



Staff Report

To: Planning Commission

Through: Chris Kerr, AICP, Community Development Director *CK*

From: Colin Cortes, AICP, CNU-A, Senior Planner

Meeting Date: September 8, 2022 (Prepared September 1, 2022)

Item: 2045 Molalla Rd NE, "Woodburn Place West Apartments" (CU 22-01)

Tax Lot(s): 051W08A005200

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Issue before the Planning Commission

Conditional Use CU 22-01 (Type III) with proposed site development DR 22-08 Woodburn Place West Apartments: Commission decision.

Executive Summary

Location

The proposal is to approve the conditional use (CU) of apartments on the subject property that is zoned Commercial General (CG), composed of one tax lot of 9.62 acres, and addressed as 2045 Molalla Road NE. The vacant property is located at northeast city limits along the north side of Oregon Highway 211 (OR 211), also known as Molalla Road, at June Way.



Subject outlined in green

Design Review

The applicant proposes redevelopment into Woodburn Place West Apartments, a conventional new construction apartment complex of 258 apartments across 22 buildings. It is a de facto expansion of Woodburn Place Apartments at 2145 Molalla Road, which the City Council approved as Woodburn Eastside Apartments through Annexation ANX 2019-01 & CU 2019-04 in May 2021.

Conditional Use

The applicant/developer applied for a conditional use (CU) for the apartments themselves.

The City Council adopted Ordinance No. 2573 in 2019 to no longer allow apartments as a permitted use in CG and instead to prohibit it in that area of CG near I-5, continue to permit it in the Gateway Overlay District that is just east of downtown, and most relevant to the subject proposal to allow it conditionally elsewhere within CG.

A “conditional” use is called such because (1) it’s conditional upon discrete approval by the City, and (2) the City can condition physical or operation aspects of a proposal, including on issues particular to the case at hand and above and beyond what Woodburn Development Ordinance (WDO) provisions directly address.

The Proposal

Staff and the developer have worked to produce a good site development by focusing on several things:

1. Site layout, including common area improvements and lush landscaping in the front yard;
2. Frontage/street improvements including sidewalk and street trees;
3. Off-site sidewalk improvements connecting the frontage to the west and south with existing bicycle and sidewalk infrastructure at OR 99E;
4. Additional off-site improvements or fees in lieu of construction to alleviate traffic congestion and improve walking, cycling, local and regional bus ridership, and vanpooling; and
5. Identification and reservation of part of Street Corridor “C” and all of Street Corridor “J” for future streets per the City Transportation System Plan (TSP).

The staff analyses and findings (Attachment 102), especially the Conditional Use Provisions section, provide much more detail, and the recommended conditions of approval secure the above things.

Site Plan

A site plan excerpt follows on the next page, and a larger version is among the attached site plans (Attachment 103).



Site plan (Sheet SDR3 excerpt colored)

Staff finds that the proposal meets applicable Woodburn Development Ordinance (WDO) provisions per the analyses and findings (Attachment 102).

Recommendation

Approval: Staff recommends that the Planning Commission consider the staff report and attachments and approve the consolidated applications package with the conditions that staff recommends.

Conditions of Approval

The conditions are copied from towards the end of the analyses and findings (Attachment 102):

General

G1. As part of building permit application, the applicant shall submit revised site plans meeting the WDO and conditions of approval and obtain Planning Division approval through sign-off on permit issuance.

G2. The applicant or successors and assigns shall develop the property in substantial conformance with the final plans submitted and approved with these applications, except as modified by these conditions of approval. Were the applicant to revise plans other than to meet conditions of approval or meet building code, even if Planning Division staff does not notice and signs off on building permit issuance, Division staff retains the right to obtain restoration of improvements as shown on an earlier land use review plan set in service of substantial conformance.

G3. References: Attachment 201 serves as a dictionary or glossary defining certain abbreviations, acronyms, phrases, terms, and words in the context of the conditions of approval. The 200 series of attachments are as binding as the conditions of approval in the main body of the final decision.

G4. Due dates / public improvements:


- a. When public street improvements, and any fees in lieu of public improvements, are due shall be per WDO 3.01.02E and 4.02.12 unless if and where a condition of approval has more restrictive timing. By this condition, there is more restrictive timing: In any case, the improvements are due no later than by Building Division issuance of certificate of occupancy (C of O) for the first of any of the buildings.
- b. Where a Phasing Plan per WDO 5.03.05 is relevant, building permit issuance means issuance for the phase in which the conditioned improvement is located.

- c. Where changes to street addresses are necessary, the developer shall apply through the Planning Division for and obtain approval of an Address Assignment Request. This is due prior to building permit application, and if and where land division is relevant, then also after recordation with County.

G5. Recordation due dates: The applicant shall apply to the County for recordations of items that the City requires no later than 6 months prior to expiration of the land use approval as WDO 4.02.04B establishes, and shall complete recordations no later than three years past the land use “final decision” date. The due date to complete recordations shall not supersede when recordations are due relative to the building permit stage.

G6. Administration:

- a. Conformance: That a land use approval does not reiterate any and each particular detail, provision, requirement, rule, spec, or standard from any of the WDO, other ordinances, resolutions, public works construction code, or department policies does not exempt development from conformance with them.
- b. Copies: The developer, including any succeeding contract, shall provide copies of documentation that a City staff person requests regardless if the documentation source is another City staff person or department.
- c. Fees: The developer shall pay fees per Attachment 203.



Conditional Use 22-01

CU1. Frontage/street improvements: These shall be:

- a. Bicycle lane: 6 ft wide min.
- b. Planter strip: 8 ft wide min, exc. curb dimension.
- c. Street trees: Per WDO 3.06.03A.1 (1:30), equaling 14 trees per frontage. For max 6 of the min trees required along the frontage, the developer may pay a fee in-lieu per Attachment 203. This fee provision shall substitute for the applicant invoking WDO 3.06.03A.3 (Director modification/relocation).
- d. Sidewalk: 8 ft wide min, which may overlap the PUE with granting of public access via either the PUE or separate easement, and with the gap between its west dead-end and the OR 211 shoulder connected with ADA-compliant pavement min width 5 ft.
- e. Electric power pole: Removal of the electric power pole at June Way and burial of lateral electric line(s) serving the development.

CU2. Walkways:

- a. Wide walkway: Per WDO 3.04.06B and:
 - (1) Extent: Min as far north as the south side of the northernmost east-west drive aisle and follow a route among the common building and min 14 of the apt buildings.
 - (2) Decorative paved areas: At least 1,500 sq ft of wide walkway shall be either paved with any of decorative bricks or pavers or paved with concrete that is scored, stamped, or otherwise treated to have a pattern. (The intent is for the developer to apply this to the proposed three circular bulges along the wide walkway and excludes the square footage of unpaved holes in the doughnut shapes of these areas.)
 - (3) Speed tables: Per WDO 3.04.06D.1.
 - (4) Trees: At each of the landings on the west side (min 5), a landscaped island min width 7.5 ft between insides of curbing and extending 14.5 ft min, exc. curb dimension. Each island shall have a tree. Along the length of the wide walkway within 10 ft of each side, total min 22 trees.
 - (5) VCA: Each crossing shall have two small VCAs, one each at the north landing, east side and the south landing, west side. The VCA triangles shall measure from 6 by 6 ft from the intersecting edges of wide walkway and drive aisle, and no parking stall shall overlap VCA.
- b. Walkways: Per WDO 3.04.06d.2, applied such that per Exhibit CU2b below min 4 crossings shall be extension of walkway poured concrete:

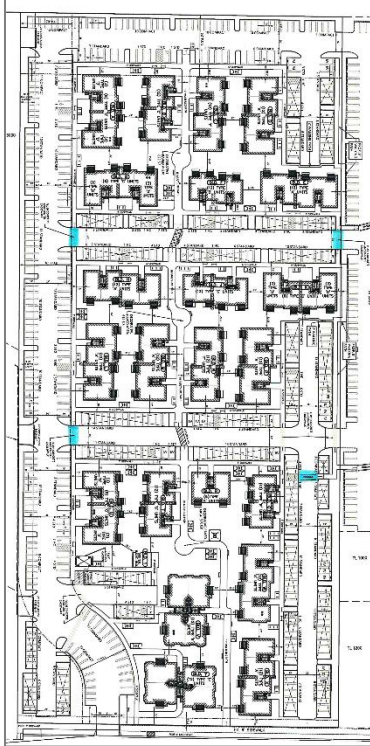


Exhibit CU2b (blue indicates extension of walkway poured concrete)

