3.05.02K. and Figure 3.05C, revise plans to illustrate parking stalls delineated by double parallel lines.
- c. Per WDO 3.05.03B. and Table 3.05B, revise plans to note which accessible stall is the "Van Accessible" stall.
- d. Subsections a., b., and c. are due prior to building permit issuance.

DR-5. Landscape area curbing: Per WDO 3.06.02I., revise plans to note a 6-inch curb between landscape areas and on-site pavement. This condition is due prior to building permit issuance.

DR-6. Trash enclosure: Per WDO Table 3.06D and 3.06.06A., revise plans to include drawings and/or details illustrating that the required Architectural Wall for the exterior trash enclosure is an approvable height and meets the design standards of 3.06.06B.

DR-7. Architectural design:

- a. Windows: Per 3.07.06B.2.b.(1), the west building façade shall have at least 30 percent of its area occupied by windows.
- b. Rooftop equipment: Per 3.07.06B.4., any rooftop equipment shall be screened from view from the street.

DR-8. Signage: To meet WDO 3.10, the applicant shall demonstrate conformance with applicable requirements for the proposed signage through the sign permit process of 5.01.10.

Property Line Adjustment

PLA-1. Recordation: The applicant shall record the subject lot consolidation with Marion County prior to building permit issuance.

Actions

The Planning Commission may act on the land use application to:

1. Approve per staff recommendations,
2. Approve with modified conditions, or
3. Deny, based on WDO criteria or other City provisions.

If the Planning Commission were to act upon the recommendation, staff would prepare a final decision for approval with the conditions that staff recommends.

Attachment List

101. Analyses & Findings
- 101A. Public Works Conditions October 15, 2020
102. Tax Map marked
103. Transportation System Plan Figure 2
104. Transportation System Plan Figure 6
105. Site plans, excerpted
106. Trip Generation Assessment Letter

Analyses & Findings

This attachment to the staff report analyzes the application materials and finds through statements how the application materials relate to and meet applicable provisions such as criteria, requirements, and standards. They confirm that a given standard is met or if not met, they call attention to it, suggest a remedy, and have a corresponding recommended condition of approval. Symbols aid locating and understanding categories of findings:

<i>Symbol</i>	<i>Category</i>	<i>Indication</i>
✓	Requirement (or guideline) met	No action needed
✗	Requirement (or guideline) not met	Correction needed
⊖	Requirement (or guideline) not applicable	No action needed
▲	<ul style="list-style-type: none"> Requirement (or guideline) met, but might become unmet because of condition applied to meet separate and related requirement that is not met Plan sheets and/or narrative inconsistent Other special circumstance benefitting from attention 	Revision needed for clear and consistent records

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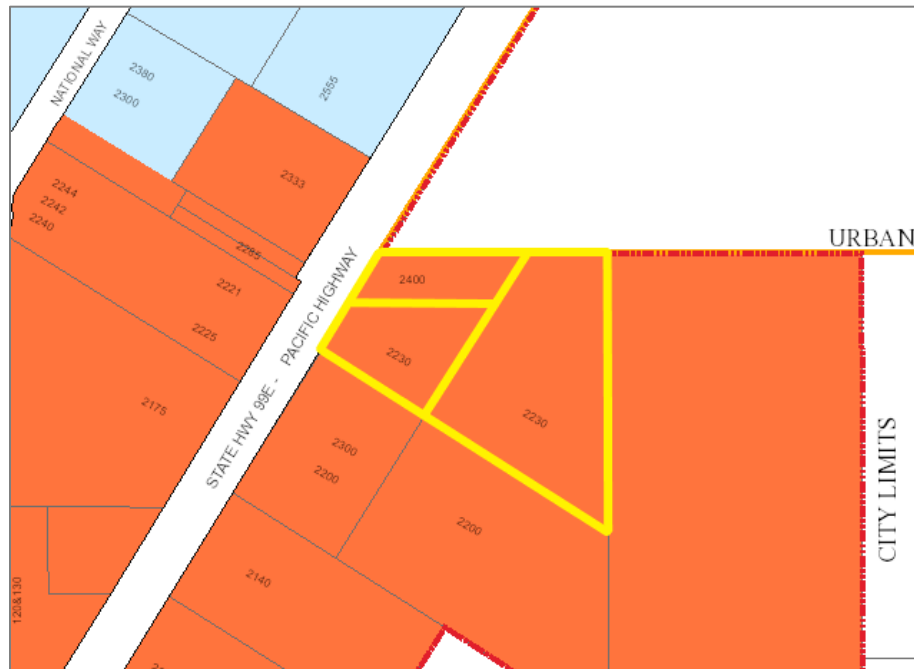
Location

<i>Addresses</i>	2230 and 2400 N. Pacific Highway
<i>Tax Lots</i>	051W08A004900, 5000, & 5100
<i>Nearest intersection</i>	N. Pacific Highway / Mt Hood Avenue / Molalla Road

Land Use & Zoning

<i>Comprehensive Plan Land Use Designation</i>	Commercial
<i>Zoning District</i>	Commercial General (CG)
<i>Overlay District(s)</i>	none
<i>Existing Use(s)</i>	Retail business; recreational vehicle (RV) storage

For context, the subject properties and adjacent zoning are illustrated and tabulated below:



Zoning Map Excerpt

<i>Cardinal Direction</i>	<i>Adjacent Zoning</i>
North	n/a – outside City Limits and Urban Growth Boundary
East	CG
South	CG
West	CG

The subject properties comprise fractions of Lots 1 and 2 of the Woodburn Fruit Farms subdivision plat recorded in 1889. Staff surmises that they are legal lots of record.

Section references on the following pages are to the [Woodburn Development Ordinance \(WDO\)](#).

Statutory Dates

The application was submitted on June 24, 2020 and deemed complete as of September 11, 2020, making the 120-day decision deadline January 9, 2021.

Design Review Provisions

5.03.02 Design Review, Type III

B. Type III Design Review is required for the following:

3. Structures greater than 2,000 square feet in the CO, CG, MUV, DDC, and NNC zones.

The proposal is a 44,400 square foot (sq ft) structure in the CG zoning district, making the DR a Type III review.

✓ The requirement is met.

1.04 Nonconforming Uses and Development

1.04.02 Change or Expansion of an Existing Use with Nonconforming Parking, Loading and/or Landscaping

Any additional parking, loading, landscaping, wall or refuse facility required by the WDO to accommodate a change in use, or expansion of an existing use shall be subject to the following:

A. For applications where the change or expansion increases the required area for parking, loading, or landscaping by 25 percent or more, all parking, loading, landscaping, buffer walls and refuse facilities shall conform to the standards of the WDO.

Existing uses of the site, which comprises three lots, include a retail appliance store and a RV storage lot. The proposed development consists of a change of use to a commercial bakery, resulting in an increase to required parking that exceeds 25 percent and requiring all parking, loading, landscaping, buffer walls, and refuse facilities to conform to current WDO standards.

✓ The requirement is met.

2.03 Commercial Zones

Uses Allowed in Commercial Zones						
Table 2.03A						
Use			Zone			
Accessory Uses (A) (P) Special Permitted Uses (S)	Conditional Uses (CU)	Permitted Uses Specific	DDC	CG	CO	MUV NNC
B	Commercial					
3	Bakeries, delicatessens, grocery and convenience stores		P ⁷	P		P P

The proposal is a commercial bakery in the CG zone, a permitted use.

✓ The requirement is met.

Commercial General (CG) - Site Development Standards Table 2.03C		
Lot Area, Minimum (square feet)		No minimum
Lot Width, Minimum (feet)		No minimum
Lot Depth, Minimum (feet)		No minimum
Street Frontage, Minimum (feet)		No minimum
Front Setback and Setback Abutting a Street, Minimum (feet)		5¹
Side or Rear Setback, Minimum (feet)	Abutting CO, CG, DDC, NNC, P/SP, IP, SWIR, or IL zone	0 or 5^{4,5}
Setback to a Private Access Easement, Minimum (feet)		5
Lot Coverage, Maximum		Not specified²
Building Height, Maximum (feet)	Primary or accessory structure	70
	Outside Gateway subarea	70
		100
1. Measured from the Special Setback (Section 3.03.02), if any 2. Lot coverage is limited by setbacks, off-street parking, and landscaping requirements. 4. A house of worship shall be set back at least 20 feet from a property line abutting a residential zone or use. 5. A building may be constructed at the property line, or shall be set back at least five feet.		

The CG zone has no minimum lot area, width, depth, street frontage, or maximum lot coverage. Considering the proposed consolidation of the three subject properties, the front setback will be in excess of 100 feet, the north side setback will be in excess of 30 feet, the south side setback will be in excess of 70 feet, the rear setback will be approximately 40 feet, and the setback to the private access easement will be in excess of 80 feet. The building height is proposed to be 24 feet.

✓ The provisions are met.

2.05 Overlay Districts

⊖ None apply.

2.06 Accessory Structures

2.06.01 Applicability

The following standards are applicable to accessory structures in all zones. Accessory Dwelling Units are exempt from these standards and are subject to the provisions of Section 2.07.20.

2.06.02 Fences and Walls

C. Height in Non-Residential Zones

1. In commercial, industrial, or public zones, the maximum height of a fence or wall located in a yard abutting a street shall be 6 feet, relative to the ground elevation under the fence or wall. Fence height may increase to 9 feet once flush with the building face, or 20 feet from street right-of-way.

D. Fence Materials

2. Chain link fences are acceptable as long as the fence is coated and includes slats made of vinyl, wood or other durable material. Slats may not be required when visibility into features such as open space, natural areas, parks and similar areas is needed to assure visual security, or into on-site areas in industrial zones that require visual surveillance.

Sheet C3 notes chain link fencing around the perimeter of the property. Because 5.01.03 requires a fence permit for any new fencing, staff applies *Condition of Approval DR-1* to confirm conformance through such permit, which is separate from and may come after land use review.

▲ In order to secure conformance with fence regulations, staff applies *Condition DR-1*.

2.07 Special Uses

– None apply.

2.08 Specific Conditional Uses

– None apply.

3.01 Streets

3.01.01 Applicability

A. Right-of-way standards apply to all public streets.

B. Improvement standards apply to all public and private streets, sidewalks and bikeways.

C. Functional standards are identified in the Woodburn TSP.

D. This applies to all development, and is not limited to partitions, subdivisions, multi-family, commercial or industrial construction, or establishment of a manufactured dwelling or recreational vehicle park. Construction of a single-family dwelling or placement of a manufactured dwelling does not, for the purposes of this Section, constitute development, however, in no case can this type of development occur without minimal access as determined by the Director.

3.01.02 General Provisions

A. No development shall be approved, or access permit issued, unless the internal streets, boundary streets and connecting streets are constructed to at least the minimum standards set forth in this Section, or are required to be so constructed as a condition of approval.

3.01.03 Improvements Required for Development

A. With development, the Internal, Boundary, and Connecting streets shall be constructed to at least the minimum standards set forth below.

C. Boundary Streets

The minimum improvements for a Boundary Street shall be:

1. One paved 11-foot travel lane in each direction;
2. On-street parking on the side of the street abutting the development, if on-street parking is indicated in the TSP;
3. Curb on the side of the street abutting the development;
4. Drainage facilities on the side of the street abutting the development;
5. Street trees on the side of the street abutting the development; and
6. A sidewalk on the side of the street abutting the development.

3.01.04 Street Cross-Sections

A. These standards are based on the functional classification of each street as shown in the Woodburn TSP. The street right-of-way and improvement standards minimize the amount of pavement and right-of-way required for each street classification, consistent with the operational needs of each facility, including requirements for pedestrians, bicycles, and public facilities.

B. All public streets under the jurisdiction of the City of Woodburn shall comply with the cross-sections depicted in this Section.

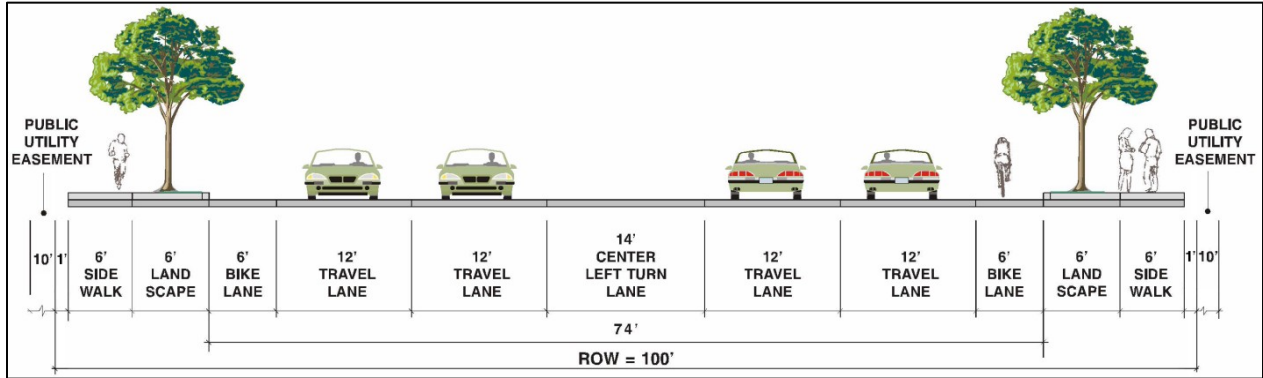


Figure 3.01B – Major Arterial

The proposal includes construction of a 44,400 sq ft commercial bakery therefore the right-of-way (ROW) standards, improvement standards, and function standards of the TSP apply.

N. Pacific Highway

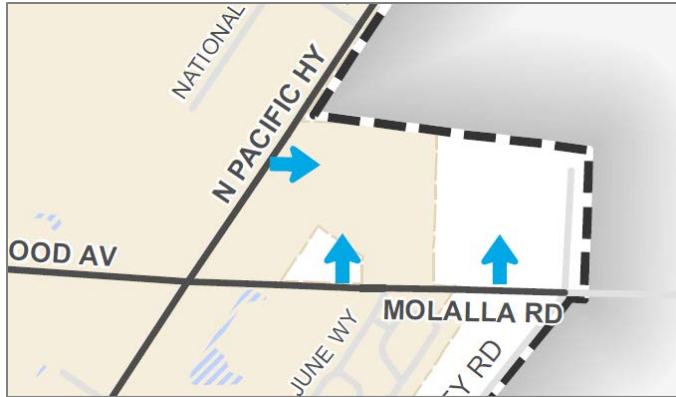
The site abuts N. Pacific Highway, a public street. Transportation System Plan (TSP) Figure 2 classifies N. Pacific Highway as a Major Arterial street, for which the corresponding WDO cross-section is Figure 3.01B.

Existing conditions of N. Pacific Highway along the site's frontage include a variable ROW width that appears to be between 87 and 95 feet, four travel lanes, a center turn lane, a bike lane, curb and drainage facilities, and a curbside sidewalk (no landscape strip). Additionally, City records do not indicate the existence of a public utility easement (PUE) along the frontage.

Staff applies *Condition of Approval DR-2* to dedicate ROW to achieve 50 ft from centerline, dedicate a 10-ft PUE along widened ROW, remove existing curbside sidewalk, and construct new 6-ft landscape strip with street trees and 6-ft sidewalk.

Future local street connection

Sections 3.01.01C. and 3.01.04A. reference the TSP and its functional classifications for streets. In addition to classifying existing streets, the TSP also identifies future street locations and their classifications. For example, TSP Figure 6 outlines future Local street connections throughout the city by illustrating arrows to show the approximate location of where a connection would begin and its direction.



Excerpt of TSP Figure 6

As can be seen in TSP Figure 6, a blue arrow pointing eastward appears to cover the subject properties. Considering this blue arrow in terms of regional connectivity, the City has clearly indicated its intention to construct several Local streets in the area northeast of the N. Pacific Highway / Mt Hood Avenue / Molalla Road intersection. Figure 6 is not intended as a construction document that illustrates an exact location of a future Local street. Rather, as development or redevelopment of properties in this area occur, staff is obligated to consider these identified Local street connections and how they would fit in with the proposed development. WDO Figure 3.01G is the default cross-section for local streets.

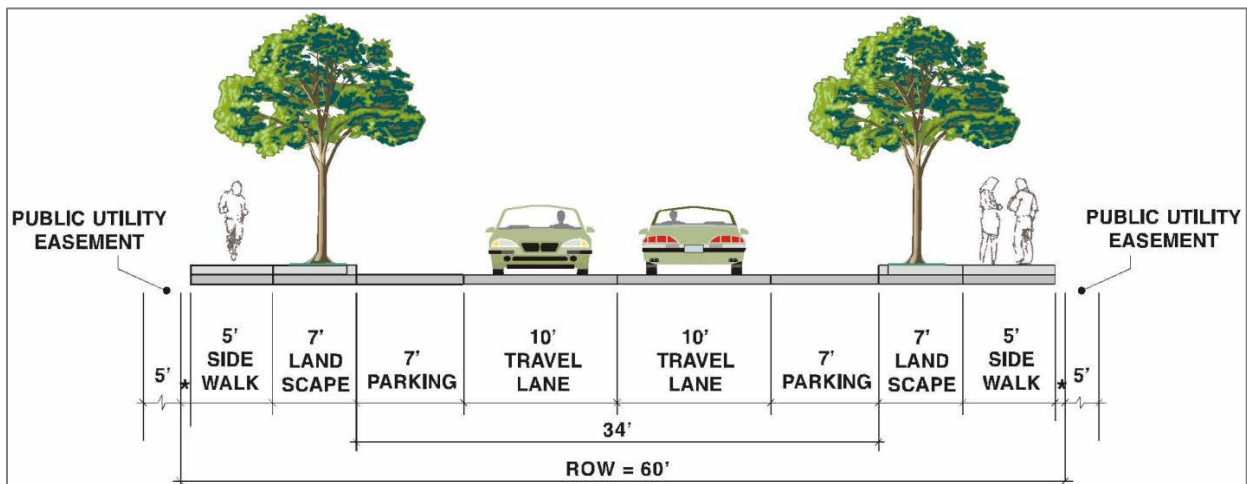


Figure 3.01G – Local Residential Street with Parking Both Sides, 60 Foot Right-of-Way

Based on these facts, staff considered the subject properties and neighboring properties to determine the most feasible path for the future east/west Local street. Siting the street along the north property boundary of the resulting subject property (after lot consolidation) was the most logical solution because such an alignment could be continued straight east across the neighboring properties and could connect with the additional northward future Local streets coming from Molalla Road.

The property owner of this proposal would therefore be responsible for a “half-street” portion of the land needed for the future Local street, which is 30 feet plus a 5-foot PUE. As the future Local street is not yet a planned Capital Improvement Project, staff directed the applicant to dedicate a 35-foot PUE along the north property line of the consolidated lot to ensure the land remains free of structures and buildings, but could still be used by the property owner in the meantime.

Staff addresses this topic here in detail because a significant portion of the applicant’s narrative argues against the 35-ft PUE. Staff adds *Condition of Approval DR-3b* to confirm this PUE is required.

▲ The provisions are met with *Conditions DR-2* and *DR-3b*.

3.02 Utilities & Easements

3.02.01 Public Utility Easements

A. The Director shall require dedication of specific easements for the construction and maintenance of municipal water, sewerage and storm drainage facilities located on private property.

B. A five-foot wide public utility easement shall be dedicated along each lot line abutting a public street.

C. As a condition of approval for development, including property line adjustments, partitions, subdivisions, design reviews, or Planned Unit Developments (PUDs), the Director may require dedication of public utility easements.

The proposal includes a 6-inch looped public water line around the building, which requires a 16-ft PUE centered along the path of the line. Staff adds *Condition of Approval DR-3a* to dedicate this easement.

As noted in the analysis for 3.01, *Condition of Approval DR-2b* requires dedication of a 10-ft PUE along the widened ROW and *Condition of Approval DR-3b* requires dedication of a 35-ft PUE along the north property line of the resulting consolidated lot.

▲ The provisions are met with *Conditions DR-2b* and *DR-3*.

3.02.03 Street Lighting

A. Public Streets

Public streets abutting a development shall be illuminated with street lights installed to the standards of the City and the electric utility.

Street lighting requirements are included in the Public Works Department conditions. Staff therefore adds *Condition of Approval G-PW* to meet the street lighting requirements.

▲ The provision is met with *Condition G-PW*.

3.03 Setbacks and Open Space

The proposal complies with setbacks and does not encroach into vision clearance areas.

✓ The provisions are met.

3.04 Vehicular Access

3.04.01 Applicability and Permit

A. Street Access

Every lot shall have:

1. Direct access to an abutting public street, or

The proposal includes a lot consolidation to consolidate the three subject properties into a single lot that would abut N. Pacific Highway, a public street.

✓ The provision is met.

3.04.03 Driveway Guidelines and Standards

A. Number of Driveways

3. For nonresidential uses, the number of driveways should be minimized based on overall site design, including consideration of:

a. The function classification of abutting streets;

b. The on-site access pattern, including parking and circulation, joint access, turnarounds and building orientation;

c. The access needs of the use in terms of volume, intensity and duration characteristics of trip generation.

B. Joint Access

1. Lots that access a Major Arterial, Minor Arterial, or Service Collector should be accessed via a shared driveway.

3. Every joint driveway or access between separate lots shall be established by an access easement and maintenance agreement to the satisfaction of the Director and revocable only with the concurrence of the Director.

C. Interconnected Parking Facilities

2. Similar or compatible uses on abutting lots shall have interconnected access and parking facilities.

Access Requirements Table 3.04A		
		Commercial or Industrial Use
Paved Width of Driveway (feet) ^{3,4}	2-way	24 minimum 36 maximum (Add 8' if a turn lane is provided)
Curb Flare Radius (feet)		30 minimum
Throat Length (feet)	Major Arterial, Minor Arterial, Service Collector	50 minimum
Driveway Separation Guidelines (feet) ^{1,2} (See Figure 3.04B)	Major Arterial	300 minimum
<p>3. Driveways over 40 feet long and serving one dwelling unit may have a paved surface 12 feet wide.</p> <p>4. Notwithstanding the widths listed in this table, the minimum clearance around a fire hydrant shall be provided (See Figure 3.04D).</p>		

3.04.04 Improvement Standards

The portion of a driveway on private property shall be paved with:

- A. Portland cement concrete to a minimum depth of six inches, or
- B. Asphalt concrete to a minimum depth of two inches, or
- C. Brick or pavers with a minimum depth of two and one-fourth inches.

As illustrated on Sheet C2, the site currently has two driveway access points onto N. Pacific Highway. The proposal illustrates on Sheet C3 the removal of the two existing driveways and the construction of a single, new driveway centered along the frontage and aligned with a driveway on the west side of the highway. This new driveway is 36 ft wide and paved with asphalt concrete.

Supporting the Highway 99E Corridor Plan, the proposal also includes a shared access point with the adjacent property to the south along the shared property line. This includes a 20-ft shared access easement and drive aisle improvements along the length of the easement. Staff adds *Condition of Approval DR-3c* to dedicate this shared access easement prior to building permit issuance.

▲ The provisions are met with *Condition DR-3c*.

3.04.05 Traffic Impact Analysis

A. A Traffic Impact Analysis (TIA) may be required by the Director prior to the approval of a City access permit when the Director estimates a development proposal may generate either 100 or more additional, peak hour trips, or 1,000 or more additional daily trips, within ten years of a development application.

The applicant's Trip Generation Assessment Letter (Attachment 106) indicated that the proposal would not generate over 100 peak hour trips nor over 1,000 daily trips.

⊖ The provision is not applicable.

3.05 Off-Street Parking and Loading

3.05.02 General Provisions

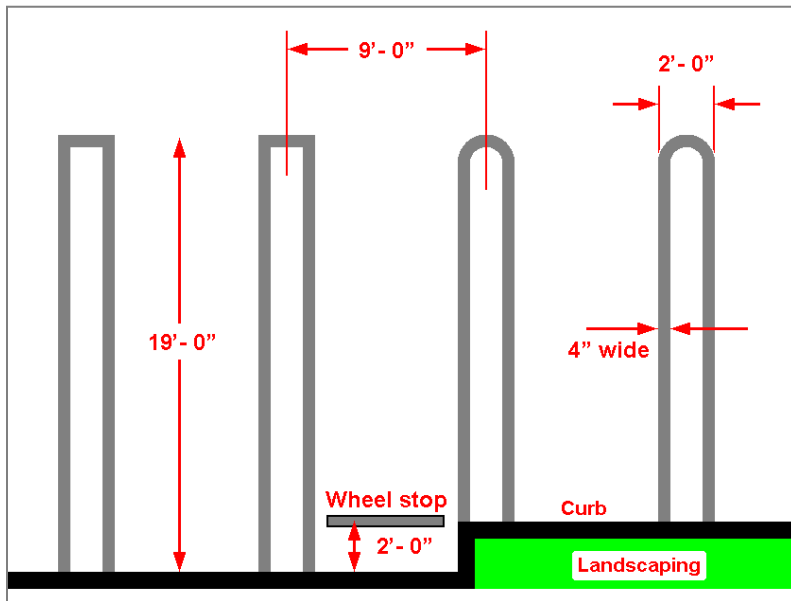


Figure 3.05C - Parking Space Striping

The plans show generally what these provisions require, except no directional markings or signs are illustrated per 3.05.02J. and stalls are not illustrated to be delineated by double parallel lines per 3.05.02K. and Figure 3.05C. Staff therefore adds *Conditions of Approval DR-4a* and *DR-4b* to meet these provisions.

▲ The provisions are met with *Conditions DR-4a* and *DR-4b*.

3.05.03 Off-Street Parking

A. Number of Required Off-Street Parking Spaces

1. Off-street vehicle parking spaces shall be provided in amounts not less than those set forth in this Section (Table 3.05A).

2. Off-street vehicle parking spaces shall not exceed two times the amount required in this Section (Table 3.05A).

B. Accessible parking shall be provided in amounts not less than those set forth in Table 3.05B. The number of accessible spaces shall be included as part of total required vehicle parking spaces.

C. A maximum of 20 percent of the required vehicle parking spaces may be satisfied by compact vehicle parking spaces.

D. Off-street vehicle parking spaces and drive aisles shall not be smaller than specified in this Section (Table 3.05C).

E. All uses that are required to provide 10 or more off-street parking spaces and residential structures with four or more dwelling or living units shall provide a bicycle rack within 50 feet of the main building entrance. The number of required rack spaces shall be one space per ten vehicle parking spaces, with a maximum of 20 rack spaces.

Off-Street Parking Ratio Standards Table 3.05A	
Use ¹	Parking Ratio - spaces per activity unit or square feet of gross floor area
7. General retail sales (such as food and beverages, clothing, sporting goods, health and personal care items, and motor vehicle parts)	1/250 square feet
22. Commercial Bakery	Greater of 1/800 square feet or 1/employee
55. Warehousing	Greater of: a. 1/ 5000 square feet (0 to 49,999 square feet) b. 10 plus 1/ 10,000 square feet over 50,000 (50,000 to 99,999 square feet) c. 15 plus 1/ 15,000 square feet over 100,000 (100,000 square feet or more) or 1/ employee
1. The Director may authorize parking for any use not specifically listed in this table. The applicant shall submit an analysis that identifies the parking needs, and a description of how the proposed use is similar to other uses permitted in the zone. The Director may require additional information, as needed, to document the parking needs of the proposed use.	

Accessible Parking Ratio Standards Table 3.05B			
Total Spaces	Minimum Total Accessible Spaces ¹	Minimum Van Accessible Spaces	Minimum "Wheelchair User Only" Spaces
51 to 75	3	1	
1. "Van Accessible Spaces" and "Wheelchair User Only" are included in "Total Accessible Spaces."			

The proposal includes 374 sq ft of wholesale retail space, 16,009 sq ft of commercial bakery space (including offices and conference areas), and 28,017 sq ft of warehouse space. The Trip Generation Assessment Letter notes 13 employees for the morning shift and 8 employees for the afternoon shift, totaling 21 employees. This results in 1 required parking stall for the wholesale retail space, 21 required parking stalls for the commercial bakery space, and 21 required parking stalls for the warehouse space.

The plans note a total of 56 parking stalls provided, 3 of which are accessible stalls, and 6 bicycle parking stalls. Staff adds *Condition of Approval DR-4c* to note that at least one of the three accessible stalls is to be “Van Accessible”.

▲ The provisions are met with *Condition DR-4c*.

Parking Space and Drive Aisle Dimensions Table 3.05C							
Parking Angle	Type of Space	Stall Width (feet)	Curb Length (feet)	Stripe Length (feet)	Stall to Curb (feet)	Drive Aisle Width (feet)	
						1-way	2-way
A		B	C	D	E	F	G
45°	Standard or Accessible	9.0	12.7	28	19.8	15.0	24.0
	Compact	7.5	10.6	22.5	15.9		
	Car Accessible Aisle	6.0	8.5	25.0	17.7		
	Van Accessible Aisle	8.0	11.3	27.0	19.1		
90°	Standard or Accessible	9.0	9.0	19.0	19.0	24.0	24.0
	Compact	7.5	7.5	15.0	15.0	22.0	
	Car Accessible Aisle	6.0	6.0	19.0	19.0	24.0	
	Van Accessible Aisle	8.0	8.0	19.0	19.0		

1. A parking space may occupy up to two feet of a landscaped area or walkway. At least four feet clear width of a walkway must be maintained.
2. Space width is measured from the midpoint of the double stripe.
3. Curb or wheel stops shall be utilized to prevent vehicles from encroaching on abutting properties or rights-of-way.
4. The access aisle must be located on the passenger side of the parking space, except that two adjacent parking spaces may share a common access aisle.
5. Where the angle of parking stalls differ across a drive aisle, the greater drive aisle width shall be provided.

The plans show parking stalls and drive aisles in conformance with these dimensional requirements.

✓ The provisions are met.

3.05.04 Off-Street Loading

A. Off-street loading spaces shall comply with the dimensional standards and amounts not less than those set forth in this Section (Table 3.05D).

B. The off-street loading facilities shall be on the same lot, or site, as the use or structure they are intended to serve. Required loading spaces and required parking spaces shall be separate and distinct,

except that if authorized through a land use decision, a parking area may be used for loading during those times when the vehicle parking area is not in use.

Loading Space Requirements Table 3.05D				
Use and Area (square feet)	Minimum Number of Spaces	Minimum Size of Space (feet)		
		Width	Length	Height
Nonresidential uses, except office, in the CO, CG, and NNC zones				
0 – 9,999	1	12	30	14
10,000 – 41,999	2			
42,000 – 81,999	3			
82,000 or more	4			

The proposal includes 3 loading spaces meeting the minimum dimension requirements.

✓ The provisions are met.

3.06 Landscaping

3.06.02 General Requirements

The landscape plans illustrate the general provisions are met, except the curbing requirement per 3.06.02I. Staff adds *Condition of Approval DR-5* to address this requirement.

▲ The provisions are met with *Condition DR-5*.

3.06.03 Landscaping Standards

3.06.03A Street Trees

Within the public street right-of-way abutting a development, street trees shall be planted to City standards, prior to final occupancy.

1. One tree per every entire 50 feet of street frontage shall be planted within the right-of- way, subject to vision clearance area standards and placement of public utilities.
2. Street trees shall be planted according to the property’s zoning, and the abutting street’s classification in the Transportation System Plan:
 - a. Large trees shall be planted along Major and Minor Arterial streets. Large trees shall also be planted along all streets in the Neighborhood Conservation Overlay District (NCOD), regardless of street classification;
- B. Site landscaping shall comply with Table 3.06A.

Planting Requirements Table 3.06A		
Location	Planting Density, Minimum	Area to be Landscaped, Minimum
Setbacks abutting a street	1 PU/15 square feet	Entire setback excluding driveways
Buffer yards	1 PU/20 square feet	Entire yard excluding off-street parking and loading areas abutting a wall
Other yards	1 PU/50 square feet	Entire yard, excluding areas subject to more intensive landscaping requirements and off-street parking and loading areas
Off-street parking and loading areas	<ul style="list-style-type: none"> • 1 small tree per 10 parking spaces; or¹ • 1 medium tree per 15 parking spaces; or¹ • 1 large tree per 25 parking spaces¹ and <ul style="list-style-type: none"> • 1 PU/20 square feet excluding required trees² 	<ul style="list-style-type: none"> • RS, R1S, RSN, RM, RMN, P/SP, CO, CG and MUV zones: 20% of the paved surface area for off-street parking, loading and circulation • DDC, NNC, IP, IL, and SWIR zones: 10% of the paved surface area for off-street parking, loading and circulation • Landscaping shall be within or immediately adjacent to paved areas
Common areas, except those approved as natural common areas in a PUD	3 PU/50 square feet	Entire common area
<ol style="list-style-type: none"> 1. Trees shall be located within off-street parking facilities, in proportion to the distribution of the parking spaces. 2. Required landscaping within a setback abutting a street or an interior lot line that is within 20 feet of parking, loading and circulation facilities may also be counted in calculating landscaping for off-street parking, loading and circulation areas. 		

Plant Unit (PU) Value Table 3.06B		
Material	Plant Unit (PU) Value	Minimum Size
1. Significant tree ¹	15 PU each	24" Diameter
2. Large tree (60-120 feet high at maturity) ¹	10 PU each	10' Height or 2" Caliper
3. Medium tree (40-60 feet high at maturity) ¹	8 PU each	10' Height or 2" Caliper
4. Small tree (18-40 feet high at maturity) ¹	4 PU each	10' Height or 2" Caliper



Plant Unit (PU) Value Table 3.06B		
Material	Plant Unit (PU) Value	Minimum Size
5. Large shrub (at maturity over 4' wide x 4' high) ¹	2 PU each	3 gallon or balled
6. Small to medium shrub (at maturity maximum 4' wide x 4' high) ¹	1 PU each	1 gallon
7. Lawn or other living ground cover ¹	1 PU / 50 square feet	
8. Berm ²	1 PU / 20 lineal feet	Minimum 2 feet high
9. Ornamental fence ²	1 PU / 20 lineal feet	2½ - 4 feet high
10. Boulder ²	1 PU each	Minimum 2 feet high
11. Sundial, obelisk, gnomon, or gazing ball ²	2 PU each	Minimum 3 feet high
12. Fountain ²	3 PU each	Minimum 3 feet high
13. Bench or chair ²	0.5 PU / lineal foot	
14. Raised planting bed constructed of brick, stone or similar material except CMU ²	0.5 PU / lineal foot of greatest dimension	Minimum 1 foot high, minimum 1 foot wide in least interior dimension
15. Water feature incorporating stormwater detention ²	2 per 50 square feet	None
1. Existing vegetation that is retained has the same plant unit value as planted vegetation. 2. No more than twenty percent (20%) of the required plant units may be satisfied by items in lines 8 through 15.		

The landscape plan illustrates 4 large street trees along the 200 ft of frontage. The plan also notes a landscaping requirement of 849 Plant Units (PUs) and 1,900 PUs provided.

✓ The requirements are met.

3.06.05 Screening

A. Screening between zones and uses shall comply with Table 3.06D.

Screening Requirements Table 3.06D	
N = No screening required F = Sight-obscuring fence required W = Architectural wall required D = Architectural wall, fence, or hedge may be required in the Design Review process	
Adjacent properties – zone or use that receives the benefit of screening 	CG or MUV zone
Property being Developed – must provide screening if no comparable screening exists on abutting protected property 	
CG or MUV zone	D
Refuse and recycling collection facilities except for single-family dwelling, duplex, child care	W ^{2,6,7}
2. Six to seven feet in height 6. In industrial zones, screening is required only where the refuse collection facility is in a yard abutting a public street, parking lot, or residentially zoned property. 7. Child care facility for 12 or fewer children, group home for five or fewer persons. Child care facility for 13 or more children, group home for six or more persons.	

B. All parking areas, except those for single-family and duplex dwellings, abutting a street shall provide a 42-inch vertical visual screen from the abutting street grade. Acceptable design techniques to provide the screening include plant materials, berms, architectural walls, and depressed grade for the parking area. All screening shall comply with the clear vision standards of this ordinance (Section 3.03.06).

3.06.06 Architectural Walls

A. This Section shall apply to required architectural walls in all zoning districts.

B. Design Standards and Guidelines

1. An architectural wall shall meet the texture, color, and articulation requirements on the face away from the proposed development.
2. An architectural wall should meet the texture, color, and articulation requirements on the face toward the proposed development.
3. An architectural wall shall have a minimum three inch horizontal articulation of at least one linear foot of the wall of intervals not more than 40 feet; and
4. An architectural wall shall have a minimum six inch vertical articulation of at least one linear foot of the wall of intervals not more than 40 feet.
5. An architectural wall shall incorporate at least two colors.

6. An architectural wall shall have an earth tone coloration other than grey on at least eighty percent (80%) of the surface.

7. An architectural wall shall be architecturally treated with scoring, texture, or pattern on at least eighty percent (80%) of the surface.

No screening is required between CG-zoned properties. The proposal includes an exterior refuse collection facility, which is required to be contained by an Architectural Wall at 6 - 7 ft in height. Staff adds *Condition of Approval DR-6* to address the design criteria for the Architectural Wall.

▲ The provisions are met with *Condition DR-6*.

3.07 Architectural Design

3.07.01 Applicability of Architectural Design Standards and Guidelines

A. For a Type I review, the criteria of this Section shall be read as “shall” and shall be applied as standards. For a Type II or III review, the criteria of this Section shall be read as “should” and shall be applied as guidelines.

3.07.06 Standards for Non-Residential Structures in Residential, Commercial and Public/Semi-Public Zones

A. The following design guidelines shall be applicable to all non-residential structures and buildings in the RS, RSN, R1S, RM, RMN, CO, CG, and P/SP zones.

B. Architectural Design Guidelines

1. Mass and Bulk Articulation Guidelines

a. Building facades visible from streets and public parking areas should be articulated, in order to avoid the appearance of box-like structures with unbroken wall surfaces.

b. The appearance of exterior walls should be enhanced by incorporating three-dimensional design features, including the following:

(1) Public doorways or passage ways through the building

(2) Wall offsets or projections

(3) Variation in building materials or textures

(4) Arcades, awnings, canopies or porches

2. Materials and Texture Guidelines

a. Building exteriors should exhibit finishes and textures that reduce the visual monotony of bulky structures and large structural spaces. Building exteriors should enhance visual interest of wall surfaces and harmonize with the structural design.

b. The appearance of exterior surfaces should be enhanced by incorporating the following:

(1) At least 30% of the wall surface abutting a street should be glass.