- c. Connections with Woodburn Place Apts.: Min 2 walkways shall connect east across the east lot line and across the west north-south drive aisle of Woodburn Place Apts. (2145 Molalla Rd) with the walkway system of that apartment development. The crossings of the drive aisle shall be striped per WDO 3.04.06D.2.
- d. Nominal walkways: At each of the east dead-end north-south drive aisles, a walkway min width 2 ft shall extend from the dead-ends of standard width walkways east across the east lot line to the curb of the Woodburn Place Apts. (2145 Molalla Rd) west north-south drive aisle.
- e. East walkway: The walkway that dead-ends near the site SE corner shall extend south to sidewalk.
- f. Mail shelter: The walkway that dead-ends east of the mail shelter shall extend northeasterly to the wide walkway bulge.
- g. Outermost drive aisle: A walkway shall serve the southeastern outermost drive aisle per WDO 3.05.02N.

CU3. Overhang / wheel stops:

- a. Overhang: In parking aisles along the rear and sides of the subject property, standard size stalls may be per WDO Table 3.05B as amended by Ordinance No. 2602 (18 ft instead of 19) and shall overhang curbing and landscaping by min 0.5 ft, as WDO Figure 3.05C allows up to 1.5 ft max.

- b. Wheel stops: Wheel stops are required along walkways. Wheel stops anywhere within the site development based on WDO 3.05.02H as amended by Ordinance No. 2602 shall be max 4.25 inches high.

CU4. Maximizing available parking for residents / mail carrier stall: The proposed stall designated for mail carrier parking (near Building U) shall be available for resident parking on official postal holidays, Sundays, and remaining days outside the hours of 8 a.m. to 6 p.m. A sign 1½ by 1 ft min shall note the range of hours when a space is limited to mail carrier parking and specify that it is available for resident parking outside the specified hours.

CU5. Bicycle parking:

- a. Based on WDO Table 3.05D, except at a lesser rate of 1.05 per dwelling [270 stalls], & 3.05.06.
- b. Guest: Of the stalls outdoors, min 2 stalls w/in 10 to 20 ft of ROW and along or near the wide walkway.
- c. Stairwell signage: WDO 3.05.06C.4 applies such that each stairwell that has bicycle parking requires a sign.
- d. Vertical clearance: 8 ft min from floor or grade or, where a stall is under stairs, 6 ft min.
- e. Wide walkway: Min 60 stalls distributed along the wide walkway.

CU6. EV: Electric vehicle parking shall be:

- a. Per WDO Table 3.05E [26 stalls] and 3.05.03I.
- b. Grouping/distribution: Per Exhibit CU6b below, min 2 groups per block/island. For example, the 4 along the north block/island north face may be one group of 4 or two of 2.

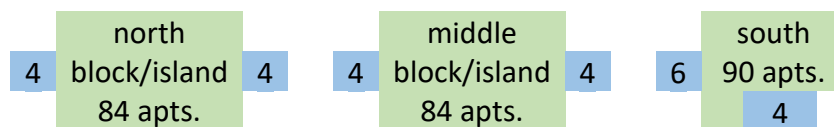


Exhibit CU6b EV distribution/grouping: max number per group

CU7. General:

- a. Bark dust: 5.0% max of landscaped area may be bark dust.
- b. Screening: Evergreen hedge or shrubbery shall be screen at-grade electrical and mechanical equipment along their sides, excepting the side intended for technician access.

CU8. Evergreen: Min 3 trees. The 3 shall be 1 min of the following coniferous or evergreen species:

Cedar, Western Red	Madrone, Pacific
Douglas-Fir	Oak, Oregon White
Fir, Grand	Pine, Ponderosa; and
Hemlock, Western	Yew, Pacific

CU9. Front yard:

- a. Trees: The front yard shall have a loose row of trees that complements the row of street trees. 12 min, with trees new to the site placed min 3.5 ft from edge of sidewalk and max 20 ft from ROW.
- b. Hedge/shrubbery: Besides that parking area screening which WDO 3.06.05B requires, the remainder of the front yard also shall have a hedge or row of evergreen shrubbery that can mature to 3 ft height. Such shall be planted and pruned to be min 2 ft from sidewalk, wide walkway, and walkways and max height 3.5 ft. Min number equal to 1 plant per 3 ft of length.
- c. Fence/fencing/free-standing wall: If any proposed in the front yard between the east and west walkways, the height limits shall be the same as if the property was residentially zoned, that is, per 2.06.02A and Figure 2.06A.

CU10. Parking area trees:

- a. South and west caps of parking aisles: Conform to WDO 3.06.03C.1.
- b. Middle: Each parking aisle shall have between the ends of the aisle at least one landscaped island that is 6 ft wide min between insides of curbing and extends 14 ft min, exc. curbing, into aisles with perpendicular or angled stalls and at least 7 ft, exc. curbing, into aisles with parallel parking. A drive aisle with parking on both sides has two parking aisles, and the wide walkway crossing landscaped islands conditioned elsewhere do not count towards this condition. Each island shall have a tree.

CU11. Trash enclosure: If any later proposed, per WDO 3.06.06B & D.

CU12. Tree preservation:

- a. ROW: Street improvements, including both frontage and off-site improvements, shall preserve any existing alive trees, including through meandering sidewalk.
- b. On-site: The developer shall pay a fee for Significant Tree removal per Attachment 203.

CU13. Balconies and patios:

- a. Patios: 8 ft min narrowest dimension and 96 sq ft min.
- b. Balconies: 8 ft min narrowest dimension and 80 sq ft min.

See also Condition D7.

CU14. Common area improvements: They shall include:

- a. Benches: 9 min, each 6 ft wide min, 75.0% min with backs, color(s) other than black or charcoal. A concrete or masonry seat wall may substitute for a backless bench for each segment that is 6 ft wide min, 1.5 ft high and deep min, and includes a cap of smoother concrete. Place 8 min benches along the access way preferably near the three circular bulges, 1 at or near the wide walkway bend that is east of the south bulge, and 1 w/in 20 ft of sidewalk. Benches shall be set back 1½ ft min from edge of access way or walkway.
- b. Picnic benches: 2 min, each square. 1 min ADA-compliant (with one of the four sides omitting a bench seat) on a paved pad. Place 1 min in the shelter.
- c. Shelter: At least 2 gazebos, pavilions, or shelters with pad (pavement options same as WDO 3.04.04), narrowest dimension of 14 ft, 225 sq ft min, ceiling height 10 ft min measured to underside of roof cross beams, and placed in the south island/block of apt buildings along or near the wide walkway. Set back columns, piers, or posts min 1.5 ft from any walkway.
- d. Parking area flood signage: Because the proposed parking area is proposed as itself the stormwater management facility such that during the 25-year storm, water would pool within curbing two inches or higher, the same as for Woodburn Place Apts., there shall be on-site signage that alerts residents:
 - (1) Min 8 signs each at a different location within both the subject property (4 min) and Woodburn Place Apts. (4 min), each location within max 10 ft of drive aisle or parking stall boundaries. Locations shall include the south and middle east-west drive aisles of both properties and near the Woodburn Place Apts. common building.
 - (2) Face min size 12 by 18 inches, mounted min 2 ft above grade and max the same as whatever the state rules are for ADA parking signage, and text size min 4 inches high. An example of acceptable text is: "Parking Lot Floods During Heavy Rain".

CU15. Buildings: It shall be:

- a. Windows:
 - (1) Area/size: Min of a simple majority of all windows proposed shall be min 20 sq ft.
 - (2) Proportion: All windows shall be square or vertically proportioned, except that horizontally proportioned windows are allowed if they have grilles or muntins dividing lights or panes so as to be vertically proportioned. Exception per (3) below.