(2) All walls visible from a street or public parking area should be surfaced with wood, brick, stone, designer block, or stucco, or with siding that has the appearance of wood lap siding.

(3) The use of plain concrete, plain concrete block, corrugated metal, plywood, T-111 and sheet composite siding as exterior finish materials for walls visible from a street or parking area should be avoided.

(4) The color of at least 90 percent of the wall, roof and awning surface visible from a street or public parking area should be an “earth tone” color containing 10 parts, or more of brown or a “tinted” color, containing 10 parts or more white.

(5) Fluorescent, “day-glo,” or any similar bright color shall not be used on the building exterior.

3. Multi-Planed Roof Guidelines

a. The roof line at the top of a structure should establish a distinctive top to the building.

b. The roof line should not be flat or hold the same roof line over extended distances. Rather, the roof line should incorporate variations, such as:

(1) Offsets or jogs in the plane of the roof;

(2) Changes in the height of the exterior wall for flat roof buildings, including parapet walls with variations in elevation or cornices

4. Roof-Mounted Equipment Guidelines

All roof-mounted equipment, except solar collectors, should be screened from view by:

a. Locating roof-mounted equipment below the highest vertical element of the building, or

b. Screening roof-mounted equipment using materials of the same character as the structure’s basic materials

5. Weather Protection Guidelines

All building faces abutting a street or a public parking area should provide weather protection for pedestrians. Features to provide this protection should include:

a. A continuous walkway at least eight feet wide along the face of the building utilizing a roof overhang, arcade, awnings or canopies

b. Awnings and canopies that incorporate the following design features:

(1) Angled or curved surfaces facing a street or parking area

(2) A covering of fabric, or matte finish vinyl

(3) A constant color and pattern scheme for all buildings within the same development

(4) No internal back lighting

6. Solar Access Protection

Obstruction of existing solar collectors on abutting properties by site development should be minimized.

C. Building Location Guidelines

1. Within the prescribed setbacks, building location and orientation should compliment abutting uses and development patterns.

2. The maximum yard abutting a street should be 150 feet.

The proposal is a Type III Design Review. The elevations show largely what the provisions describe.

Notably, the applicant’s Incompleteness Letter Response received on August 12 outlines an argument regarding the window requirement of 3.07.06B.2.b.(1). The applicant argues that the proposed building does not directly abut the N. Pacific Highway ROW therefore this standard should not apply. Staff responds by considering the required setback abutting a street in the CG zone, a 5-ft minimum per Table 2.03C. This site development standard does not allow any building to abut a ROW therefore the Community Development Director has interpreted the intent of 3.07.06B.2.b.(1) to refer to building facades facing and visible from a street. Further, the proposal is construction of a new building and there is no reason the provision cannot be met. To that end, staff adds *Condition of Approval DR-7a* to confirm the 30 percent window coverage provision applies to the proposal.

The applicant's narrative indicates there will be no roof-mounted equipment. In the event that this changes between land use approval and building permit issuance, staff adds *Condition of Approval DR-7b*.

▲ The provisions are met with *Condition DR-7*.

3.08 Partitions and Subdivisions

⊖ The proposal does not include a partition or subdivision.

3.09 Planned Unit Developments

⊖ The proposal does not include a planned unit development.

3.10 Signs

The applicant indicated in plans and in the narrative that the proposal will incorporate various signage. Because 5.01.10 requires a sign permit for any new signage, staff applies *Condition of Approval DR-8* to confirm conformance through such permit, which is separate from and may come after land use review.

▲ In order to secure conformance with sign regulations, staff applies *Condition DR-8*.

Property Line Adjustment Provisions

5.01.08 Property Line Adjustment; Consolidation of Lots

A. Purpose: The purpose of this review is to ensure that adjustments to property lines or the consolidation of existing lots and parcels, complies with the standards of this ordinance (Section 2), and State Statutes (ORS Chapters 92 and 209). Property line adjustments and consolidation of lots are allowed in all zones.

B. Criteria:

- 1. Lot area, depth, width, frontage, building setbacks, vehicular access and lot coverage comply with the standards of this ordinance (Sections 2 and 3);**
- 2. Existing easements are accurately reflected;**
- 3. Existing land use and development on the subject property comply with the requirements of prior land use actions; and**
- 4. Buildings and structures abutting the adjusted property lines comply with State building codes and with respect to current occupancy.**
- 5. Property line adjustments are surveyed and monumented to the requirements set forth in State statutes (ORS Chapters 92 and 209) and recorded by the County Surveyor.**

The proposal includes a lot consolidation for the three subject properties. Lot area, depth, width, frontage, building setbacks, vehicular access, and lot coverage were analyzed throughout this staff report and found to be in conformance with requirements. Existing easements were accurately reflected. Staff is not aware of any relevant requirements from prior land use actions.

Criterion 5 requires recordation of the property line adjustment. To that end, staff adds *Condition of Approval PLA-1* to complete recordation of the lot consolidation prior to building permit issuance.

▲ The provisions are met with *Condition PLA-1*.

Recommended Conditions of Approval

General

G-1. The applicant or successor shall develop the property in substantial conformance with the final plans submitted and approved with these applications, except as modified by these conditions of approval. Were the applicant to revise plans other than to meet conditions of approval or meet building code, even if Planning Division staff does not notice and signs off on building permit issuance, Division staff retains the right to obtain restoration of improvements as shown on an earlier land use review plan set in service of substantial conformance.

G-PW. Public Works: Follow the attached "Public Works Conditions October 15, 2020" (Attachment 101A).

Design Review

DR-1. Fencing: To meet WDO 2.06.02, the applicant shall demonstrate conformance with applicable requirements for the proposed fencing through the fence permit process of 5.01.03.

DR-2. N. Pacific Highway improvements:

- a. Per WDO 3.01.01A. and Figure 3.01B, dedicate right-of-way (ROW) along N. Pacific Highway to achieve 50 feet from centerline.
- b. Per WDO Figure 3.01B and 3.02.01C., dedicate a 10-foot public utility easement (PUE) along the widened ROW of N. Pacific Highway.
- c. Per WDO Figure 3.01B, remove the existing curbside sidewalk and construct a new 6-foot landscape strip (including curb) and 6-foot sidewalk, which shall transition to existing sidewalk along adjacent property frontages in a manner acceptable to the City Engineer.
- d. Per WDO Figure 3.01B and 3.06.06A, plant four large street trees (large tree per Table 3.06B and of a species not listed in Table 3.06C) within the new landscape strip.
- e. Subsections a. and b. are due prior to building permit issuance, subsections c. and d. are due prior to building permit final inspection.

DR-3. Additional on-site public utility easements (PUEs):

- a. Per WDO 3.02.01A., dedicate a 16-foot PUE centered along the looped 6-inch public water line.
- b. Per WDO 3.01.01C., 3.01.04A., 3.02.01C., and Transportation System Plan (TSP) Figure 6, dedicate a 35-foot PUE along the north property line of the resulting consolidated lot.
- c. Per WDO 3.04.03C.2., dedicate a 20-foot shared access easement benefitting 2200 N. Pacific Highway (Tax Lots 051W08A004700 and 4800).
- d. Subsections a., b., and c. are due prior to building permit issuance.

DR-4. Parking:

- a. Per WDO 3.05.02J., revise plans to illustrate painted directional markings on pavement or directional signage throughout the parking lot.
- b. Per WDO 3.05.02K. and Figure 3.05C, revise plans to illustrate parking stalls delineated by double parallel lines.
- c. Per WDO 3.05.03B. and Table 3.05B, revise plans to note which accessible stall is the "Van Accessible" stall.
- d. Subsections a., b., and c. are due prior to building permit issuance.

DR-5. Landscape area curbing: Per WDO 3.06.02I., revise plans to note a 6-inch curb between landscape areas and on-site pavement. This condition is due prior to building permit issuance.

DR-6. Trash enclosure: Per WDO Table 3.06D and 3.06.06A., revise plans to include drawings and/or details illustrating that the required Architectural Wall for the exterior trash enclosure is an approvable height and meets the design standards of 3.06.06B.

DR-7. Architectural design:

- a. Windows: Per 3.07.06B.2.b.(1), the west building façade shall have at least 30 percent of its area occupied by windows.
- b. Rooftop equipment: Per 3.07.06B.4., any rooftop equipment shall be screened from view from the street.

DR-8. Signage: To meet WDO 3.10, the applicant shall demonstrate conformance with applicable requirements for the proposed signage through the sign permit process of 5.01.10.

Property Line Adjustment

PLA-1. Recordation: The applicant shall record the subject lot consolidation with Marion County prior to building permit issuance.

Notes to the Applicant

The following are not planning / land use / zoning conditions of approval, but are notes for the applicant to be aware of and follow:

1. Recordations: Dedications of ROWs and PUEs involve recordations with Marion County. The applicant (or applicant's surveyor) would need to:
 - a. Prior to recordation with the County, follow City Public Works Department review and dedication process as directed by the City Engineer or higher authority, including the use of Public Works document templates;
 - b. Submit a draft copy of the ROW dedications and easements, including text and drawing(s), to the attention of both the Director and City Engineer for their reviews and directions. Easement text should contain references to "City of Woodburn Variance DR 2020-05", "La Morenita Tortilleria", street address, and tax lot number. Submit through the Planning Division both Adobe PDF and written copies under a cover or transmittal letter providing project context, including land use / planning / zoning case file numbers, street address, tax lot number, and project name and an explanation of the purpose of the submittal; and
 - c. Record dedications and submit proof of recordation prior to building permit issuance.
2. Permits: Permits are applied for using the [Oregon ePermitting](#) online permit system. The City Building Division administers building and mechanical permits; Marion County Public Works administers plumbing and electrical permits.
3. Records: Staff recommends that the applicant retain a copy of the subject approval.
4. Fences, fencing, & free-standing walls: The approval excludes any fences, fencing, & free-standing walls, which are subject to WDO 2.06 and the permit process of 5.01.03.
5. Signage: The approval excludes any signage, which is subject to WDO 3.10 and the permit process of 5.01.10.
6. Other Agencies: The applicant, not the City, is responsible for obtaining permits from any county, state and/or federal agencies, which may require approval or permit, and must obtain all applicable City and County permits for work prior to the start of work and that the work meets the satisfaction of the permit-issuing jurisdiction. The Oregon Department of Transportation (ODOT) might require highway access, storm drainage, and other right-of-way (ROW) permits. All work within the public ROW or easements within City jurisdiction must conform to plans approved by the Public Works Department and must comply with a Public Works Right-of-Way permit issued by said department. Marion County plumbing permits must be issued for all waterline, sanitary sewer, and storm sewer work installed beyond the Public Right-of-Way, on private property.

7. Inspection: The applicant shall construct, install, or plant all improvements, including landscaping, prior to City staff verification. Contact Planning Division staff at least three (3) City business days prior to a desired date of planning and zoning inspection of site improvements. This is required and separate from and in addition to the usual building code and fire and life safety inspections. Note that Planning staff are not primarily inspectors, do not have the nearly immediate availability of building inspectors, and are not bound by any building inspector's schedule or general contractor convenience.
8. Stormwater management: The storm sewer system and any required on-site detention for the development must comply with the City Storm Water Management Plan, Public Works storm water practices and the Storm Drainage Master Plan.
9. Public Works Review: Staff performs final review of the civil plans during the building permit stage. Public infrastructure must be constructed in accordance with plans approved by the City, as well as current Public Works construction specifications, Standard Drawings, Standard Details, and General Conditions.
10. Franchises: The applicant provides for the installation of all franchised utilities and any required easements.
11. Water: All water mains and appurtenances must comply with Public Works, Building Division, and Woodburn Fire District requirements. Existing water services lines that are not going to be use with this new development must be abandoned at the main line. The City performs required abandonment of existing water facilities at the water main with payment by the property owner. All taps to existing water mains must be done by a "Hot Tap" method and by approved City of Woodburn Contractors. The applicant shall install the proper type of backflow preventer for all domestic, lawn irrigation and fire sprinkler services. The backflow devices and meters shall be located near the city water main within an easement, unless approved otherwise by Public Works. Contact Byron Brooks, City of Woodburn Water Superintendent, for proper type and installation requirements of the backflow device at (503) 982-5380.
12. Grease Interceptor/Trap: If applicable, a grease trap would need to be installed on the sanitary service, either as a central unit or in the communal kitchen/food preparation area. Contact Marion County Plumbing Department for permit and installation requirements, (503) 588-5147.
13. Fire: Fire protection requirements must comply with the Woodburn Fire District standards and requirements. Place fire hydrants within the public ROW or public utility easement and construct them in accordance with Public Works Department requirements, specifications, standards, and permit requirements. Fire protection access, fire hydrant locations and fire protection issues must comply with current fire codes and Woodburn Fire District standards.

See City of Woodburn Standard Detail No. 5070-2 Fire Vault. The fire vault must be placed within the public right-of-way or public utility easement.

14. SDCs: The developer pays System Development Charges prior to building permit issuance. Staff will determine the water, sewer, storm and parks SDCs after the developer provides a complete Public Works Commercial/Industrial Development information sheet.



**LA MORENITA TORTILLERIA, INC.
DR 2020-05, & PLA20-03
TAX LOTS 051WO8A 04900, 051WO8A 5000, 051WO8A 05100
Public Works**

October 15, 2020

CONDITIONS OF LAND USE APPROVAL:

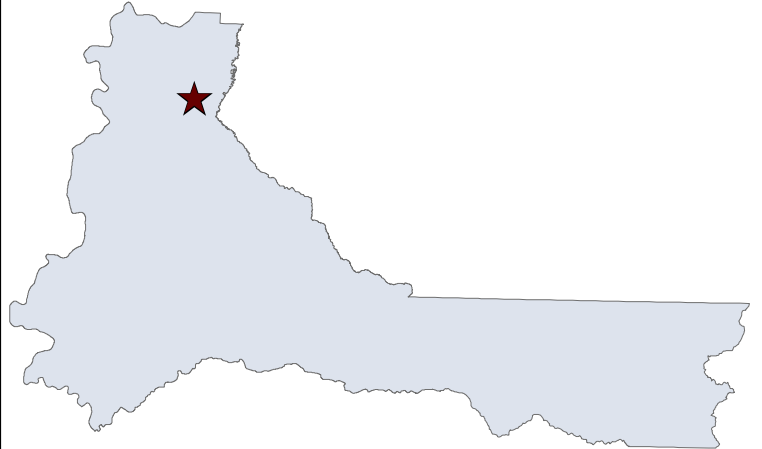
1. The Applicant, not the City, is responsible for obtaining permits from state, county and/or federal agencies that may require such permit or approval. All work within the Oregon Department of Transportation (ODOT) right-of-way requires the applicant to obtain approval and permits from ODOT.
2. The Applicant shall obtain approval from the Oregon Department of Transportation (ODOT) for the storm drainage analysis that will impact ODOT's system. Provide a final storm drainage analysis for detention and conveyance system. The storm drainage hydraulic analysis shall comply with both ODOT and City's requirements.
3. The applicant shall obtain approval from the Oregon Department of Transportation (ODOT) for all required improvements along Highway 211, included but not limited to street improvements, striping, Highway signage, construction of new sewer line, construction of new water line, and additional improvements as required by ODOT's review/approval of the applicants' traffic study.
4. Applicant to provide for the installation of all franchise utilities and shall provide any required easements for these facilities. All permanent utility services to the development shall be underground.
5. Department of Environmental Quality Erosion Control 1200C permit will need to be obtained for this development prior to City issuance of permit.
6. Final review of the Civil Plans will be done during the building permit application. Public infrastructure will be constructed in accordance with plans approved by public works and ODOT.
7. Provide and record the required right-of-way dedication, public utility easements, and waterline easements prior to building permit issuance.
8. Provide street lighting plan and design for review to the City and ODOT.
9. Applicant to provide plans indicating the construction of a new gravity sanitary sewer main in Hwy211 – Woodburn-Estacada Highway. As indicated on previous meetings, the existing sewer main in Hwy 211 is an abandoned force main. Applicant is responsible for

constructing a public gravity sewer main to the proposed development.

10. Fire hydrants locations and fire protection requirements shall be as per the Woodburn Fire Districts and City of Woodburn requirements. Actual fire hydrant locations and in-line valving locations shall not be determined until the construction final plan review.
11. System Development fees shall be paid at the time of building permit issuance.
12. All public easements and right-of-way dedications shall be properly recorded prior to building permit issuance.

05 1W 08A
103 03 03 0

05 1W 08A
WOODBURN



MARION COUNTY, OREGON
NE1/4 SEC8 T5S R1W W.M.
SCALE 1" = 200'

LEGEND

- LINE TYPES**
- Taxlot Boundary
 - Road Right-of-Way
 - Railroad Right-of-Way
 - Private Road ROW
 - Subdivision/Plat Bndry
 - Waterline - Taxlot Bndry
 - Historical Boundary
 - Easement
 - Railroad Centerline
 - Taxcode Line
 - Map Boundary
 - Waterline - Non Bndry

- CORNER TYPES**
- + 1/16TH Section Cor.
 - ⊙ DLC Corner
 - ⊕ 1/4 Section Cor.
 - ⊕ Section Corner
 - 16 15
 - 21 22

NUMBERS

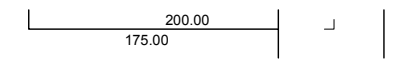
Tax Code Number
000 00 00 0

Acreege
0.25 AC

All acres listed are Net Acres, excluding any portions of the taxlot within public ROWs

NOTES

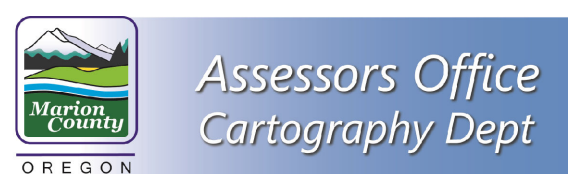
Tick Marks: A tick mark in the road indicates that the labeled dimension extends into the public ROW



ATTACHMENT 102

CANCELLED NUMBERS	
200	4300A4
500	4500
1300A1	
2100	
2200	
2600	
2900	
3100	
3200	
3400A1	
3400A2	
3500	
3900	
4000	
4200	
4300A2	
4300A3	

DISCLAIMER: THIS MAP WAS PREPARED FOR ASSESSMENT PURPOSES ONLY



FOR ADDITIONAL MAPS VISIT OUR WEBSITE AT www.co.marion.or.us

PLOT DATE: 1/23/2018

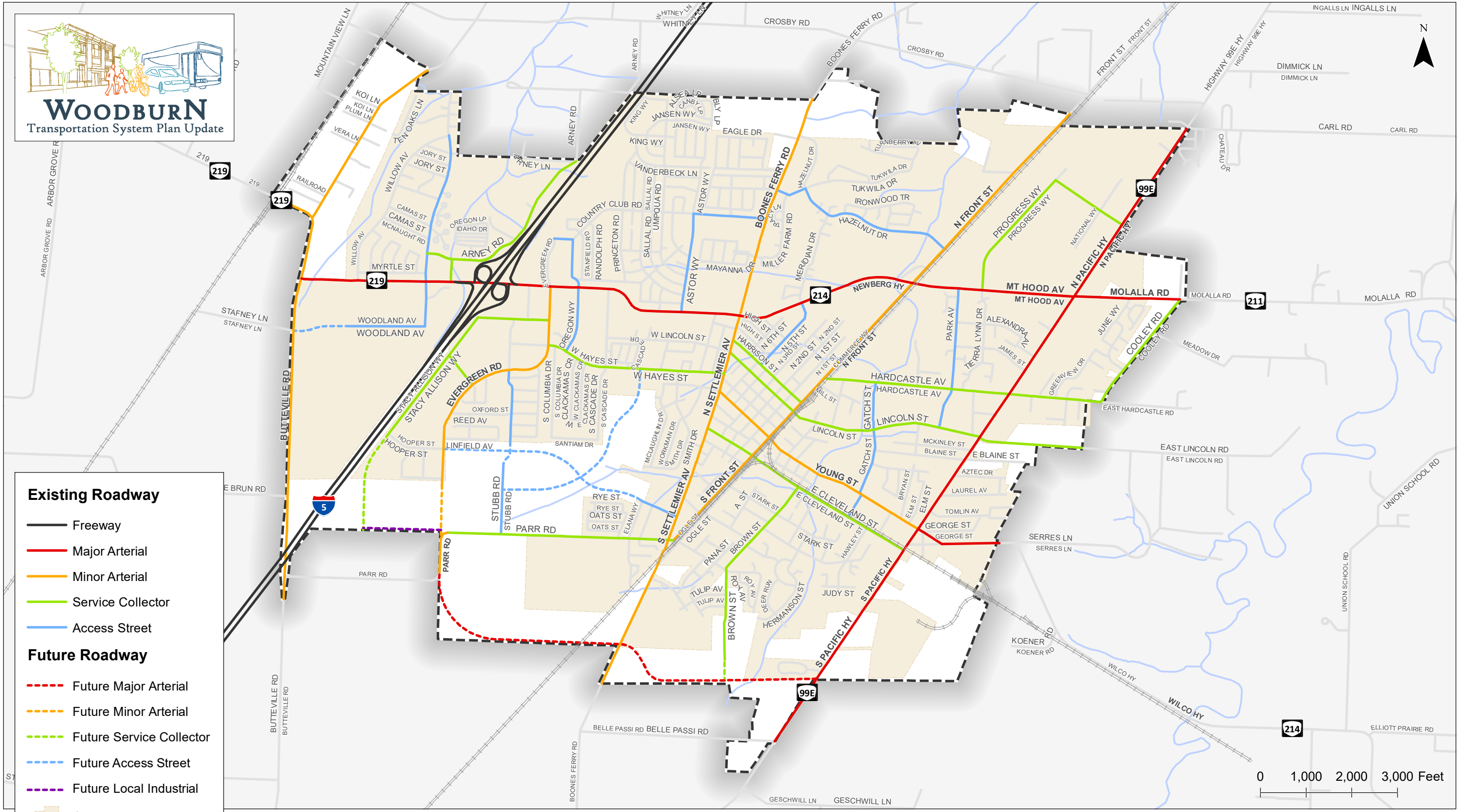
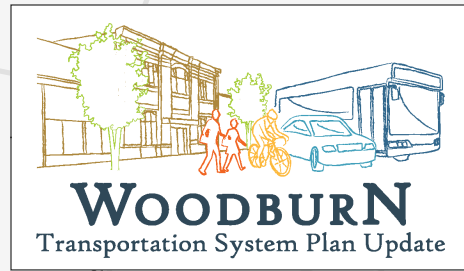
WOODBURN
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103 03 03 5

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Existing Roadway

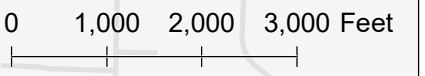
- Freeway
- Major Arterial
- Minor Arterial
- Service Collector
- Access Street

Future Roadway

- Future Major Arterial
- Future Minor Arterial
- Future Service Collector
- Future Access Street
- Future Local Industrial

City Boundary

Urban Growth Boundary

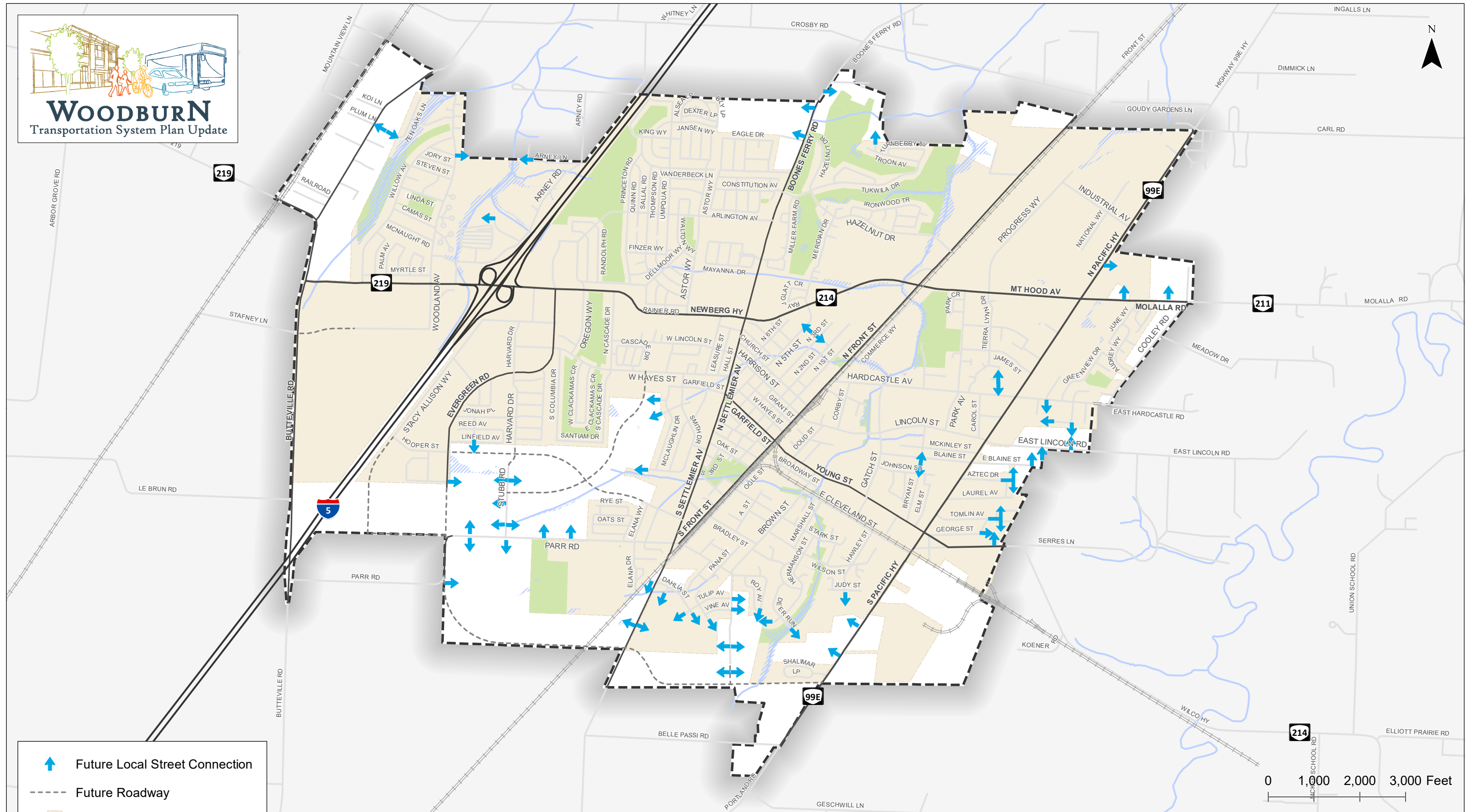
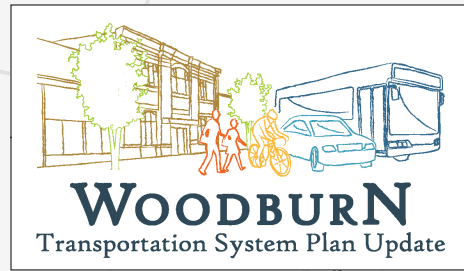






**Functional Roadway Classification
Woodburn, Oregon** Figure
2

Note: Future roadway alignments are approximate and subject to further refinement.

Coordinate System: NAD 1983 HARN StatePlane Oregon North FIPS 3601 Feet Intl
Data Source: City of Woodburn, Oregon Department of Transportation

H:\1212\1071 - Woodburn TSP Update\GIS\TSP02 Functional Roadway Classification.mxd - mmcormick - 5:25 PM 9/18/2019



-  Future Local Street Connection
-  Future Roadway
-  City Boundary
-  Urban Growth Boundary

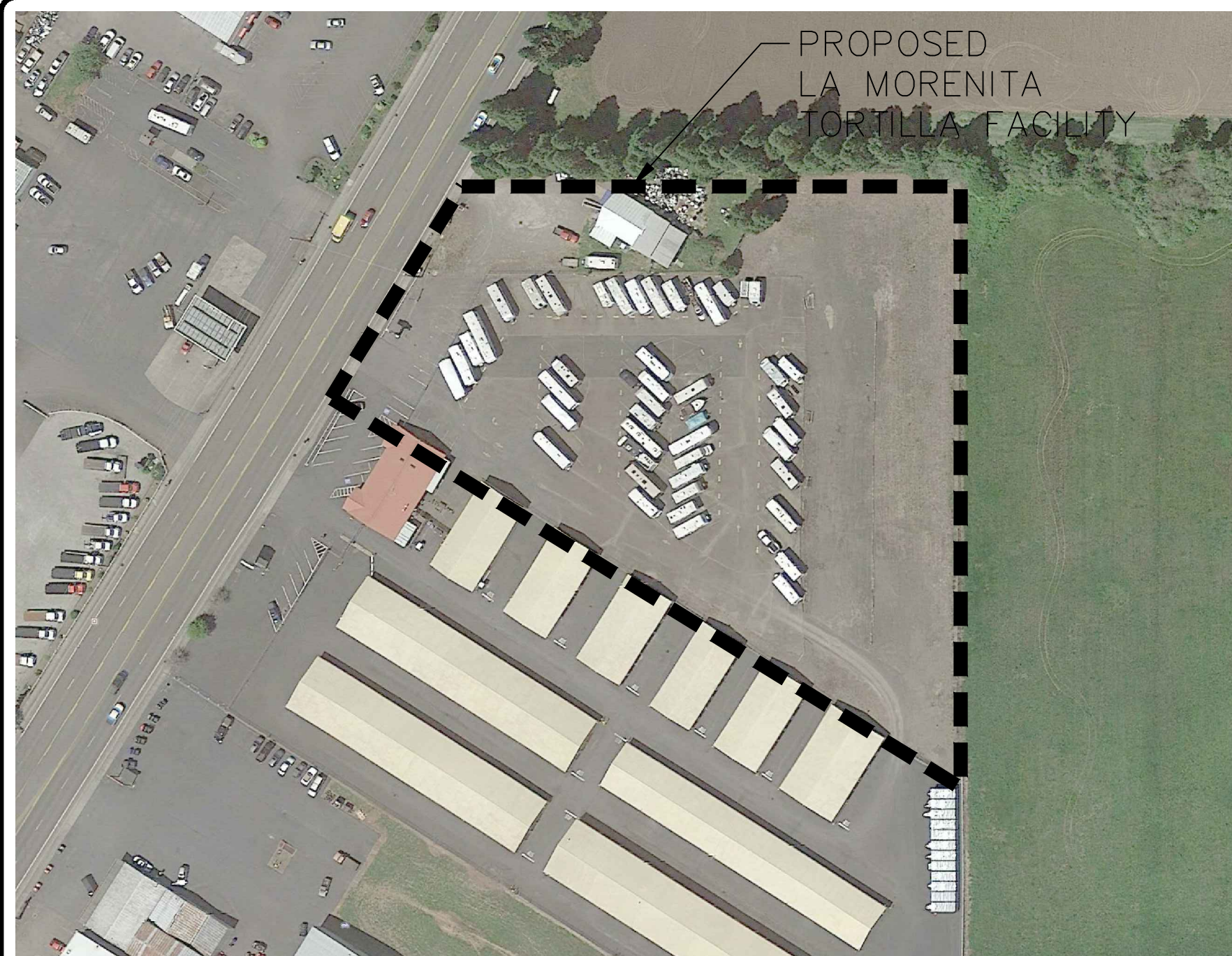
Note: Future roadway alignments are approximate and subject to further refinement.

**Local Street Connectivity Plan
Woodburn, Oregon**

**Figure
6**

Coordinate System: NAD 1983 HARN StatePlane Oregon North FIPS 3601 Feet Intl
Data Source: City of Woodburn, Oregon Department of Transportation

H:\1212\1071 - Woodburn TSP Update\GIS\TSP\06 Local Street Connectivity Plan.mxd - mmccormick - 5:46 PM 9/16/2019



VICINITY MAP - NTS

LA MORENITA TORTILLAS WOODBURN, OREGON

149,581 SF OR 3.43 ACRES
TAX LOT(S) 051W8A 4900/5000/5100

INDEX OF DRAWINGS

- 1 COVER SHEET
- 2 EXISTING CONDITIONS
- 3 SITE PLAN
- 4 PROPOSED GRADING & EROSION CONTROL PLAN
- 5 COMPOSITE UTILITY PLAN
- 6 LANDSCAPE PLAN
- 7 BUILDING FLOOR PLAN
- 8 OFFICE AND COMMERCIAL FLOOR PLAN
- 9 BUILDING ELEVATIONS
- 10 LIGHTING PLAN

APPLICANT:

GILBERTO DIAZ
270 GRANT ST.
WOODBURN, OR 97071
O: (503) 982-3959
EMAIL: GIL.DIAZ@HOTMAIL.COM

SITE PLANNERS/ENGINEERS/SURVEYORS:

WELKIN ENGINEERING, P.C.
ED CHRISTENSEN, P.E.
25260 SW PARKWAY AVE., SUITE G
WILSONVILLE, OR 97070
EMAIL: EKC@WELKINPC.COM
PHONE: 503.380.5324

UTILITY/SERVICE PROVIDERS:

SANITARY SEWER - CITY OF WOODBURN
DOMESTIC WATER - CITY OF WOODBURN
STORMWATER - ODOT
FIRE - WOODBURN FIRE DISTRICT

BUILDING AREAS:

RETAIL: 374 SF
OFFICE: 4,126 SF
MANUFACTURING: 11,883 SF
WAREHOUSE: 28,017 SF

EXISTING CONDITIONS:

IMPERVIOUS SURFACES: 114,843 SF, 2.64 AC
PERVIOUS AREAS: 34,412 SF, 0.79 AC

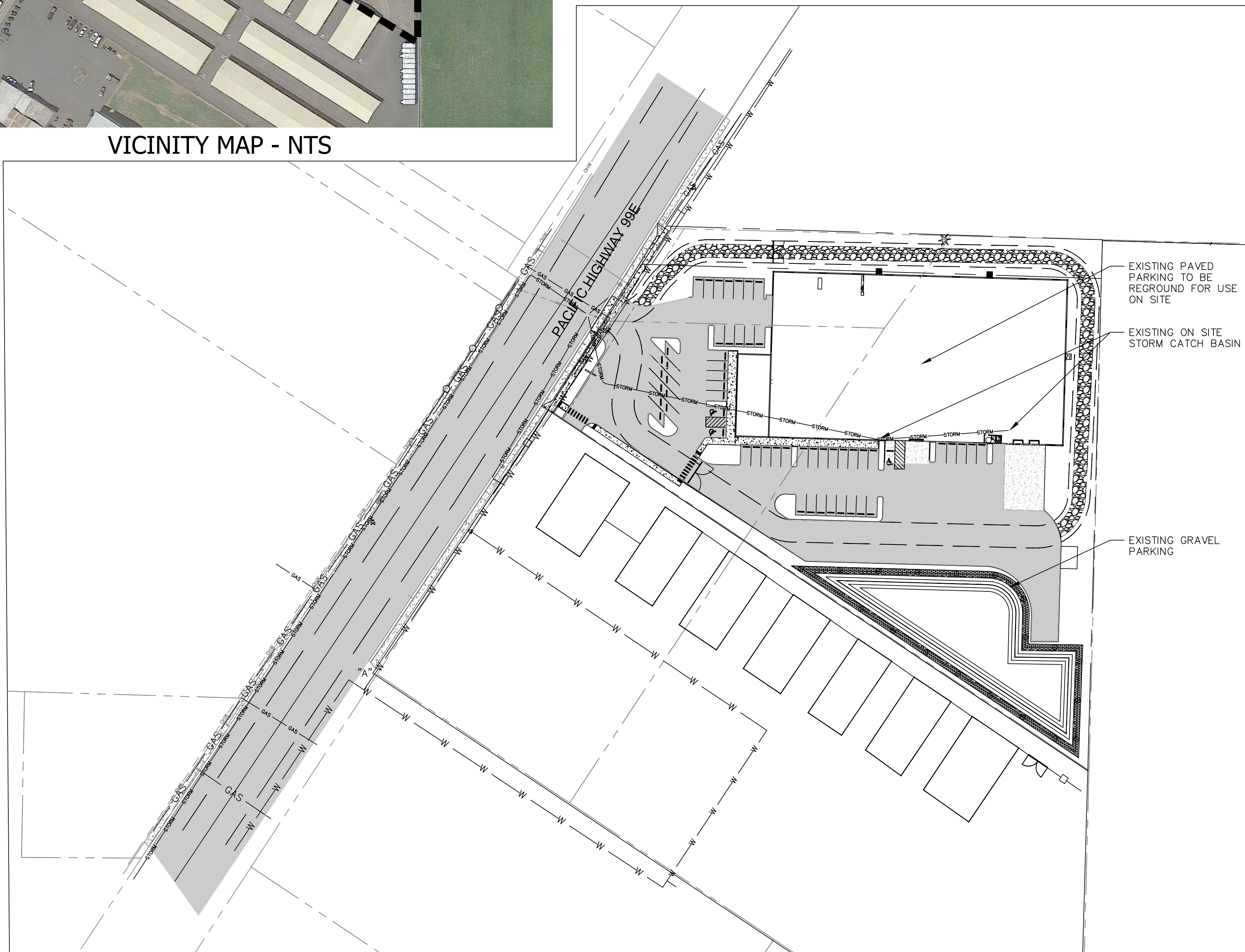
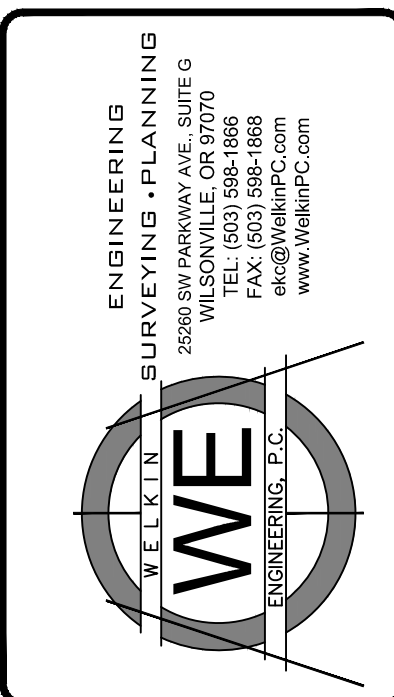
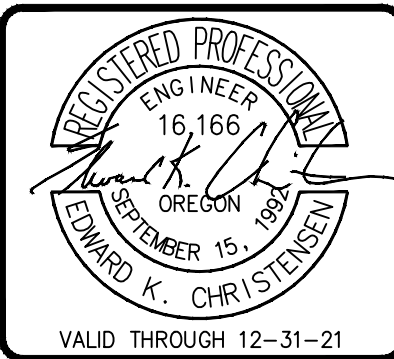
DEVELOPED CONDITIONS:

IMPERVIOUS SURFACES: 102,189 SF, 2.34 AC
PERVIOUS AREAS (LANDSCAPING): 47,045 SF, 1.08 AC

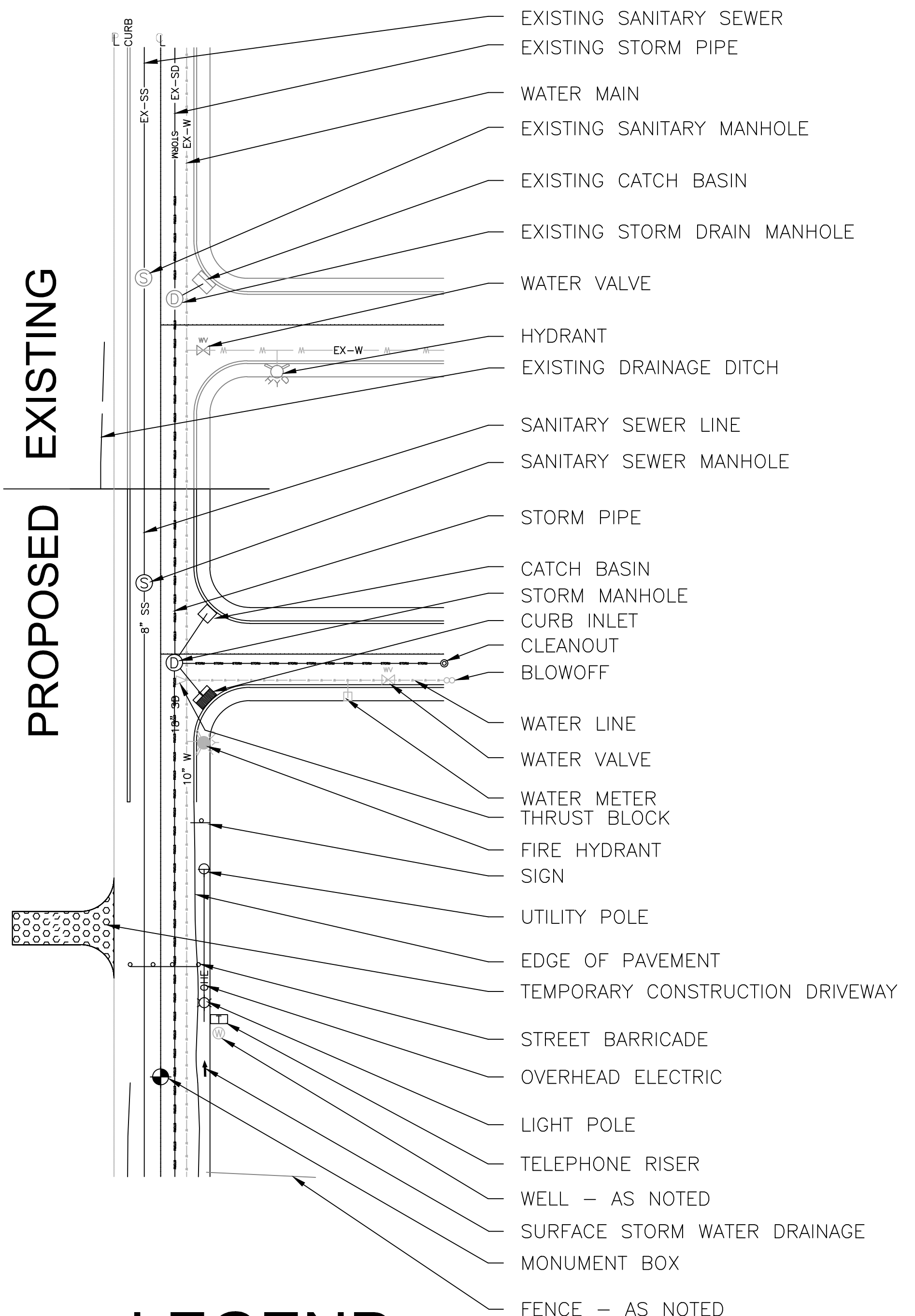
LANDSCAPE AREAS:

47,045 SF OR 31.58%

NO.	DATE	REVISION



LA MORENITA SITE
SCALE: 1" = 60'



LEGEND

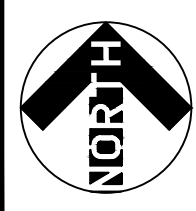
COVER SHEET &
EXISTING CONDITIONS

LA MORENITA TORTILLAS, INC.
WOODBURN OREGON
MARION COUNTY

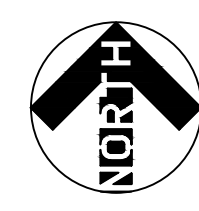
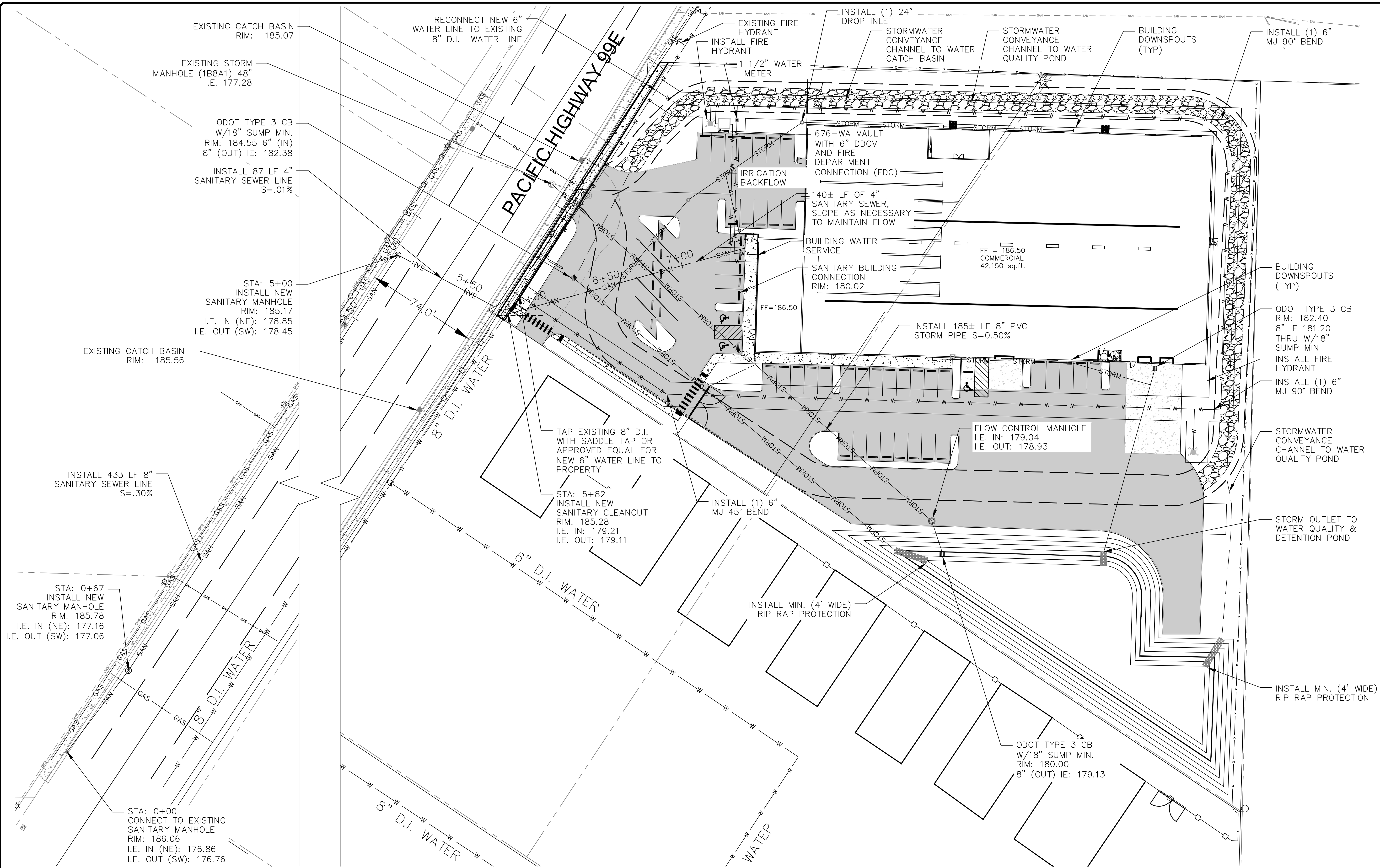
DESIGNED BY:	EKC
DRAWN BY:	
CHECKED BY:	
DATE:	8/11/20
SCALE:	1"=20' HORIZONTAL
PROJECT NO:	20-119.01
SHEET	C1

DATE PLOTTED: Aug 12, 2020 - 9:29am
WFPC DRAWING FILE: P:\Project Data\20-119.01 La Morenita Ldwg\Civil\PRELIMINARY SUBMITTAL.dwg

DATE PLOTTED: Aug 26, 2020 - 12:12pm
WFPC DRAWING FILE: P:\Project Data\20-119-01-La Morenita\dwg\CONSTRUCTION PLANS.dwg

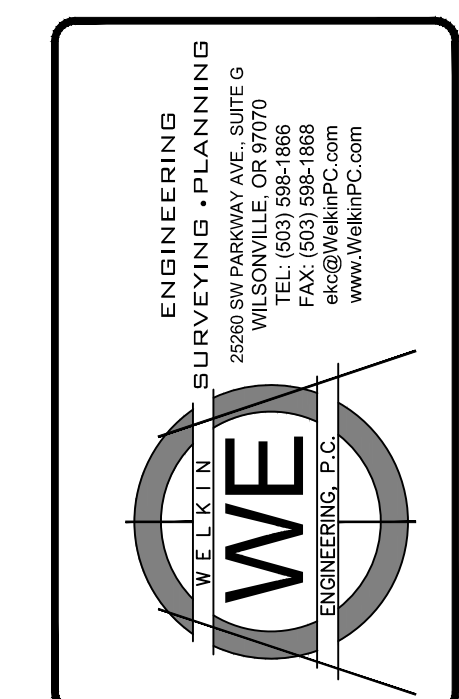
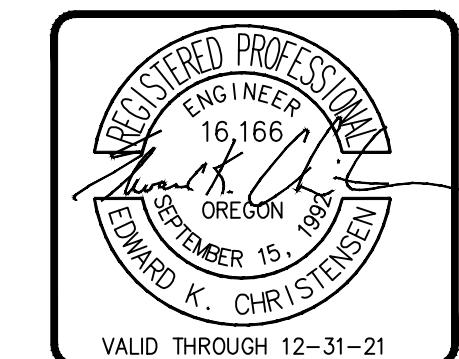


COMPOSITE UTILITY PLAN (CONTINUED)
SCALE: 1" = 30'



COMPOSITE UTILITY PLAN
SCALE: 1" = 30'

NO.	DATE	REVISION

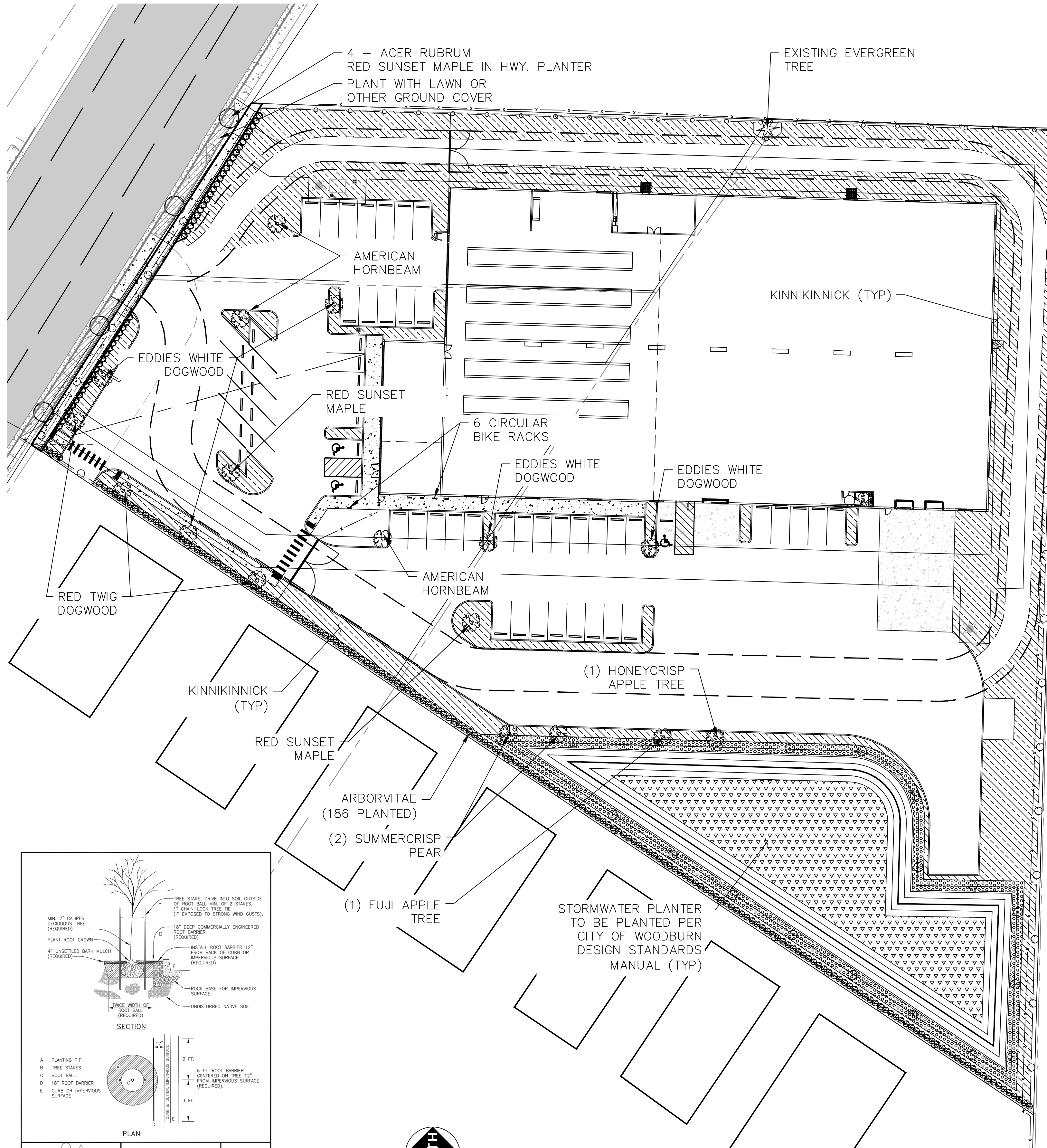


COMPOSITE UTILITY PLAN

LA MORENITA TORTILLAS, INC.
WOODBURN OREGON
MARION COUNTY

DESIGNED BY:	EKC
DRAWN BY:	
CHECKED BY:	
DATE:	8/26/20
SCALE:	1" = 20' HORIZONTAL
	VERTICAL
PROJECT NO:	20-119-01
SHEET	C5

IRRIGATION PLAN TO BE DESIGNED AND BUILT BY LANDSCAPING CONTRACTOR



PLANTING SCHEDULE:

BOTANICAL NAME COMMON NAME	QTY.	SIZE	CONDITION	HEIGHT	SPREAD	REMARKS
ACER RUBRUM RED SUNSET MAPLE	4	2" Cal.	B&B	70'	65'	
OSTRYA VIRGINIANA AMERICAN HORNBEAM	2	2" Cal.	B&B	35'	35'	
CORNUS NUTTALLI EDDIES WHITE DOGWOOD	5	2" Cal.	B&B	25'	20'	
MALUS HONEYCRISP APPLE HONEYCRISP	1	2" Cal.	B&B	14'-18'	12'-15'	
MALUS FUJI FUJI APPLE	1	2" Cal.	B&B	10'-15'	8'-10'	
PYRUS COMMUNIS SUMMERCRISS PEAR	1	2" Cal.	B&B	18'-25'	10'-15'	
PYRUS COMMUNIS COMICE PEAR	1	2" Cal.	B&B	12'-18'	12'-18'	
CORNUS FLORIDA RED TWIG DOGWOOD	3	1.5" Cal.	B&B	20'-25'	20'-25'	

SHRUBS

VIBURNUM LAURUSTINUS SPRING BOUQUET SPRING BOUQUET VIBURNUM	46	1 GAL.	B&B		36" O.C.
THUJA ARBORVITAE	182	1 Gal.	B&B		36" O.C.

GROUND COVER

ARCTOSTAPHYLOS UVA-URSI KINNIKINICK	12,800 SF	1 GAL. CONT.	B&B		30" O.C.
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LANDSCAPE AREAS:

TREE/SHRUB AREA:	25,835 sq.ft.
STORM WATER PLANTER AREA:	3,670 sq.ft.
SUBTOTAL LANDSCAPE AREA: SUBJECT TO PU REQUIREMENTS	29,505 sq.ft.
Right of Way:	423 sq.ft.
Total Landscape Area:	29,928 sq.ft.

STORM WATER PLANTER: 3,670 sq ft

Plant calculations:	Zone A area:	3,670 sf x 100/100 sf = 3,670 Herbaceous plants
		3,670 sf x 1/200 sf = 18.35 (19) Trees

BOTANICAL NAME COMMON NAME	QTY.	SIZE	CONDITION	SPACING
TREES				
RHAMNUS PURSHIANA CASCARA	18	1.5" Cal.	B&B	as shown
HERBACEOUS PLANTS				
CAREX OBNUPTA SLOUGH SEDGE	1,835	1 Gal.	Can	12" o.c.
SCIRPUS MICROCARPUS SMALL-FRUITED BULRUSH	1,835	1 Gal.	Can	12" o.c.