- (3) Per room: Within apts, every habitable room abutting a building exterior wall shall have min 1 window. Each ground floor corner room proposed through land use review to one on each elevation shall continue to have 2; however, where the developer to propose additional location(s) of exterior wall-mounted utilities and meters, then for each room, the second window may shrink to min 6 sq ft and be horizontally proportioned.

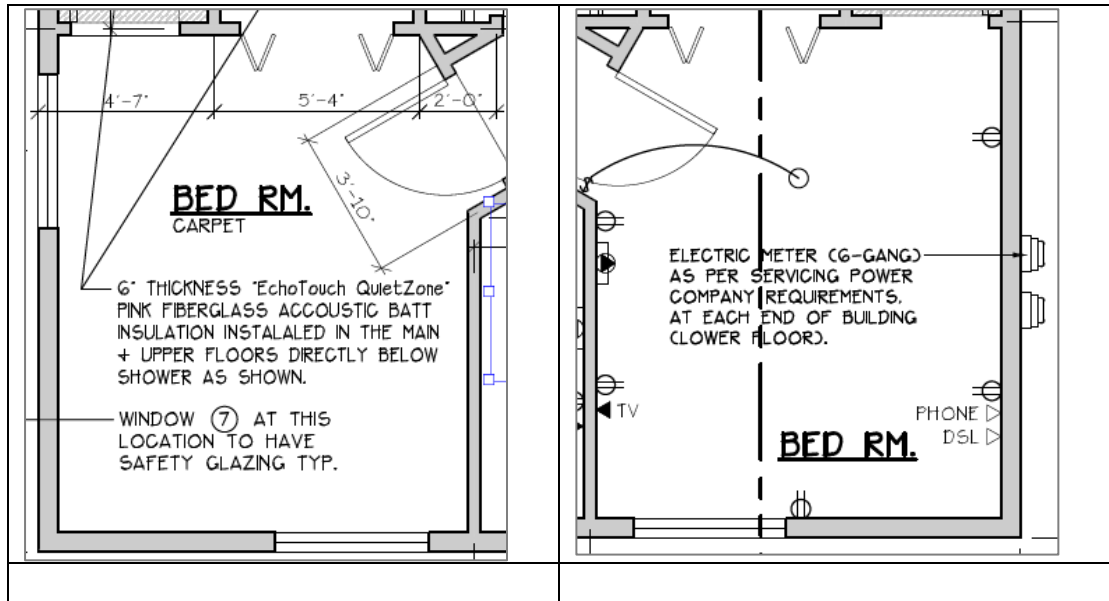


Exhibit CU15a(3)

- (4) For units with floor plans resembling Exhibit CU15a(4) below with 3 ft or more offset, each rear in the wall dividing dining/living area and patio or balcony shall have a window min width 1.5 ft and min area 7.5 sq ft approximately where the blue letter “B” indicates:

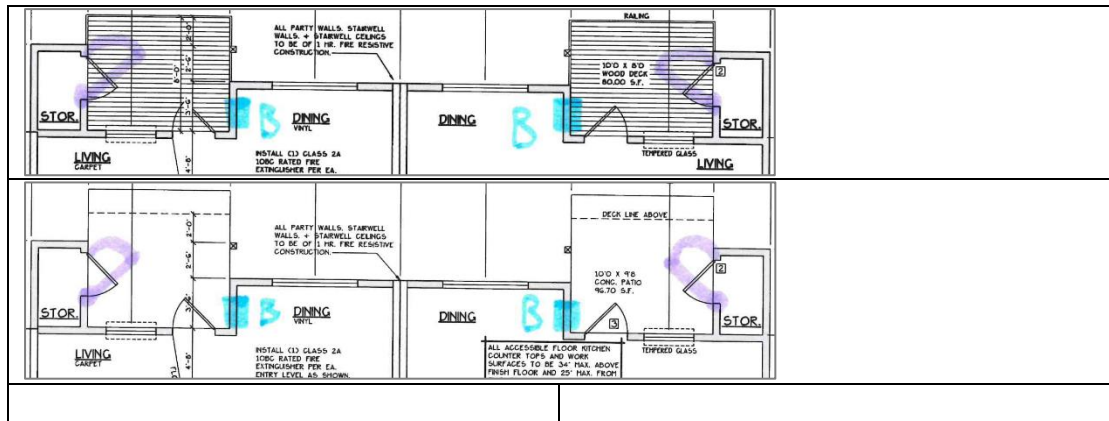


Exhibit CU15a(4)

- (5) Insect screens: All operable windows shall have insect screens.
- b. Scuppers: Any building rainwater scuppers shall not to dump onto the pavement of an access way or walkway.

CU16. Signage: If any permanent signage, particularly in the front yard, is later proposed, the allowed signage for both the subject property and Woodburn Place Apts. shall be as if the property was residentially zoned, that is, per Table 3.10.10A.

CU17. Parking management: This shall be as follows (the same as ANX 2019-01 Condition CU17):

- a. Survey: The applicant or any successor and assigns such as a property manager shall collect data about off-street parking usage or allocation and provide it to the City to the attention of the Director.
 - (1) Reporting period: Collect data by each half of a year – January through June and July through December. Submit each biannual report by the last City business day in the last month of the next quarter of a year and that is not a federal holiday. (For example, a report for January through June 2022 would be due by September 30, 2022.)
 - (2) First report: The first report shall cover whatever irregular length of time would pass between phase occupancy and the end of the next half of a year ending June or December.
 - (3) Attributes: Collect and report on:
 - (a) Geography: Report numbers divided between Phases 1 & 2 (as defined in Condition G3).
 - (b) The number of off-street spaces/stalls that are available and how many, if any, are closed due to occasional events such as parking area resurfacing, temporary outdoor events, outdoor storage, or the stationing of large trucks or truck trailers.
 - (c) Track stalls and usage by type: regular standard size, regular compact, accessible/ADA/handicap, EV, and any other type (such as those designated for visitors, leasing office employees, staff golf cart, or mail carrier).
 - (d) Collection: The property manager shall do field counts as per condition subpart (4 “field count”) below and also provide a separate set of assumed counts based on lease agreements, i.e. what tenant households are allocated a stall or stalls and for what periods, assumed that stalls are occupied as lease agreements describe.
 - (e) Usage: Report how many stalls are used and allocated. For vacant apartments in the context of assumed counts, record stalls associated with vacant apartments as unallocated.
 - (f) If and when a parking area resurfacing project were to happen, provide written notice to the Director of approximate start date and duration, location, and number of stalls involved.
 - (g) Format: Use tables to report by phase absolute numbers and percentages of stall type occupancies. Include phase and sitewide totals.

- (4) Field count: The property manager shall do at least two field counts per reporting period, meaning to travel the project and count in real time occupied and vacant stall types such as by marking a project site plan. Each count shall be on Tuesday, Wednesday, or Thursday that is neither a federal holiday nor within a week (7 days) of a federal holiday. One count shall be daytime starting no earlier than 9:30 a.m. and concluding no later than 4:30 p.m., and one count shall be nighttime starting no earlier than 10:00 p.m. and concluding no later than 12:30 a.m. Report when on a given date the counts were done and how long it took, for example, from 11:30 a.m. to noon.
 - (5) Bicycle parking: For outdoor bicycle parking stalls, including those within stairwells but excluding outdoor closets, the property manager shall also do field counts the same way as per condition subpart (4) above and as part of the larger report confirm the total number of existing outdoor bicycle stalls.
 - (6) Parking demand management: The reporting that a parking demand management condition requires, if it exists, may be incorporated with the parking usage data collection report.
 - (7) Context: In each report, cite the project name, phases, street addresses, master/parent case file number ANX 2019-01 and child case file number CU 2019-04, and the condition identification(s), state what period the report covers, state the number of vacant apartments and when and how the number was determined given fluctuation over six months, and provide an employee name and direct contact information for questions City staff might have.
 - (8) Intent: It is not the express intent of this condition to police property management or punish tenants or management for perceived misuse of parking, but instead without judgment to collect data on how parking is actually used in a conventional large apartment complex.
 - (9) Change of ownership: If and when property ownership were to change, the property manager shall pass along record of the conditions of approval to the contract purchaser and successive property manager.
- b. Expiration: This parking usage/allocation data collection condition becomes optional as of July 1, 2031. If reporting were to cease, the last report – for the January to June 2031 period – would be due September 30, 2031.