* ALL PLANTING REQUIREMENTS ARE TAKEN FROM TABLE 3.06A PER CITY OF WOODBURN DEVELOPMENT ORDINANCE

PLANTING REQUIREMENTS:	CALCULATED VALUES:
OTHER YARDS 1 PU/50 SQUARE FEET	13,776/50 = 276 PU
OFF-STREET PARKING AND LOADING AREAS 1 PU/20 SQUARE FEET 59 PARKING SPACES	10,260/20 = 513 PU 6 MEDIUM TREES = 60 PU
	TOTAL REQUIRED = 849 PU

* ALL (PU) VALUES ARE TAKEN FROM TABLE 3.06 B PER CITY OF WOODBURN DEVELOPMENT ORDINANCE

PLANT UNIT (PU) VALUES:

- 2.) 10 X 5 = 50 PU
- 4.) 16 X 4 = 64 PU
- 6.) 228 X 1 = 228 PU
- 5.) 232 X 4 = 464 PU
- 7.) 17.677/50 = 354 PU
- 15.) 18497/50X2 = 740 PU

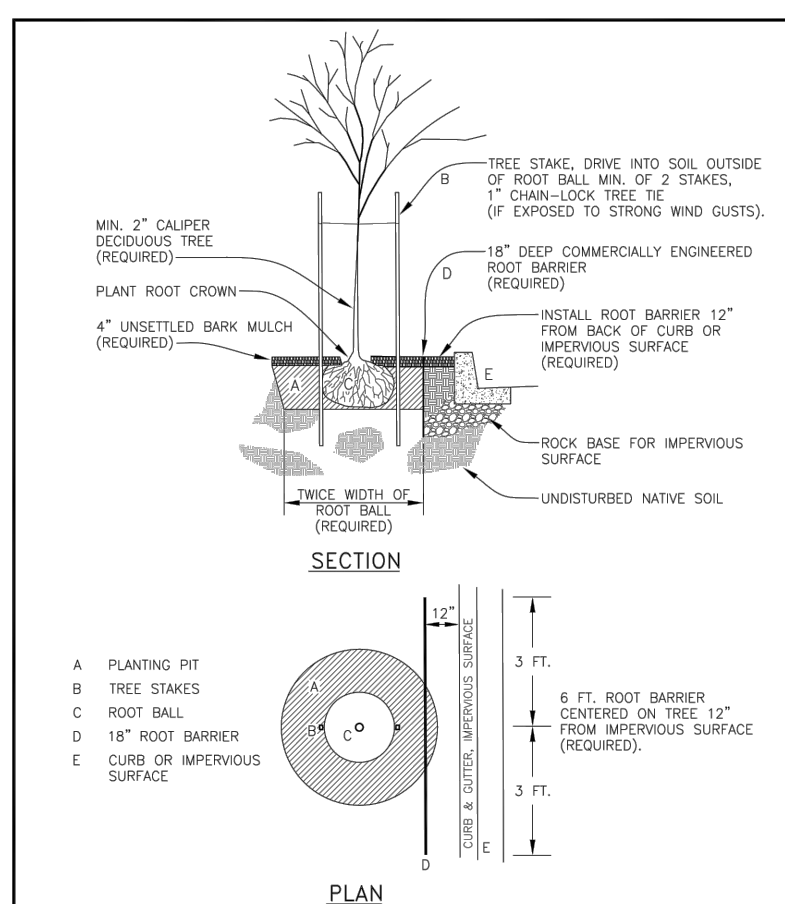
PLANTING UNITS PROVIDED = 1,900

STORM WATER PLANTING SPECIFICATIONS

THESE ARE MAN-MADE STORM FACILITIES AND ALL EXISTING VEGETATION WILL HAVE BEEN REMOVED.

ALL PLANTS TO BE PIT PLANTED WITH ADDITIONAL ORGANIC MATTER IF REQUIRED BUT NO TRADITIONAL FERTILIZER IS NECESSARY. PLANT PLACEMENT SHALL BE CONSISTENT WITH THE FORM OF THE NATURALLY OCCURRING PLANT COMMUNITY. TREES & SHRUBS SHALL BE PLACED IN SINGLES OR CLUSTERS OF THE SAME SPECIES TO PROVIDE A NATURAL PLANTING SCHEME, AND TAGGED FOR IDENTIFICATION.

- 1) PLANT INSTALLATION REQUIREMENTS. CONTAINERIZED STOCK SHALL BE INSTALLED ONLY FROM FEBRUARY 1 THROUGH MAY 1 AND OCTOBER 1 THROUGH NOVEMBER 15. BARE ROOT STOCK SHALL BE 12-16" LONG AND BE INSTALLED ONLY FROM DECEMBER 15 THROUGH APRIL 15.
- 2) TREES AND SHRUBS PLANTED IN UPLAND AREAS (ZONE B) TO BE MULCHED A MINIMUM OF THREE INCHES IN DEPTH AND 18 INCHES IN DIA. (NO PART OF THE WQ FEATURE IS ZONE B FOR THIS PROJECT). APPROPRIATE MULCHES INCLUDE THOSE MADE FROM COMPOSTED LEAVES OR BARK THAT HAVE NOT BEEN CHEMICALLY TREATED.
- 3) MONITORING AND MAINTENANCE. UV3 PROTECTION TUBING IS REQUIRED FOR ALL NEW TREES AND SHRUBS AVAILABLE AT ACF WEST (503-771-5115). WILDLIFE PROTECTION MEASURES REQUIRED WHEN APPROPRIATE. THE REMOVAL OF NON-NATIVE, INVASIVE WEEDS IS NECESSARY THROUGHOUT THE MAINTENANCE PERIOD, OR UNTIL A HEALTHY STAND OF DESIRABLE VEGETATION IS ESTABLISHED. THE SITE IS TO BE MONITORED THREE TIMES PER GROWING SEASON AND ONCE PRIOR TO ONSET OF GROWING SEASON FOR A MIN. OF TWO YEARS. INSTALLED PLANTS THAT FAIL TO MEET SITE REVIEW INSPECTIONS BY THE CITY ENGINEER SHALL BE REPLACED DURING THE MAINTENANCE PERIOD. PRIOR TO REPLACEMENT, THE CAUSE OF LOSS (WILDLIFE DAMAGE, POOR PLANT STOCK, ETC.) SHALL BE DOCUMENTED WITH A DESCRIPTION OF THE BY A LANDSCAPE, ARBORICULTURE, AND/OR WILDLIFE SPECIALIST CORRECTIVE ACTIONS TAKEN.
- 4) TEMPORARY IRRIGATION WILL BE USED DURING THE TWO YEAR ESTABLISHMENT/ MAINTENANCE PERIOD, AT A MINIMUM RATE OF 1" PER WEEK FROM JUNE 15 THROUGH OCTOBER 15.



WOODBURN
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION

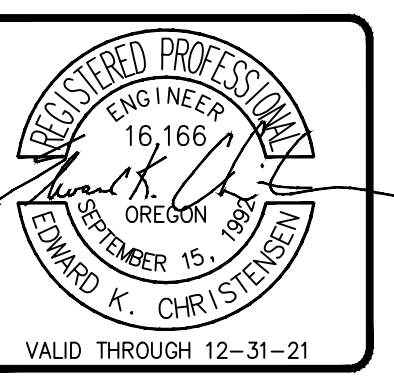
STREET TREE PLANTING NEW CONSTRUCTION

REV: OCT. 2007
SCALE: NTS
SHEET NO. 1



LANDSCAPE PLAN
SCALE: 1" = 30'

NO.	DATE	REVISION



ENGINEERING PLANNING
SURVEYING
2520 SW PARKWAY AVE. SUITE G
WILSONVILLE, OR 97070
TEL: (503) 896-1866
www.WelkPC.com

WE
ENGINEERING, P.C.

LANDSCAPE PLAN

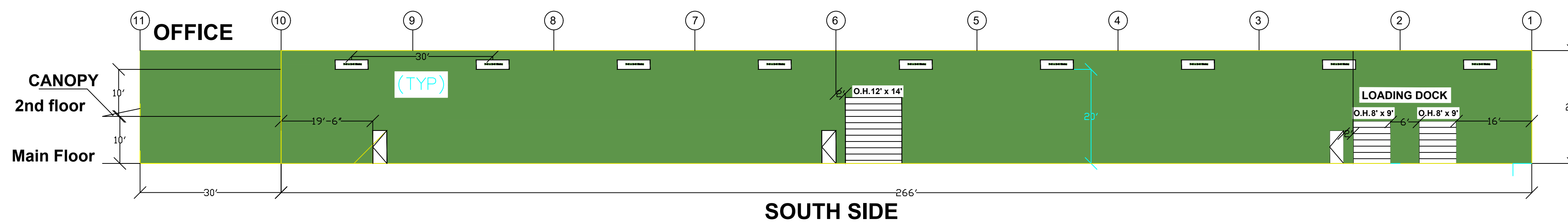
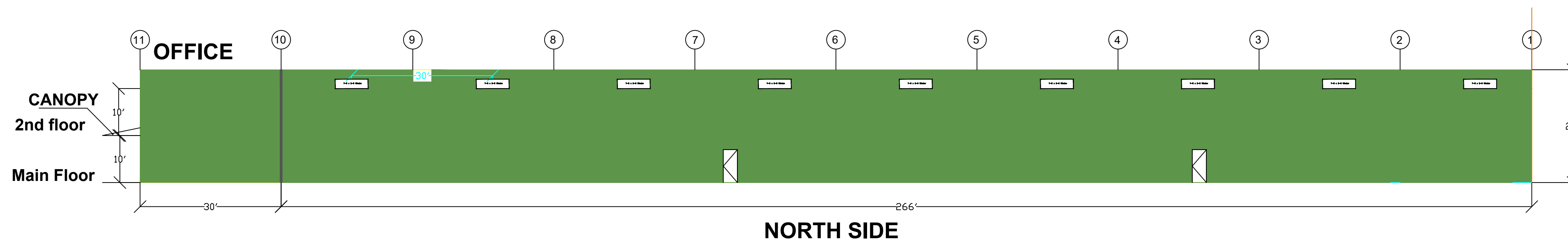
LA MORENITA TORTILLAS, INC.
WOODBURN OREGON
MARION COUNTY

DESIGNED BY:	EKC
DRAWN BY:	
CHECKED BY:	
DATE:	9/19/20
SCALE:	1"=20' HORIZONTAL
	VERTICAL
PROJECT NO.:	20-119.01
SHEET	C7



CIRCULAR BIKE RACK

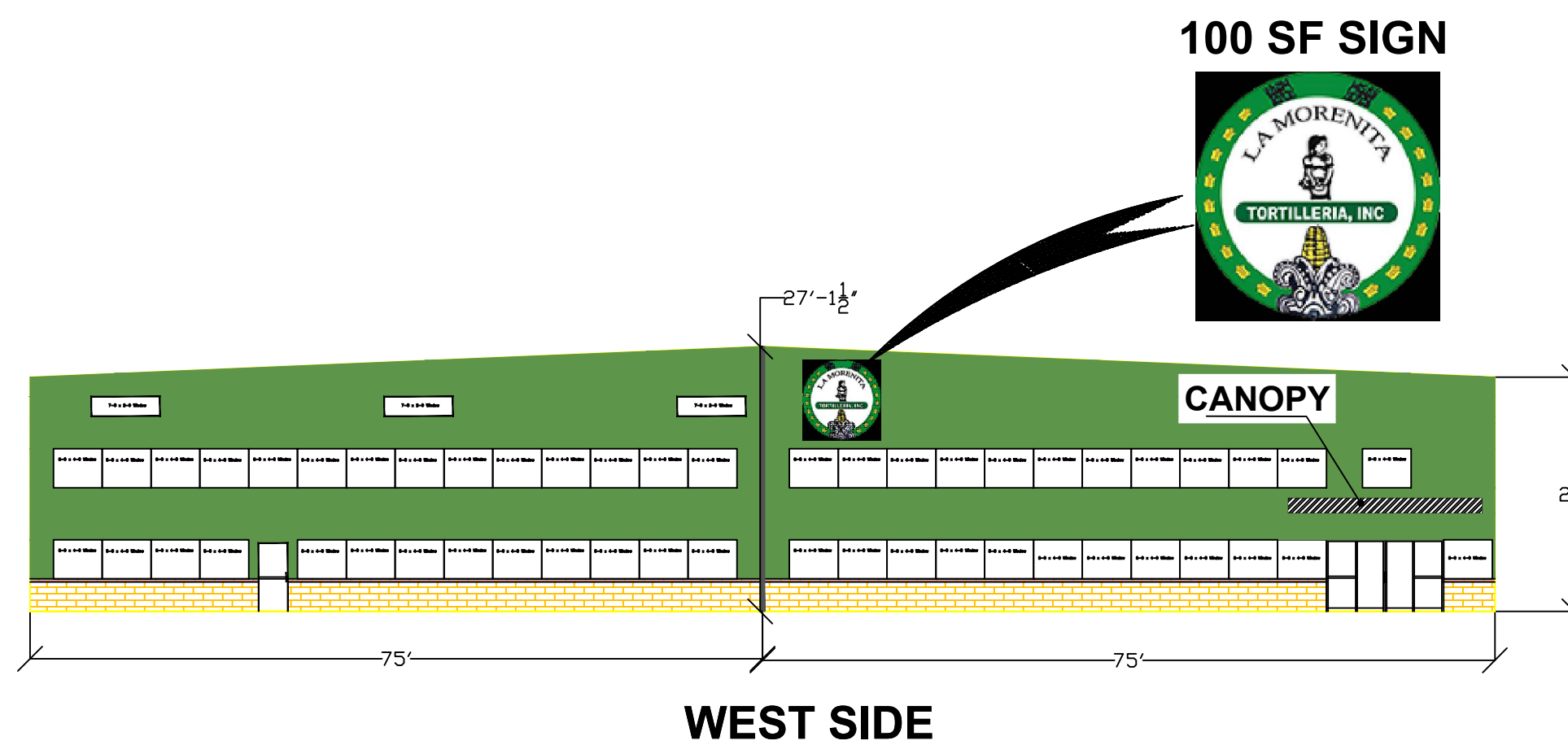
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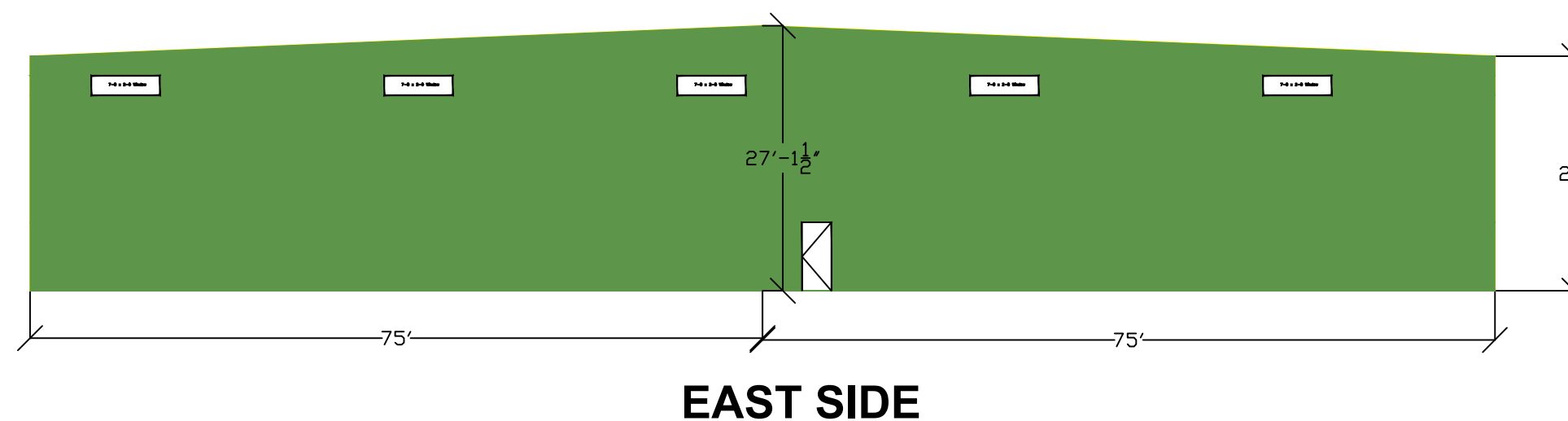
ENTRY DOOR CANOPY
5' WIDE X 20' LONG

FRONT FASCIA WINDOW GLAZING REQUIREMENT:
 $BUILDING: \frac{0.3 \times 150' \times 27.125' + 24'}{2} = 1,150 \text{ SF OF GLASS}$

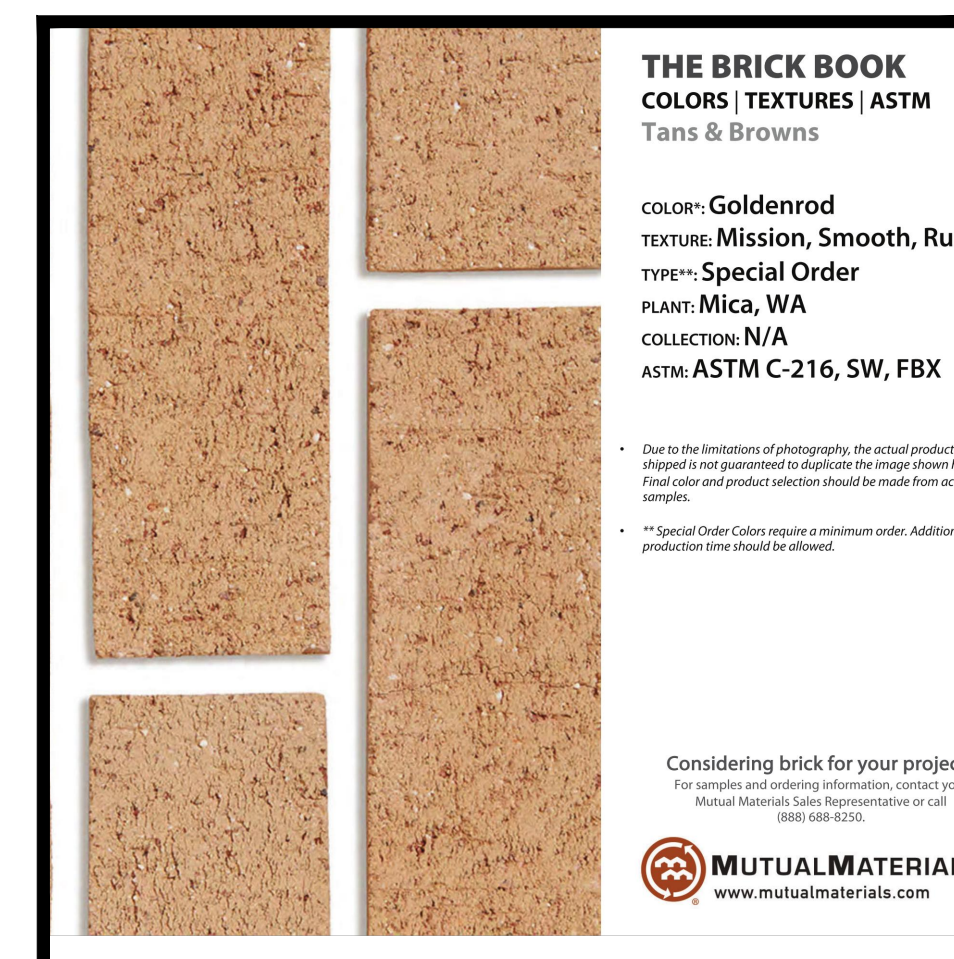
FRONT FASCIA WINDOW GLAZING PROVIDED:
 51 - 5'0" x 4'0" = 1,020 SF
 3 - 7'0" x 2'0" = 32 SF
 2 - DOORS = 109 SF
1,161 SF or 30.3% OF GLASS



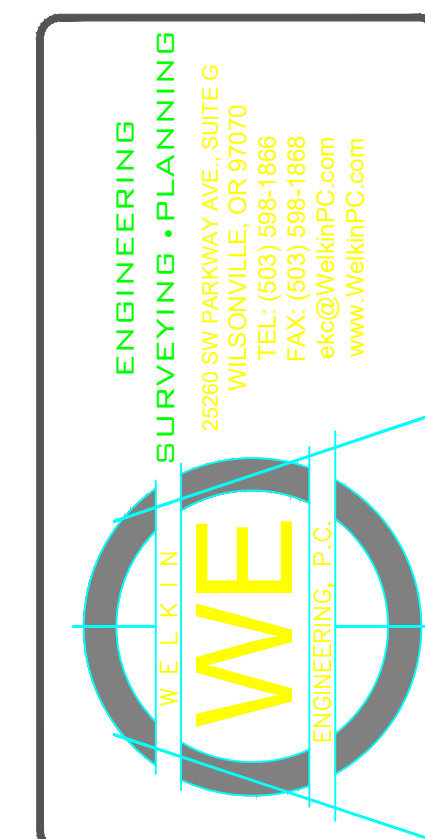
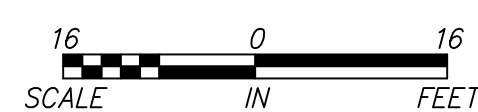
WEST SIDE



EAST SIDE



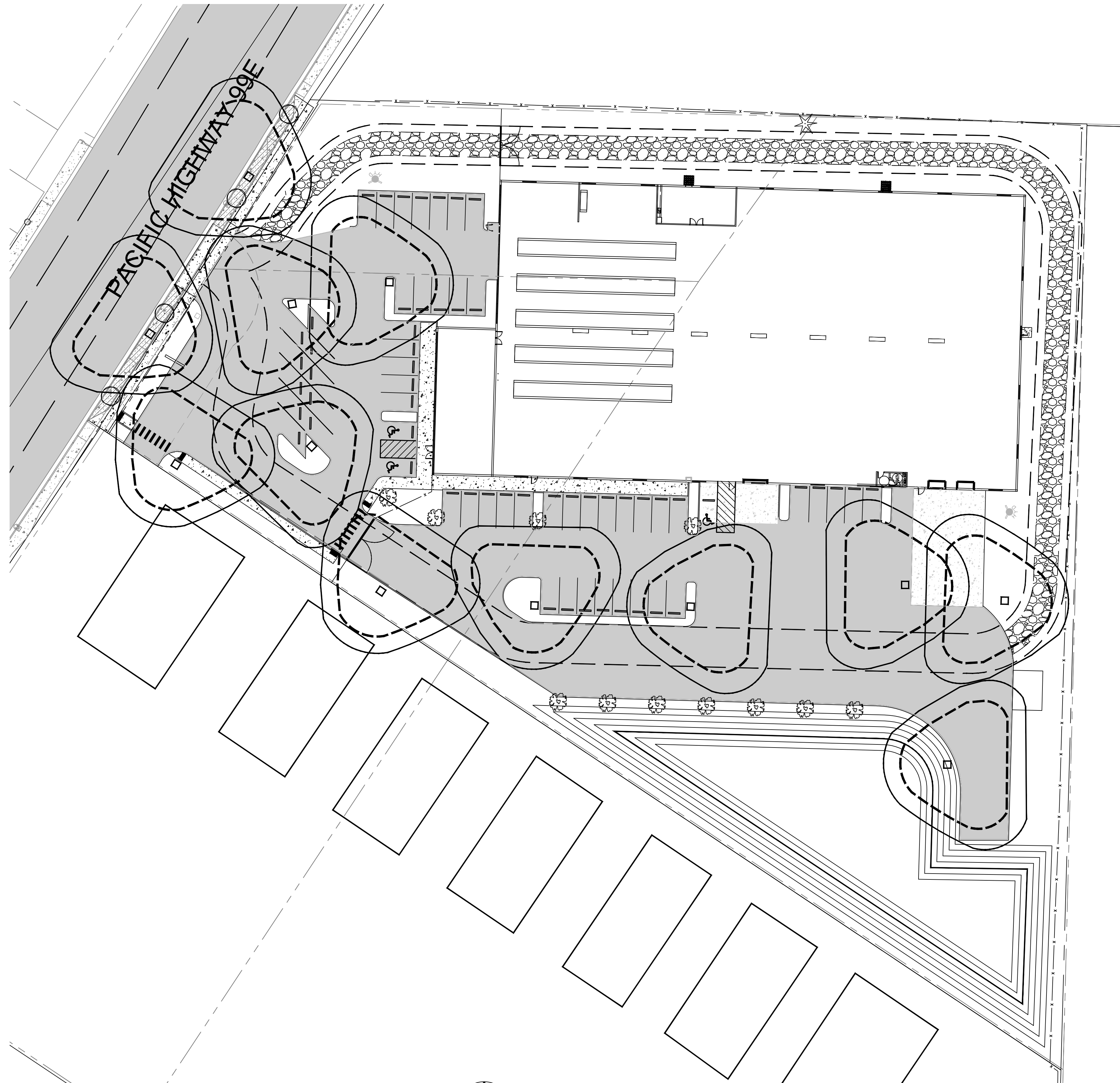
BRICK FACING



LA_MORENITA_TORTILLAS_INC.
WOODBURN, OR
BUILDING_ELEVATIONS

DESIGNED BY:	DESIGN
DRAWN BY:	DRAWN
CHECKED BY:	CHECKED
DATE:	DATE
SCALE:	HORIZ HORIZONTAL VERT VERTICAL
PROJECT NO:	20-119.01
SHEET	9

DATE PLOTTED: Aug 12, 2020 - 9:36am
 MEPC DRAWING FILE: P:\Project Data\20-119.01 La Morenita\dwg\Civil\PRELIMINARY SUBMITTAL.dwg



LIGHTING PLAN
 SCALE: 1" = 30'

LEDway® Series
 LEDway® High Output Street Light - Single Module

Product Description
 The LEDway® High Output Street Light series for roadway lighting expands the application range from 70W to 450W HPS. With industry leading optimized optical control and adjustable output for mounting, the LEDway® High Output Series significantly improves the economic value for the end application.

Performance Summary

Manufacturer's Product Catalog (PDF) Link	
Assembled in the U.S.A. of U.S. and Imported parts	
CRB Minimum 70 CRI	
CRB 3000K Lum. 3000K and 5700K Lum. 5000K	
Lumen Maintenance: 100,000 hrs (L70) or 100,000 hrs on ColorShift DataSheet® Link	

Accessories

Final Module	Re-light Signal	Mounting Pole
See Order for Details	210x10x10" (LxWxH)	30x10x10" (LxWxH)

Module Weight

Length	14.1 lbs. (6.4kg)
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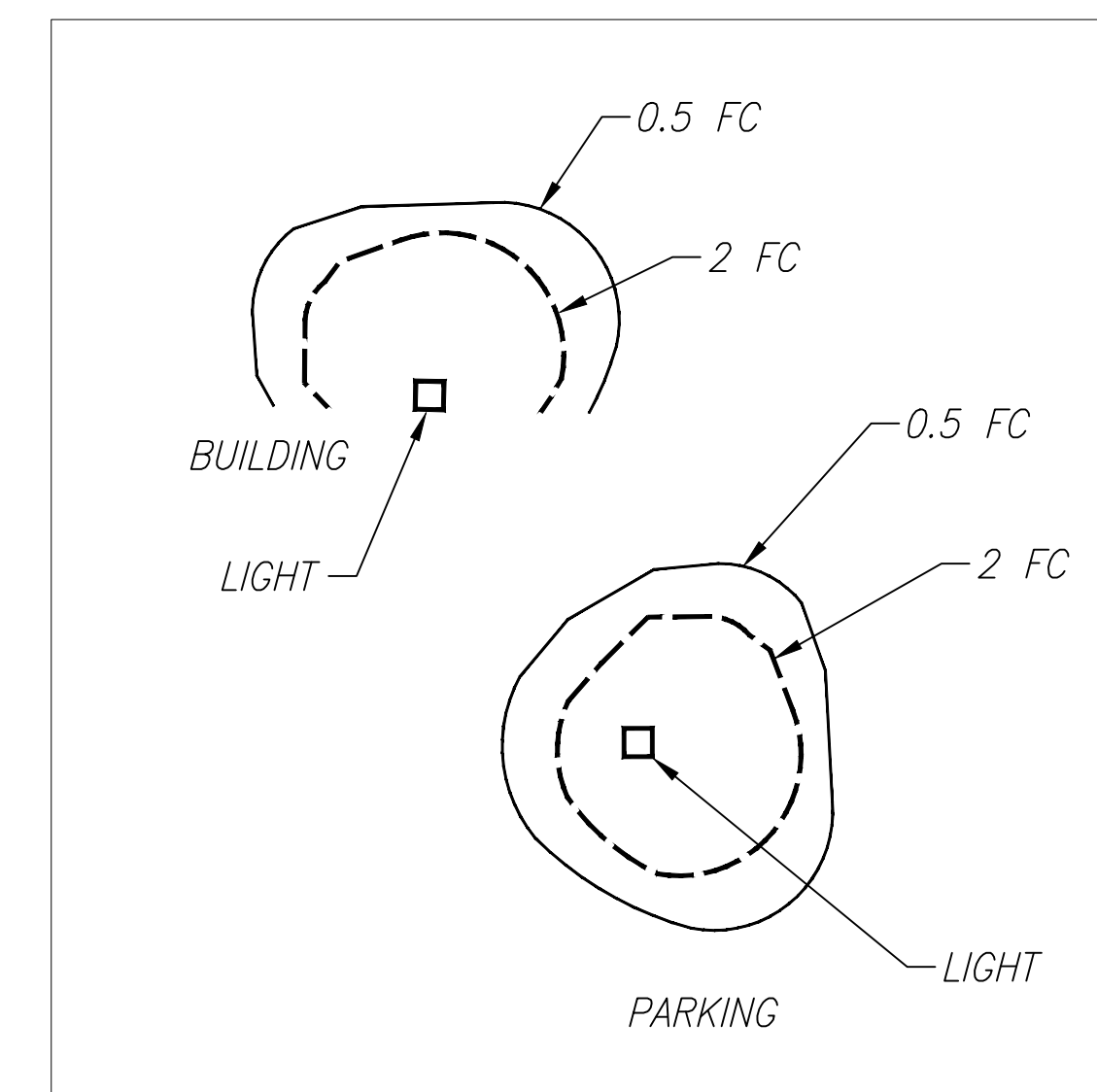
HIGHWAY LIGHTING DETAIL (1)



PARKING LOT LIGHTING DETAIL (2)

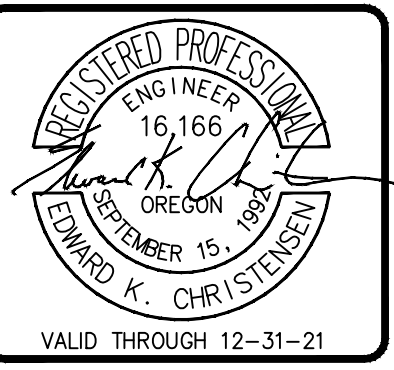


WALL PACK LIGHTING DETAIL (3)



LIGHTING LEGEND
 NO SCALE

NO.	DATE	REVISION



ENGINEERING - PLANNING
 SURVEYING
 W. E. L. I. N.
 WOODBURN, OREGON

25000 SW PARKWAY AVE. SUITE G
 WOODBURN, OR 97150
 TEL: (503) 596-1888
 FAX: (503) 596-1888
 www.welinc.com

LIGHTING PLAN

LA MORENITA TORTILLAS, INC.
 WOODBURN OREGON
 MARION COUNTY

DESIGNED BY:	EKC
DRAWN BY:	
CHECKED BY:	
DATE:	8/11/20
SCALE:	1" = 30' HORIZONTAL
	VERTICAL
PROJECT NO:	20-119.01
SHEET	C10

August 8, 2020

To: Mr. Chris Kerr, AICP
Community Development Director
City of Woodburn
270 Montgomery Street
Woodburn, OR 97071

From: Ejaz Khan, P.E.

Re: Commercial Bakery Facility (PRE 2020-09)
Trip Generation Assessment Letter

La Morentia Totillas is proposing to relocate its existing tortilla manufacturing facility from 270 Grant Street, Woodburn, Oregon to approximately one mile north along Pacific Highway (Tax lot number 051W08A 04900, 5000, & 5100. The three lots are zoned community commercial. The proposed relocated development will consist of an 18,000 square feet tortilla manufacturing plant, 3,906 square feet of office space that would include break room, bath rooms, locker space for worker employees, conference room, office space for three owner employees and an additional 374 square feet wholesale retail outlet, in the same building catering to wholesale customers only. There will be 21,900 square feet storage and distribution center as well. The vicinity map, the site plan and the office and retail plan for the proposed development are attached as exhibit 1.

Per WDO 3.04.05A a “traffic impact analysis” (TIA) is required if a development generates 100 peak hour trips or 1,000 daily trips. Based on Oregon Department of Transportation’s recent past direction for another site development:

The first step is to conduct a trip generation assessment using the most recent 10th edition of the Institute of Transportation Engineers (ITE) Trip Generation Manual. The ITE trip generation manual is a nationally recognized established standard for estimating trip generation for large number of land uses ranging from industrial land uses to residential, institutional, medical, office, and retail land uses etc. The trip generation rates for numerous land use have been studied and documented in a variety of ways including trip generation during morning and afternoon peak hours of adjacent street traffic, average daily trips produced by the land use etc. Further breakdown has been established in the manual including trip generation estimates based on 1000 square feet of gross floor area and based on number of employees etc. In addition to peak hour, ITE trip generation manual also has trip generation profile for various land uses throughout

the day. Since manufacturing, wholesale retail outlet and distribution center are three distinct and separate operation, the trip generation for each has been estimated separately.

TRIP GENERATION ESTIMATE FOR MANUFACTURING OPERATION

The tortilla manufacturing operation is projected to employ 15 employees and will operate in two shifts. The first shift is estimated to be staffed by 10 workers while the second shift is estimated to be staffed by five workers. The first shift will start at 6:00 AM and end at 3:00 PM, while the second shift will start at 3:00 PM and end between 6:30 PM to 7:00 PM. Employees are expected to arrive 10 to 15 minutes before the start of each shift. The retail outlet is expected to be staffed by one employee. Hours of retail operation will be from 8:00 AM to 5:00 PM. Since the number of employees or workers in tortilla manufacturing plant is known, it is a more representative estimate to calculate the trip generation for manufacturing operations based on number of employees as defined in the ITE trip generation manual land use code 140 (manufacturing). The trip generation rate for each manufacturing shift is estimated separately. As stated above, the manufacturing plant workers and office staff will arrive prior to 6:00 AM which is before the start of morning peak hour of adjacent street traffic. The AM peak of adjacent street traffic, would be approximately two hours after the AM peak of generator. The manufacturing plant would therefore add negligible number of trips to the system during the AM peak of adjacent street traffic. Based on ITE trip generation profile for manufacturing, it will add 8.4 percent of the daily trips during the AM peak of adjacent street traffic (exhibit 2), which comes out to be 2 trips. This study is conservatively assuming the higher figure based on ITE trip generation manual and assuming a later shift start time. Since the morning shift will end before the start of PM peak of adjacent street traffic, PM peak hour trip for the morning shift is not applicable. The same process for trip generation estimate is used for the afternoon manufacturing shift.

Number of employees for morning shift (manufacturing) including office staff = 13

AM Peak
 Entering 0.27
 Exiting 0.10

ITE LUC 140	Employees	Trip Generation Rate					
		Daily	AM -Peak	ADT*	AM Peak Hour**		
	13	2.47	0.37	Total	Total	Enter	Exit
Site Trips				32	4	3	1

*Assuming constant number of employees throughout the day.

La Morentia Totillas commercial bakery

Number of employees for afternoon shift (manufacturing) including office staff = 8

PM Peak
 Entering 0.13
 Exiting 0.20

ITE LUC 140	Employees	Trip Generation Rate							
		Daily	PM -Peak		ADT*	PM Peak Hour**			
	8	2.47	0.33		Total	Total	Enter	Exit	
Site Trips					20	3	1	2	

*Assuming constant number of employees throughout the day.

** The shift for this manufacturing plant starts at 3:00 PM and ends around 7:00 PM. The PM shift end time means that there would hardly be any trip generation during the PM peak of adjacent street traffic. Based on ITE trip generation profile for manufacturing, it will add 12.2 percent of the daily trips during the PM peak of adjacent street traffic, which comes out to be 1 trip. (exhibit 2). This study has conservatively estimated the higher figure based on ITE trip generation manual assuming an earlier shift end time.

The ITE daily trip distribution profile for manufacturing is attached as exhibit 2. The net daily trip generation for the manufacturing operation is calculated based on weighted average of the morning and afternoon shifts and estimated to be 28 daily trips. (exhibit 2)

TRIP GENERATION ESTIMATE FOR RETAIL OUTLET

The hours of operation for retail wholesale outlet store will be 8:00 AM to 5:00 PM and will be staffed by one employee. The retail store will be 374 square feet. The trip generation for the retail store is based manual’s square footage of the floor area as defined in the ITE trip generation manual land use code 860. (wholesale retail). The AM and PM peak hour trips in the table below refer to the trips generated by the retail store during AM and Peak hour of adjacent street traffic.

AM Peak PM Peak
 Entering 0.37 0.93
 Exiting 0.18 0.83

ITE LUC 860	GFA (Sq. ft)	Trip Generation Rate			Weekday						
		Daily	AM	PM	ADT	AM Peak Hour			PM Peak Hour		
	374	***	.55	1.76	Total	Total	Enter	Exit	Total	Enter	Exit
Site Trips					6	2	1	1	2	1	1

La Morentia Totillas commercial bakery

The trip generation for retail outlet for AM and PM peak of adjacent street traffic is calculated to be less than 1 entering and 1 exiting trip. This table above has conservatively rounded it off to 1 trip each.

*** No ITE data is available for daily trips. Assumed to be eight trips per day.

TRIP GENERATION ESTIMATE FOR STORAGE AND DISTRIBUTION CENTER

The trip generation for the 21,900 square feet storage and distribution center is based on ITE trip generation manual's land use code 154 (Short-term storage warehouse)

	AM Peak	PM Peak
Entering	0.062	0.028
Exiting	0.018	0.072

ITE LUC 154	GFA (Sq. ft)	Trip Generation Rate			Weekday						
		Daily	AM	PM	ADT	AM Peak Hour			PM Peak Hour		
					Total	Total	Enter	Exit	Total	Enter	Exit
Site Trips	21,900	1.40	0.08	.10	31	2	1	1	2	1	1

NET PEAK HOUR AND DAILY TRIP GENERATION FROM THE PROPOSED DEVELOPMENT

Land use	ITE land use code	Trip Generation Rate			Weekday						
		Daily	AM	PM	ADT	AM Peak Hour			PM Peak Hour		
					Total	Total	Enter	Exit	Total	Enter	Exit
Manufacturing	140	2.47	0.37	0.33	28	3	2	1	3	1	2
Retail outlet	860	.55	0.55	1.76	8	2	1	1	2	1	1
Storage and distribution	154	1.40	0.08	0.10	31	2	1	1	2	1	1
Total					67	7	4	3	7	3	3

CONCLUSION:

1. The net traffic impact of the proposed relocated tortilla factor on the roadway network will be minimal due to proximity of the existing factory to the relocated factory already impacting the network.
2. The total trips generated by the proposed development during the AM and PM peak hours of adjacent street traffic are seven and seven vehicles respectively.
3. The total daily trips generated by the proposed development is estimated to be 67 trips per day.