CU18. Lighting: Exterior lighting shall be:

- a. Per WDO 3.11 and, for permanent signage, per WDO 3.10.06G.
- b. Front yard: Of the south elevations of the two buildings closest to the ROW, max one exterior wall-mounted fixture and limited to Building Y, and the first 20 ft of front yard are limited to one pole-mount.

CU19. Privacy buffer:

- a. Apt building setback from highway shall be whichever is greater, either (1) 12 from ROW or (2) 10 ft from sidewalk. For accessory buildings, if any, min equal to streetside PUE.
- b. The distance between a walkway and an apt building main wall plane shall be min 6 ft.
- c. The distance between a walkway and a patio slab shall be min 3 ft.

CU20. Addressing: Addresses on apartment doors shall contain the full unit numbers per City address assignment. For example, in Building A, Apt. A101 shall read "A101", not "101".

CU21. Carports in sewer forced main easement: Regarding the existing sewer forced main that runs north-south near the west lot line, the existing easement for it that runs along the west property line (County Reel 143, Page 1685, dated 1978), and the carports proposed within the south end of the easement, at the subject property SW corner, the developer shall do one of the following:

- a. Remove or relocate the proposed carports while maintaining conformance with WDO 3.05.03F.2, due by building permit application;
- b. Through PW, rework a sewer forced main south segment to be east, sufficiently within and following a new proposed off-street PUE serving as Street Corridor "J", due the same as Condition G4; or
- c. Through PW, either obtain a revocable permit or enter into a written agreement, either legal instrument allowing for the carports to remain on terms set by PW, due by either when PW establishes or building permit issuance (of the structural permit for whatever is the first building or structure in the development), whichever is earlier.

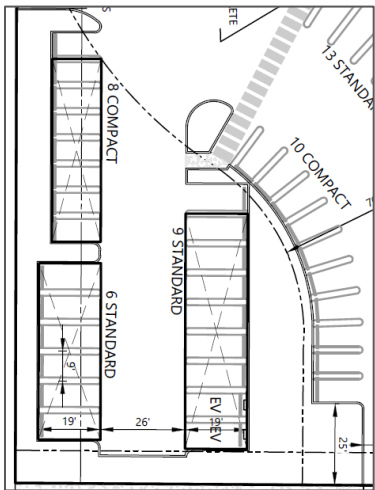


Exhibit CU21

Design Review 22-08

D1. ROW: To meet WDO Figure 3.01B, as part of recordations and regarding OR 211, the applicant shall dedicate (a) variable width ROW resulting in half-street ROW that is uniform 50 ft wide min measured from road centerline, and (b) a streetside PUE min 10 ft.

D2. Street corridors: To meet WDO 3.01.05F, as proposed as part of recordations and regarding Street Corridors "C" & "J", the developer shall reserve each future street corridor per Attachment 202.

D3. Driveways:

- a. Number: To meet WDO 3.04.03B.5 regarding access management, the number of driveways shall be limited as follows:
 - (1) 1 west, max width 30 ft, aligned with the T-intersection of the highway and June Way; and
 - (2) 1 east, max width 24 ft, towards the east lot line, but only if ODOT were to later permit a second driveway.
 - (3) If the Woodburn Fire District were to later desire a fire or emergency access driveway and ODOT to permit it, the area of driveway throat outside ROW shall be grass paving surface ("grasscrete") max width 24 ft.
- b. Approach / apron / curb cut: Driveways shall conform to PW SS&Ds, Section 4150, unless overridden by ODOT choosing to apply its standards.

D4. Cross access: To meet WDO 3.04.03B, the developer shall:

- a. Extend a drive aisle stub conforming to WDO 3.04.03C.4b to the property line along each of the following properties:

<i>Tax Lot</i>	<i>Address</i>	<i>Description</i>
051W09B000900	2145 Mollalla Rd	Woodburn Place Apts.*
051W08A005000	2400 N. Pacific Hwy	vacant; La Morenita Tortilleria Inc.
051W08A004800	2200 N. Pacific Hwy	Mini or self-storage; FCS Investment Group
051W08A004400	none	Industrial yard; Truss T Landholding LLC

*Regarding Woodburn Place Apts., the min is 3 stubs aligned with east-drive aisles on that property.

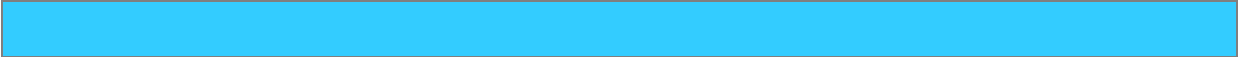
- b. At the interface of a property line and a drive aisle stub, fixed obstructions including curbing is prohibited. (The developer may instead place signed barricades atop the pavement.)
- c. To meet WDO 3.04.01A.2 and 3.04.03B.5, C.1, C.4a, & D.2, establish one or more public cross access easements revocable only with the written concurrence of the Director and that grants or grant public access to and from the main driveway at OR 211 and connects or connect with each of the min 6 drive aisle stubs.

D5. Parking striping: To meet WDO 3.05.02J, the developer shall designate compact stalls "COMPACT" in lettering one min 1 ft high.

D6. Landscaping: trees: To meet WDO Table 3.06B regarding tree minimum size at planting, the developer shall revise the landscape plan tree legend(s) to indicate for each tree species either 10-ft height or 2-inch caliper.

D7. Patios: Visual separation shall conform to WDO 3.05.06C.7 last sentence & 3.07.05B.1a as follows:

- a. Pavement: Patios shall be paved with brick, poured concrete, concrete pavers, or square or rectangular cobblestone pavers.
- b. Patio railings/fencing & gate: The outermost edges of patio concrete slabs that do not abut building walls shall have either metal or wood railings or cedar wood fencing at least 3 ft high. If the latter, then opacity shall be full, but if fencing is higher than the minimum height, it shall be no more than 90% opaque, such as by being fully opaque from grade but from the top having a lattice pattern. Gates per WDO 3.05.06C.7.
- c. Balcony railings – and where installed on patios, railings instead of fencing – shall have a bottom or near bottom horizontal member and a second horizontal member in addition to the top railing to allow residents to affix to, hang from, and thread through elements from them instead of the top of the railings. The cap or top member of balcony and patio railings, as well as the cap or top member of patio wood cedar fencing, flat and 3 inches wide min.
- d. Height maximum: The fencing or railings max height shall be either 5 ft or, where a patio faces and is within 10 ft of the wide walkway, 3.5 ft.
- e. Shrubbery: Evergreen shrubbery shall line fully the outermost edges of patio concrete slabs, except along the gated opening.



Design Review 22-08: Transportation

T-A1. OR 99E & OR 211/214:

- a. Mitigation: Similar as was for ANX 2019-01 Woodburn Place Apts. developed by the same developer, the developer shall pay a proportionate share contribution towards the same mitigation project, specifically a westbound dedicated right-turn lane or pocket that leads to OR 99E northbound based on ANX 2019-01 ODOT agency commentary (April 6, 2020).
- b. The proportionate share shall be 14.2% that equals an absolute amount per Attachment 203.

T-BP1. Off-site sidewalk improvements: To further TDM through walking, in addition to the required half-street sidewalk, the developer shall:

- a. Fill the highway south sidewalk gap within the block face between June Way and OR 99E. It is located almost wholly along the frontage of 2010 Molalla Rd (051W08DA00400) and approximately 400 to 450 ft long.
- b. Such sidewalk is not required to conform fully with WDO Figure 3.01B regarding siting relative to highway centerline and min width, and ROW dedication and streetside PUE granting are neither required nor expected. ADA compliance is expected.
- c. Sidewalk may be either curb-tight or the spatial equivalent were ODOT not to direct that there be curb, and min width along any given segment shall be any of the following:
 - (1) 3 ft, or
 - (2) Whatever min(s) ODOT establishes; andwith the proviso that along any given segment sidewalk shall be 5 ft wide where it can fit.
- d. The developer shall relocate or remove any utility boxes, cabinets, pedestals, and poles, excepting electric power poles, to accommodate sidewalk whether as necessary or as ODOT directs.
- e. Were ODOT to specify in writing standards such that sidewalk would have to meet them or not exist at all, and field constraints would prevent sidewalk from compliance, then the conditioned requirement for the off-site sidewalk would no longer be applicable. The burden of proof remains on the developer to make use of this condition part.

T-BP2. Wayfinding: To further TDM, the developer shall do one of the following:

- a. Install 1 min devices, such as signage, that provides wayfinding to bicycle routes, multi-use paths, parks, schools, and other essential destinations. If the developer were to opt for signage and assuming pole signage, sign face min dimensions shall be 2 ft by 1 ft and the placement shall be one at or near the junction of the wide walkway and sidewalk.

(Note: The developer may mimic the typical wayfinding signage the City approved for the Mill Creek Greenway as Smith Creek Development [ANX 2017-05] adapted from the City of Tualatin, Oregon greenway trail system signage which it in turn had adapted from the Regional Trails Signage Guidelines of The Intertwine Alliance, a trails coalition in the Portland metro area.)

- b. Pay a fee in-lieu per Attachment 203. [TSP B40 “wayfinding”/P62]

T-T1. Bus transit and vanpool fee: To further TDM through bus transit and vanpooling, the developer shall pay a mitigation fee per Attachment 203. [This condition relates to TSP projects T1, 2, 4, & 16, TDM1, TSP Fig. F5, and TPU projects 1, 2, 3, 11, 12, 13, 15, & 20.]

T-T2. School bus shelter: To further TDM through bus transit, the developer shall build or install a school bus shelter close to wherever the Woodburn School District bus would stop within or next to the development. Follow the direction of the school district facilities manager, or absent that default to min 48 sq ft, 6 ft narrowest dimension, and 8 ft min height clearance. Affix a min 1½ by 1 ft sign face with text min 4-inch high indicating that the shelter is a school bus stop. The shelter may be in ROW and/or a streetside PUE if ODOT allows and if set back min 1.5 ft from sidewalk. Another shelter that a developer provides for another purpose may do double duty as the school bus top shelter if it continues to meet all the other requirements, whatever they may be, for that purpose as well as this condition, and the shelter is within 240 ft of ROW. The developer may place the shelter on the Woodburn Place Apts. property (2145 Molalla Rd) as a means of meeting this condition.

T-TDM1. Car share: The same as ANX 2019-01 Condition T-TDM1, until July 1, 2025, the Director may invoke as a requirement that the property management team shall contract with a car share company or service, designate and mark a minimum number of parking spaces – which shall be at least one – for one or more shared vehicles for tenant use, and follow program details that the Director establishes as necessary to implement the requirement.

Actions

The Planning Commission may instead act on the land use application to:

1. Approve with modified conditions, or
2. Deny, based on WDO criteria or other City provisions.

Attachment List

101. Marked Tax Map
102. Analyses & Findings
- 102A. Public Works comments (Aug. 25, 2022)
103. Application materials / site plans (July 19, 2022; 11 sheets)
104. Transportation System Plan (TSP) Fig. 2 “Functional Roadway Classification”
105. Transportation System Plan (TSP) Fig. 6 “Local Street Connectivity Plan”
201. Dictionary & Glossary
202. Future Street Corridors / Street Reservations
203. Conditioned Fees

05 1W 08A

05 1W 08A
WOODBURN



MARION COUNTY, OREGON
NE1/4 SEC8 T5S R1W W.M.
SCALE 1" = 200'

LEGEND

- LINE TYPES**
- Taxlot Boundary
 - Road Right-of-Way
 - Railroad Right-of-Way
 - Private Road ROW
 - Subdivision/Plat Bndry
 - Waterline - Taxlot Bndry
 - Historical Boundary
 - Easement
 - Railroad Centerline
 - Taxcode Line
 - Map Boundary
 - Waterline - Non Bndry

- CORNER TYPES**
- + 1/16TH Section Cor.
 - ⊙ DLC Corner
 - ⊕ 1/4 Section Cor.
 - ⊕ Section Corner

NUMBERS

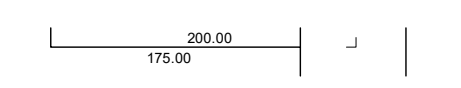
Tax Code Number
00 00 0

Acreege
0.25 AC

All acres listed are Net Acres, excluding any portions of the taxlot within public ROWs

NOTES

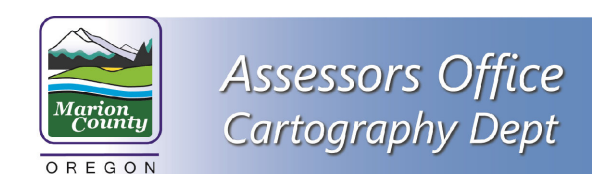
Tick Marks: A tick mark in the road indicates that the labeled dimension extends into the public ROW



Attachment 101

CANCELLED NUMBERS	
200	4300A3
500	4300A4
1100	4500
1300A1	4900
2100	5100
2200	
2600	
2900	
3100	
3200	
3400A1	
3400A2	
3500	
3900	
4000	
4200	
4300A2	

DISCLAIMER: THIS MAP WAS PREPARED FOR ASSESSMENT PURPOSES ONLY



FOR ADDITIONAL MAPS VISIT OUR WEBSITE AT www.co.marion.or.us

PLOT DATE: 4/29/2022

WOODBURN
05 1W 08A



05 1W 08A

Analyses & Findings

This attachment to the staff report analyzes the application materials and finds through statements how the application materials relate to and meet applicable provisions such as criteria, requirements, and standards. They confirm that a given standard is met or if not met, they call attention to it, suggest a remedy, and have a corresponding recommended condition of approval. Symbols aid locating and understanding categories of findings:

<i>Symbol</i>	<i>Category</i>	<i>Indication</i>
✓	Requirement (or guideline) met	No action needed
✗	Requirement (or guideline) not met	Correction needed
⊖	Requirement (or guideline) not applicable	No action needed
▲	<ul style="list-style-type: none"> Requirement (or guideline) met, but might become unmet because of condition applied to meet separate and related requirement that is not met Plan sheets and/or narrative inconsistent Other special circumstance benefitting from attention 	Revision needed for clear and consistent records
■	Variance	Request to vary from requirement

Section references are to the [Woodburn Development Ordinance \(WDO\)](#).

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Land Use & Zoning	2
Statutory Dates	3
Design Review Provisions.....	4
Conditional Use Provisions	41
Recommended Conditions of Approval.....	55
Applicant Identity.....	70
Notes to the Applicant.....	70

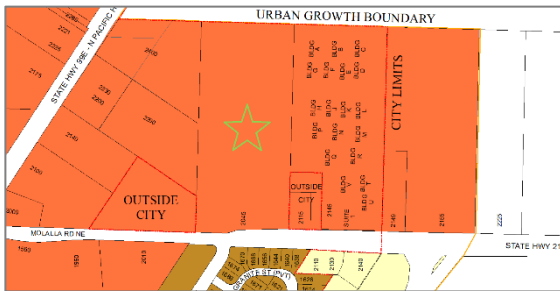
Location

<i>Address(es)</i>	2045 Molalla Rd NE (OR Hwy 211)
<i>Tax Lot(s)</i>	051W08A005200
<i>Nearest intersection</i>	Molalla Rd & June Way

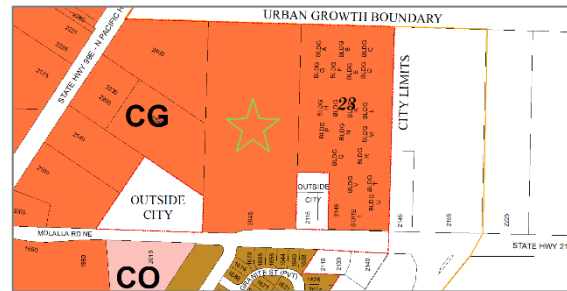
Land Use & Zoning

<i>Comprehensive Plan Land Use Designation</i>	Commercial
<i>Zoning District</i>	Commercial General (CG)
<i>Overlay District(s)</i>	None
<i>Existing Use(s)</i>	Vacant