4. The trips generated by the proposed development during either AM and PM peak hours of adjacent street traffic is less than 100 vehicles per hour and less than 1,000 daily trips.
5. Based on the trip generated by the proposed development a traffic impact study is not required.

If you have any question, please contact me at (360) 513-1074 or e-mail me at Trafficconsultnw@gmail.com.

Sincerely;

Ejaz Khan; P.E.; T.E.
Traffic Engineer

CC: Dago Garcia, P.E, City Engineer

Encl: Site map
Trip distribution profile worksheet.



08-08-20

RENEWAL: 06/30/2021

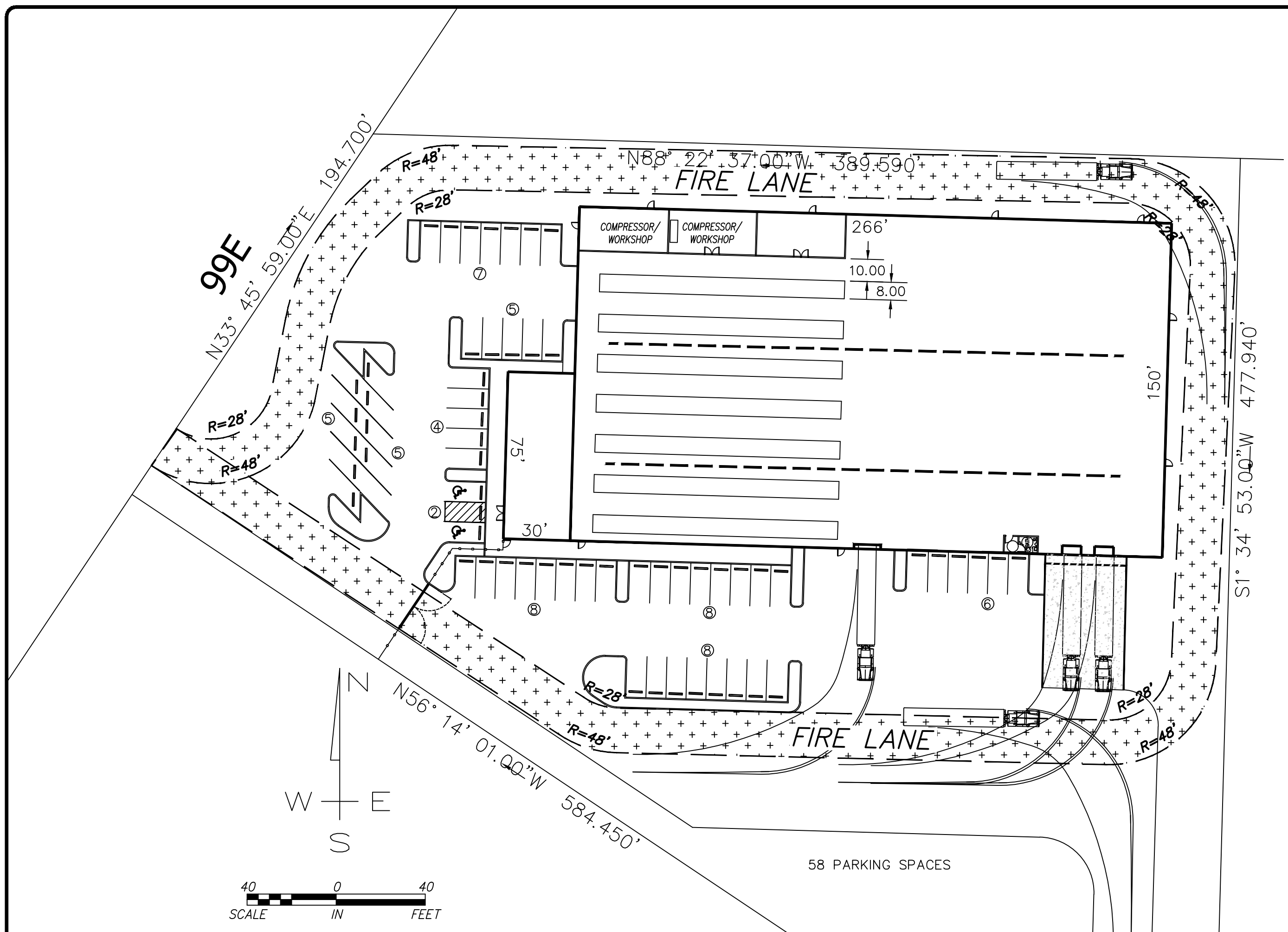
LA MORENITA TOTILLAS WOODBURN, OREGON

149,581 SF OR 3.43 ACRES
TAX LOT(S) 051W8A 4900/5000/5100

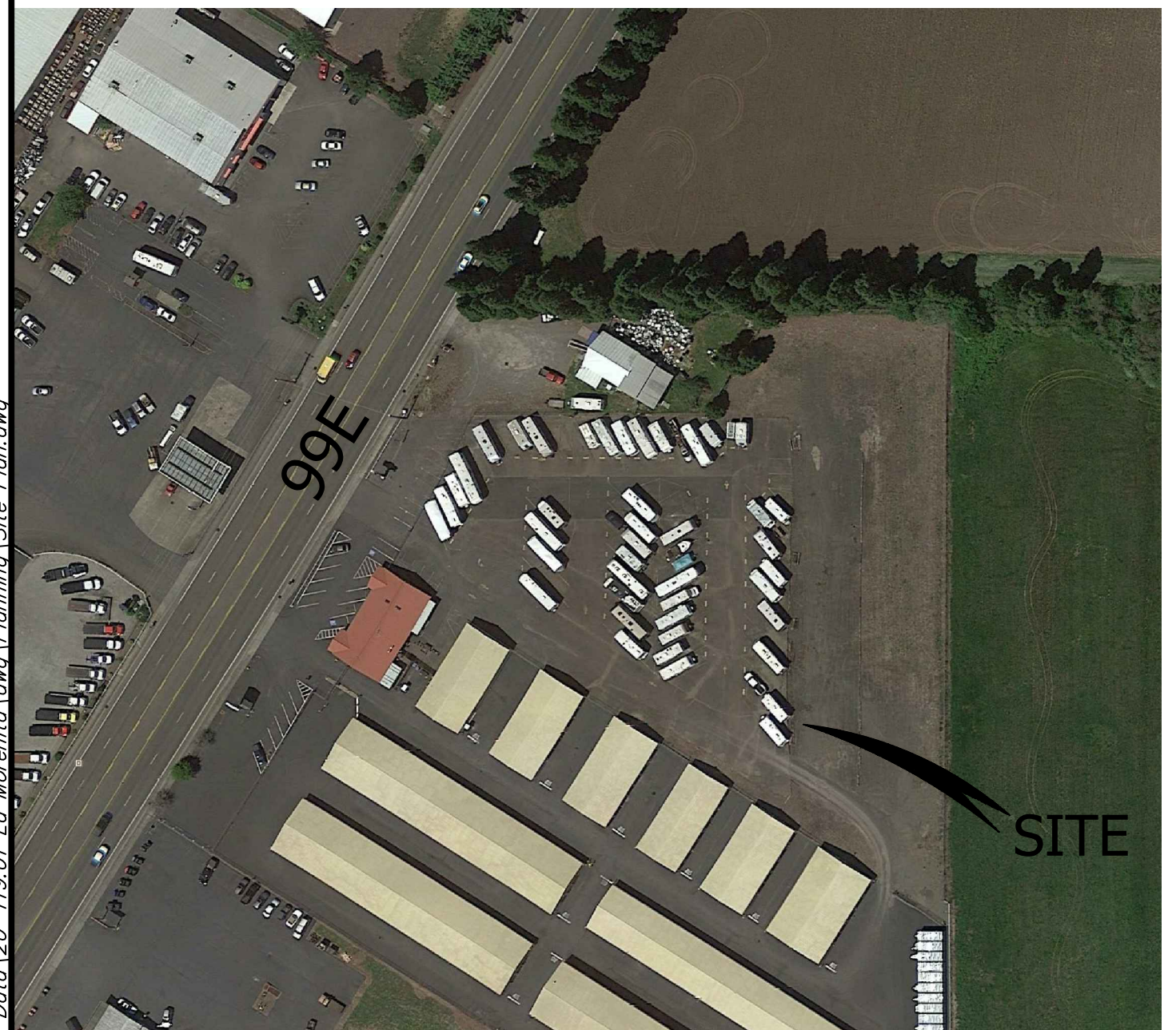
APPLICANT:
GILBERTO DIAZ
270 GRANT ST.
WOODBURN, OR 97071
O: (503) 982-3959
EMAIL: GIL.1DIAZ@HOTMAIL.COM

SITE PLANNERS/ENGINEERS/SURVEYORS:
WELKIN ENGINEERING, P.C.
ED CHRISTENSEN, P.E.
25260 SW PARKWAY AVE., SUITE G
WILSONVILLE, OR 97070
EMAIL: EKC@WELKINPC.COM
PHONE: 503.380.5324

BUILDING AREAS:
RETAIL: 386 SF
OFFICE: 3,906 SF
MANUFACTURING: 18,000 SF
WAREHOUSE: 21,900 SF



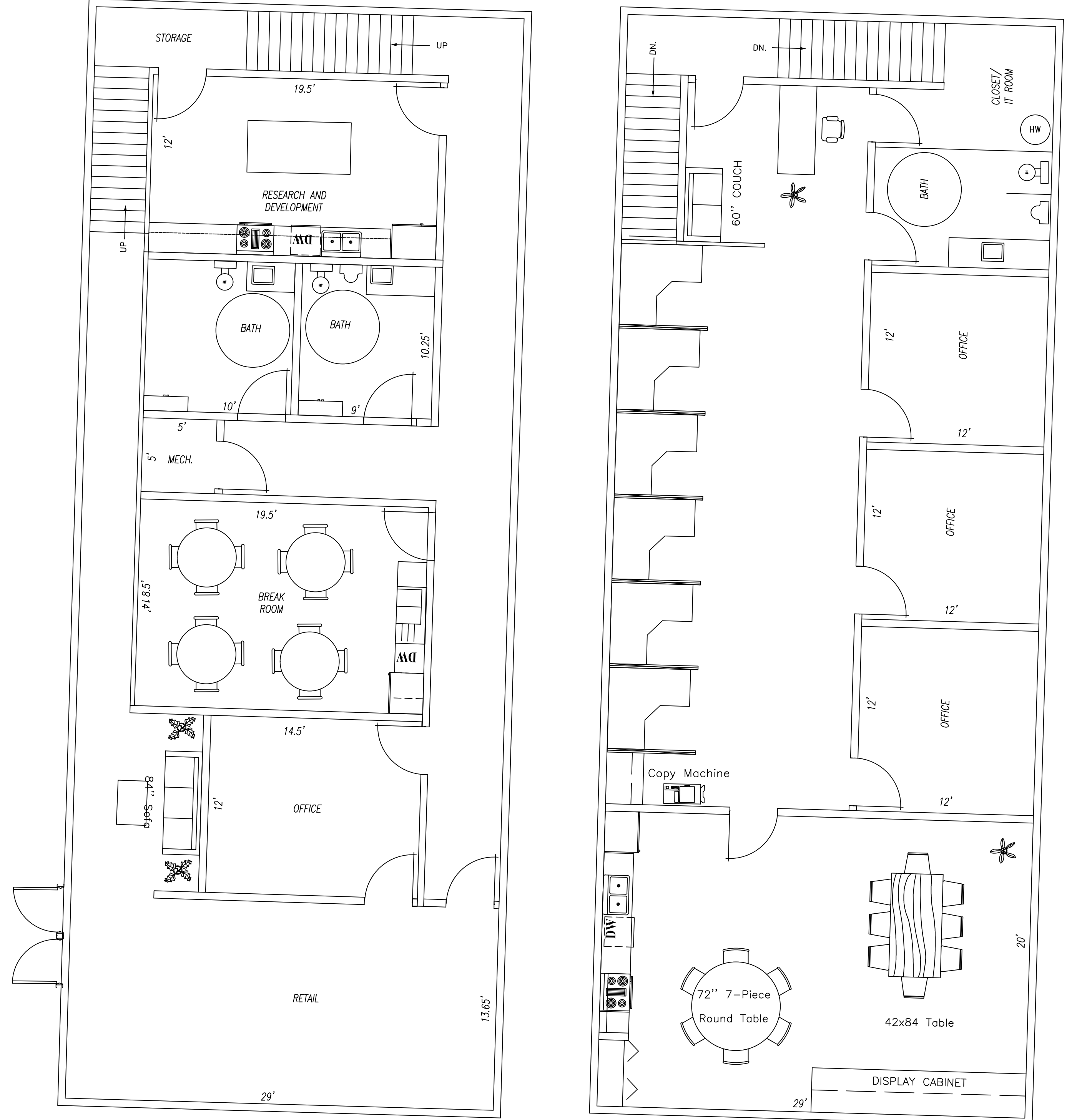
SITE AND PARKING PLAN



VICINITY MAP - NTS

Commercial General (CG) - Site Development Standards Table 2.03C	
Lot Area, Minimum (square feet)	No minimum
Lot Width, Minimum (feet)	No minimum
Lot Depth, Minimum (feet)	No minimum
Street Frontage, Minimum (feet)	No minimum
Front Setback and Setback Abutting a Street, Minimum (feet)	5 ¹
Side or Rear Setback, Minimum (feet)	Abutting RS, R1S, or RM zone
	Abutting CO, CG, DDC, NNC, P/SP, IP, SWIR, or IL zone
Setback to a Private Access Easement, Minimum (feet)	5
Lot Coverage, Maximum	Not specified ²
Residential Density (units per net acre)	Row house
	Child care facility, group home, or nursing home
	Multi-family dwelling
	Stand-alone
	In mixed use development
	No minimum
Building Height, Maximum (feet)	Row house
	Child care facility, group home, or nursing home
	Multi-family dwelling
	Stand-alone
	In mixed use development
	70
Primary or accessory structure	Outside Gateway subarea
	Western Gateway subarea
	Eastern Gateway subarea
Features not used for habitation	100

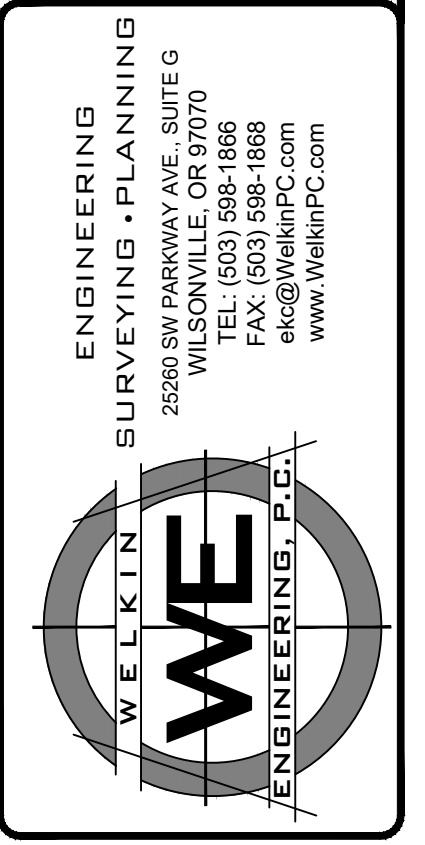
- Measured from the Special Setback (Section 3.03.02), if any
- Lot coverage is limited by setbacks, off-street parking, and landscaping requirements.
- Only allowed in the Gateway Overlay District
- A house of worship shall be set back at least 20 feet from a property line abutting a residential zone or use.
- A building may be constructed at the property line, or shall be set back at least five feet.



1ST FLOOR **2ND FLOOR**

OFFICE AND RETAIL PLAN

EXHIBIT 1



LA MORENITA TORTILLAS, INC.
WOODBURN, OR
PRELIMINARY SITE PLAN

DESIGNED BY:	EKC
DRAWN BY:	
CHECKED BY:	
DATE:	
SCALE:	HORIZONTAL
	VERTICAL
PROJECT NO:	20-119.07
SHEET	1

DATE PLOTTED: Feb 26, 2020 - 5:17pm
WFPC DRAWING FILE: P:\Project Data\20-119.07-La Morenita.dwg\Planning\Site Plan.dwg

	A	B	C
1	Hourly Distribution of Entering and Exiting Vehicle		
2	Trips by Land Use		
3	Source: ITE <i>Trip Generation Manual</i> , 10th Edition		
4			
5	Land Use Code	140	
6	Land Use	Manufacturing	
7	Setting	General Urban/Suburban	
8	Time Period	Weekday	
9	Trip Type	Vehicle	
10	# Data Sites	17	
11		% of 24-Hour Traffic	
12	Time	Entering	Exiting
13	12-1 AM	0.5	2.1
14	1-2 AM	0.3	1.5
15	2-3 AM	0.3	1.1
16	3-4 AM	0.4	0.7
17	4-5 AM	1.3	0.3
18	5-6 AM	3.1	0.1
19	6-7 AM	16.8	2.6
20	7-8 AM	13.8	3.2
21	8-9 AM	5.7	2.7
22	9-10 AM	3.6	2.9
23	10-11 AM	3.3	2.8
24	11-12 PM	6.2	4.7
25	12-1 PM	8.9	6.1
26	1-2 PM	6.8	7.0
27	2-3 PM	5.1	6.9
28	3-4 PM	9.7	20.7
29	4-5 PM	3.6	11.6
30	5-6 PM	2.7	9.5
31	6-7 PM	1.2	2.4
32	7-8 PM	1.1	1.6
33	8-9 PM	1.0	1.6
34	9-10 PM	1.3	1.9
35	10-11 PM	1.8	2.5
36	11-12 AM	1.3	3.3

AM Shift

6:00 AM to 3:00 PM = 9 hours

PM Shift

3:00 PM to 7:00 PM = 4 hours

Percent daily share of the AM shift
 $= (9/13) \times 100 = 69\%$

Percent daily share of the PM shift
 $= (4/13) \times 100 = 31\%$

Daily trips contributed by the AM shift
 $= 32 \times 0.69 = 22$ trips

Daily trips contributed by the PM shift
 $= 20 \times 0.31 = 6$ trips

Total daily trips generated by the plant
 $= 22 + 6 = 28$ trips

AM adjacent street peak hour trips generated by the plant based on daily trips = $22 \times (8.4)/100 = 1.85 \approx 2$ trips.

PM adjacent street peak hour trips generated by the plant based on daily trips = $6 \times (12.2)/100 = 0.73 \approx 1$ trip.