For context, the comprehensive plan land use map designations and zoning are illustrated below with excerpts from the City geographic information system (GIS) and the zoning is tabulated further below:



Comprehensive Plan land use map excerpt; a green star marks the subject property



Zoning map excerpt

<i>Cardinal Direction</i>	<i>Adjacent Zoning</i>
North	No City zoning because not annexed and outside the City urban growth boundary (UGB); MacLaren youth state prison
East	Commercial General (CG): Woodburn Place Apartments through ANX 2019-01 & CU 2019-04
South	Medium Density Residential (RM): Easterly: Woodburn Crest Estates subdivision developed as manufactured dwelling park Westerly: Jehovah's Witnesses church
West	North: CG: vacant following demolition but approved through DR 2020-05 for redevelopment into La Morenita Tortilleria Inc industrial bakery with retail store Middle: CG: A+ Self Storage South: No City zoning because not yet annexed; would be CG; unimproved industrial operations yard

Statutory Dates

<i>Application Completeness</i>	July 25, 2022
<i>120-Day Final Decision Deadline</i>	November 22, 2022 per Oregon Revised Statutes (ORS) 227.178 . (The nearest and prior regularly scheduled City Council date is November 14, 2022.)



Design Review Provisions

The project name is Woodburn Place West Apartments. (It is a de facto expansion westward of Woodburn Place Apartments at 2145 Molalla Road, which the City approved under the project name Woodburn Eastside Apartments.)

Note: The City Council amended the WDO by Ordinance 2602 on May 9, 2022 (Legislative Amendment LA 21-01) effective June 8, 2022 and by Ordinance 2603 on June 13, 2022 effective June 30, 2022 (LA 21-02). Because the CU 22-01 & DR 22-08 consolidated application package submittal was April 22, 2022, the version of the WDO as it was last amended by Ordinance 2579 on April 13, 2020 is the one that the proposal is subject to and the version that these analyses and findings cite because of the state “goal post” rule codified in Oregon Revised Statutes (ORS) 227.178(3)(a). (That said, because a conditional use [CU] is a discretionary application type meant to be conditioned if approved, there are a number of conditions that require conformance to certain sections of the present WDO. Practically, the proposed development as drawn for land use review largely would have conformed to the present WDO anyway.)

4.01.07 Consolidated Applications

An applicant may request, in writing, to consolidate applications needed for a single development project. Under a consolidated review, all applications shall be processed following the procedures applicable for the highest type decision requested. It is the express policy of the City that development review not be segmented into discrete parts in a manner that precludes a comprehensive review of the entire development and its cumulative impacts.

5.03.02 Design Review, Type III

A. Purpose: The purpose of Type III design review is to ensure that new buildings or additions to existing buildings comply with Land Use and Development Guidelines and Standards of this Ordinance (Sections 2 and 3).

B. Type III Design Review is required for the following:

- 1. Non-residential structures in residential zones greater than 1,000 square feet in the RS, R1S, RM, and P/SP zones.**
- 2. Multi-family dwellings not meeting all architectural design guidelines and standards.**
- 3. Structures greater than 2,000 square feet in the CO, CG, MUV, DDC, and NNC zones.**
- 4. Structures greater than 3,000 square feet in the IP, IL, and SWIR zones.**
- 5. For sites with existing buildings in the CO, CG, MUV, DDC, NNC, IP, IL, and SWIR zones; expansions or new buildings that increase lot coverage by more 25%.**
- 6. Change of use that results in a greater than 25% increase in required parking.**

Because the proposal is for buildings totaling greater than 2,000 square feet (sq ft) in the CG zoning district, per subsection 3. it requires a Type III Design Review. Additionally, the applicant submitted the Type IV application type of Annexation, which per 4.01.07 cited above elevates the consolidated applications package to the highest level required among the individual application types. The applicant submitted site plans on April 22, 2022 and revised site plans through July 19, 2022 (within Attachment 103). (Staff hosted a pre-application meeting on February 17, 2022 for PRE 22-02.)

✓ The requirement is met.

2.03 Commercial Zones

A. The City of Woodburn is divided into the following commercial zones:

2. The Commercial General (CG) zone is the community’s primary commercial area, providing for businesses requiring extensive land intensive outdoor storage and display of merchandise, equipment, or inventory.

B. Approval Types (Table 2.03A)

1. Accessory Uses (A) are allowed outright, subject to the general standards of this Ordinance.

2. Conditional Uses (CU) may be allowed, subject to the general development standards of this Ordinance and conditions of Conditional Use approval.

3. Permitted Uses (P) are allowed outright, subject to the general development standards of this Ordinance.

Uses Allowed in Commercial Zones Table 2.03A		
Use		Zone
Accessory Uses (A) Conditional Uses (CU) Permitted Uses (P) Special Permitted Uses (S) Specific Conditional Uses (SCU)		CG
E	Residential	
4	Multiple-family dwellings	CU ⁹

⁹Except allowed as a permitted use in the Gateway Overlay District and prohibited in the Interchange Management Area Overlay District (Amended by Ordinance 2573, passed June 24, 2019)

The proposed use matches E.4, which is a conditional use. (The subject property is not in either overlay district.)

✓ The requirement is met.

Commercial General (CG) - Site Development Standards Table 2.03C				
Lot Area, Minimum (square feet)			No minimum	
Lot Width, Minimum (feet)			No minimum	
Lot Depth, Minimum (feet)			No minimum	
Street Frontage, Minimum (feet)			No minimum	
Front Setback and Setback Abutting a Street, Minimum (feet)			5 ¹	
Side or Rear Setback, Minimum (feet)	Abutting RS, R1S, or RM zone		10 ⁴	
	Abutting CO, CG, DDC, NNC, P/SP, IP, SWIR, or IL zone		0 or 5 ^{4,5}	
Setback to a Private Access Easement, Minimum (feet)			5	
Lot Coverage, Maximum			Not specified ²	
Residential Density (units per net acre)	Minimum	Row house		12
		Child care facility, group home, or nursing home		12
		Multi-family dwelling	Stand-alone	12
			In mixed use development	No minimum
	Maximum	Row house		24
		Child care facility, group home, or nursing home		32
		Multi-family dwelling	Stand-alone	32
			In mixed use development	32
Building Height, Maximum (feet)	Primary or accessory structure	Outside Gateway subarea		70
		Western Gateway subarea		50
		Eastern Gateway subarea		40
	Features not used for habitation		100	
<ol style="list-style-type: none"> 1. Measured from the Special Setback (Section 3.03.02), if any 2. Lot coverage is limited by setbacks, off-street parking, and landscaping requirements. 3. Only allowed in the Gateway Overlay District 4. A house of worship shall be set back at least 20 feet from a property line abutting a residential zone or use. 5. A building may be constructed at the property line, or shall be set back at least five feet. 				

Lot Dimensions

The CG zoning district has no minimum lot size, width, depth, or street frontage or maximum lot coverage.

Setbacks

Determining setbacks requires first determining what lot lines are front, sides, and rear as 1.02 defines because these influence the applying of setback minimums:

1.02 Definitions

...

Lot Line: The property lines forming the exterior boundaries of a lot.

- **Front Lot Line:**
 1. In the case of an interior lot, a line separating the lot from the street.
 2. In the case of a corner lot, a line separating the lot from the street from the architectural front of the existing or contemplated primary building.
 3. In the case of a flag lot, the lot line which is most nearly parallel to the street that provides access to the interior lot.
- **Rear Lot Line:**
 1. In the case of an irregular, triangular, diamond, or trapezoidal shaped lot which is narrowest at the rear and has a distance between the side lot lines at the rear of less than ten feet, the rear line for setback purposes shall be an assumed line within the lot ten feet in length, parallel to, and at the maximum distance from, the front lot line; or
 2. In any other case, the lot line opposite and most distant from the front lot line.
- **Side Lot Line: Any lot line, which is not a front or rear lot line.**

Based on the definition of front and rear lot lines, south is front, north is rear, and east and west are sides. The building closest to a property line is Building E at 10 ft from the east side.

Because the application materials include cross access easements, the 5-foot setback is applicable. Because all are along the centerlines of drive aisles wider than the easements and with most driveways lined curbing and most with parking stalls, buildings are set back more than 5 ft.

Density

Both the Comprehensive Plan and WDO 1.02 define density. Because they conflict – or rather did until the Council amended the WDO definition via Ordinance No. 2602 on May 9, 2022 effective June 8, 2022 to mimic that of the Comprehensive Plan – per state law the Comprehensive Plan definition supersedes and applies to CU 22-01, for which the application date was April 22, 2022. It is found as a footnote to Policy Table 1 (p. 7):

“The net buildable area of a parcel excludes land dedicated for public rights-of-way or stormwater easements, common open space, and unbuildable natural areas. For example, if a parcel has 10 acres, and 2 acres are removed for streets and 2 acres are within the floodplain / riparian area, then 6 net buildable acres would remain. The range of allowable densities is calculated based on net buildable acres. An acre has 43,560 square feet. Allowable densities may be increased through the discretionary planned unit development review process.”

The proposal falls under the residential building type / use of “multi-family dwelling” per 1.02 under “Dwellings ... Multiple-Family Dwelling”:

“A building on a single lot containing three or more dwelling units. Note: This definition does not include row houses, where attached single-family dwelling units are located on separate lots.”

The project is also stand-alone, meaning a conventional apartment complex that includes no other primary uses such as commercial retail and is more suburban in nature than urban.

Therefore, the applicable minimum and maximum densities are 12.0 and 32.0.

Looking to the proposal itself, the proposal involves no environmental constraints such as a creek, wetlands, or remnant old forest, a stormwater easement, or common open space of the kind understood in the context of a planned unit development (PUD) managed by an association that charges maintenance dues, with open space often being in its own platted tract or tracts. (Staff considers apartment complexes as having open space, but that remains a subarea of a platted lot under the direct control of a landlord and property manager, and so open space is not common open space.)

Therefore, obtaining net acreage is as simple as subtracting ROW dedication, which staff later examines for 3.01, which is a negligible southwesterly sliver compared to the lot area that staff ignores it for calculation. The resulting density is:

Acreage	Dwelling units (DUs)	DUs per acre
9.62	258	26.8

The proposed density is 74.0% into the range between 12.0 and 32.0, meeting the density provisions.

Height

The sheets illustrating building elevations note that the 22 apartment buildings are at the height limit as measured per 1.02 “Building Height” and Figure 1.02A, meeting the provision.

✓ The site development provisions are met.

2.05 Overlay Districts

⊖ None are applicable.

2.06 Accessory Structures

2.06.02 Fences and Walls

C. Height in Non-Residential Zones

1. In commercial, industrial, or public zones, the maximum height of a fence or wall located in a yard abutting a street shall be 6 feet, relative to the ground elevation under the fence or wall. Fence height may increase to 9 feet once flush with the building face, or 20 feet from street right-of-way.
2. Fences and walls may be constructed in the special setback provided the property owner agrees to removal at such time as street improvements are made.

D. Fence Materials

1. Fences and walls shall be constructed of any materials commonly used in the construction of fences and walls, such as wood, stone, rock, or brick, or other durable materials.
2. Chain link fences are acceptable as long as the fence is coated and includes slats made of vinyl, wood or other durable material. Slat may not be required when visibility into features such as open space, natural areas, parks and similar areas is needed to assure visual security, or into on-site areas in industrial zones that require visual surveillance.
3. For manufacturing, assembly, fabricating, processing, packing, storage and wholesale and distribution activities which are the principle use of a building in industrial districts, the preceding standards apply when visible from, and within 20 feet of, a public street.

2.06.03 Structures

- A. Accessory structures attached to a primary building shall be considered as a portion of the primary building and subject to the same requirements as the primary building.**
- B. The minimum separation between detached accessory structures and the primary building shall be six feet.**

The site plans propose fencing. They propose no free-standing walls for recycling and trash enclosures because the development opts to make use of the facilities in Woodburn Place Apartments at the east. Because the subject property is to be commercially instead of residentially zoned, the stair-stepped maximum heights of fencing and walls are not applicable. Fencing can and will meet standards, as well as any CU conditions of approval, through a fence permit per 5.01.03.

✓ The provisions are met.

2.07 Special Uses

⊖ None are applicable.

(Note: Staff interprets 2.07.04 Community Club Buildings and Facilities to not apply because a “community club building” [clubhouse] requires its own parking only for stand-alone clubhouses or in the context of residential subdivisions including those that are part of planned unit developments [PUDs], and that apartment complex so-called clubhouses are not subject to

the special use. Additionally, the development includes none because it opts to make use of the common building / leasing office in Woodburn Place Apartments at the east.)

3.01 Streets

3.01.02 General Provisions

A. No development shall be approved, or access permit issued, unless the internal streets, boundary streets and connecting streets are constructed to at least the minimum standards set forth in this Section, or are required to be so constructed as a condition of approval.

D. The standards of this Section may be modified, subject to approval of an Exception to Street Right-of-Way and Improvement Requirements.

3.01.04B. All public streets under the jurisdiction of the City of Woodburn shall comply with the cross-sections depicted in this Section.

3.01.04C. For local residential streets which are not identified in the Comprehensive Plan, rights-of-way and improvements are determined by the Director at the time of development, based upon the existing and future estimated average daily trips of the development and surrounding development.

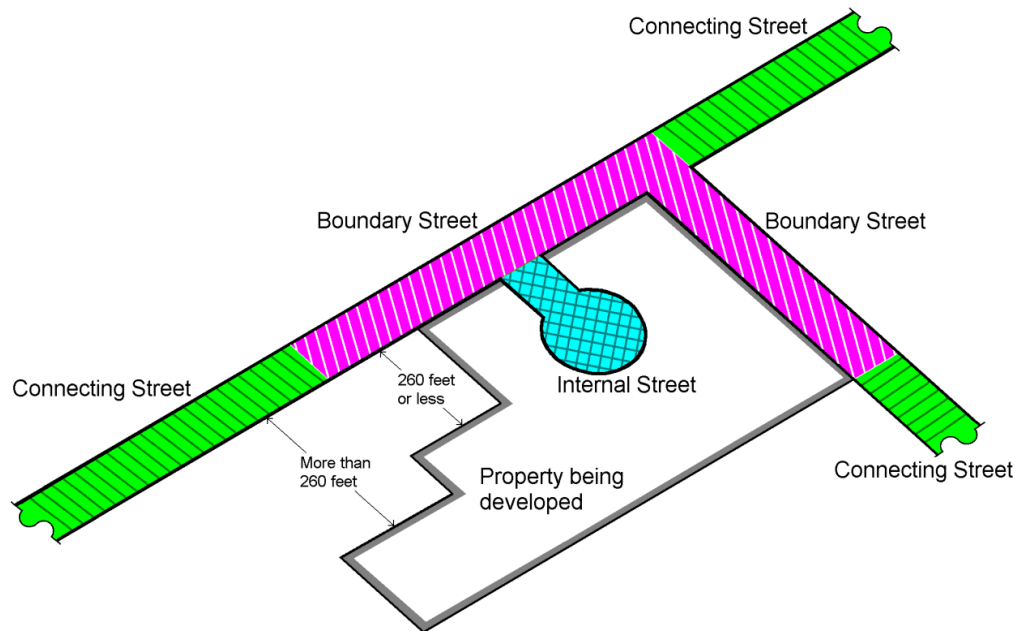


Figure 3.01A – Internal, Boundary, and Connecting Streets

The subject property has one frontage: Oregon Highway 211 (OR 211) also named Molalla Road.

Per Transportation System Plan (TSP) [Figure 2 “Functional Roadway Classification”](#) (Attachment 104), OR 211 is a Major Arterial, which is a rise from the 2005 TSP Figure 7-1 per which it was a Minor Arterial. For a Major Arterial, WDO Figure 3.01B applies:

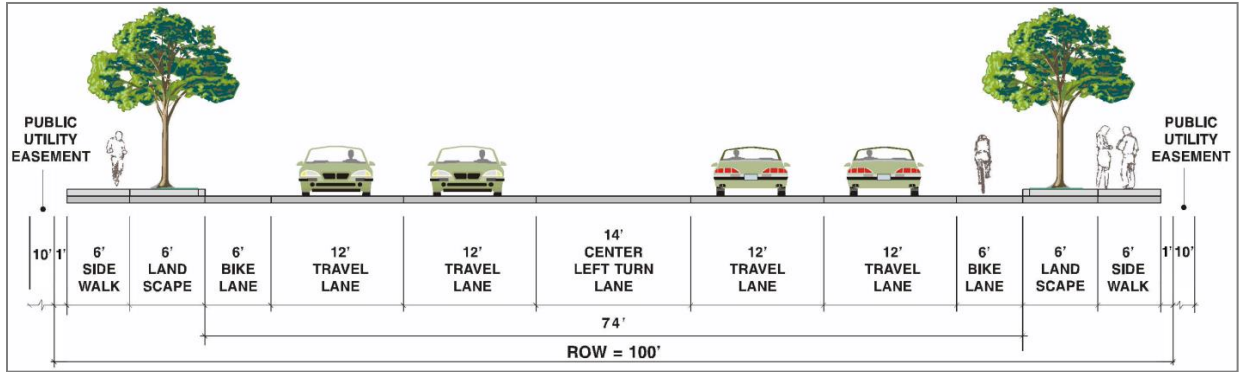
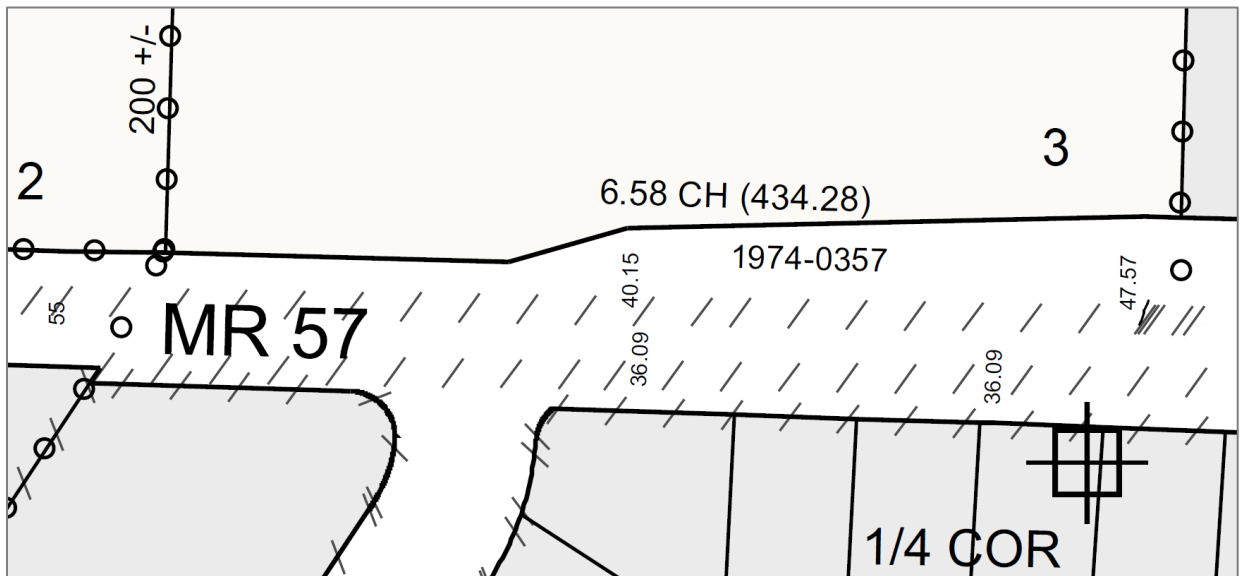


Figure 3.01B – Major Arterial

Frontage/public/street improvements are required to upgrade the frontages to present standards.

ROW

Because the planned ROW is 100 ft (50 ft “half-street”, each side of centerline), and the existing ROW varies from about 83.7 ft at the east to about 63.6 ft at the west (47.6 ft from centerline at the east and about 27.5 ft at the east), variable width dedication is required. (Rather than rely on the tax map, the developer will research or commission a survey as needed to determine exact widths and what area to dedicate as ROW.)



Tax map excerpt

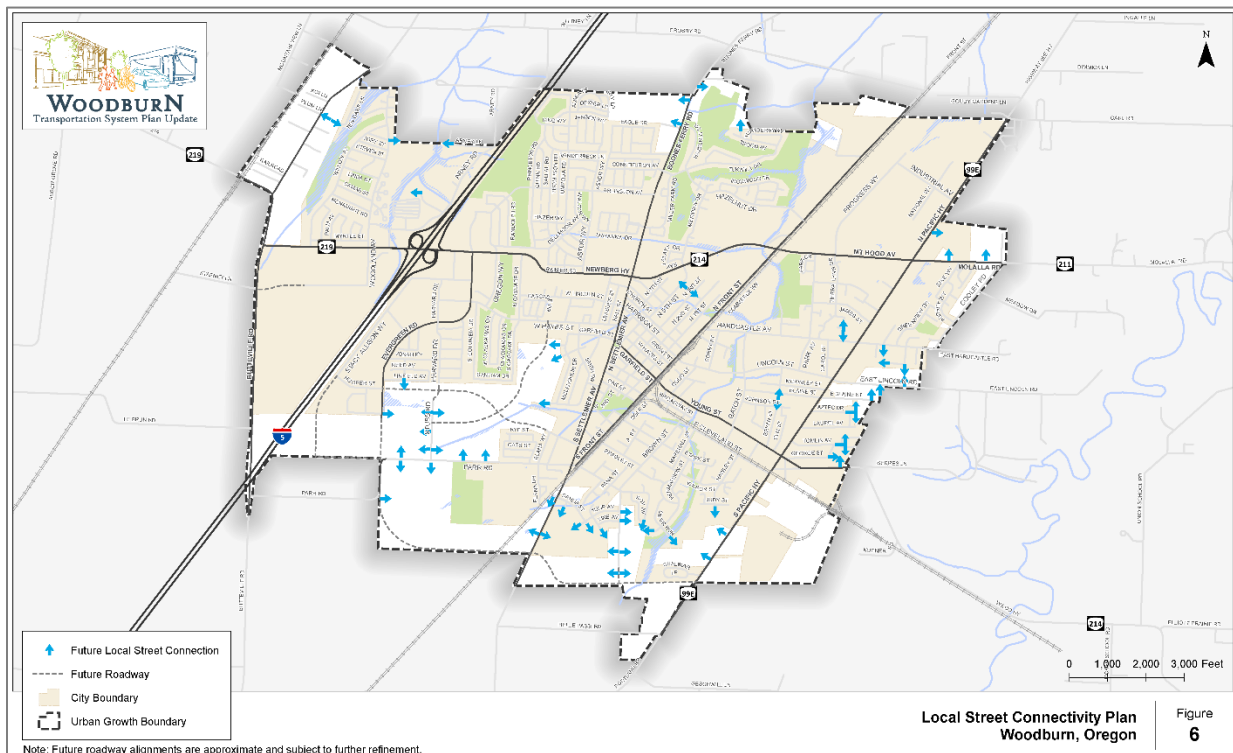
Improvements

OR 211 is a rural road with the frontage having shoulder, stormwater ditch, and no curb, planter strip with street trees, or sidewalk. (The City conditioned Woodburn Place Apartments to have off-site sidewalk, specifically to extend its sidewalk west across the frontage of most of the subject property to the east leg of the T-intersection of OR 211 & June Way.)

Per the conditional use and primarily for aesthetics and to encourage walking and accommodate the majority of local cyclists who staff observes ride on sidewalk because they feel safer that way, staff conditions a planter strip and sidewalk slightly wider than usual and more street trees than usual. Otherwise, the required improvements are the standard ones per Figure 3.01B including a second westbound travel lane and a bicycle lane.

Long-range Planning

TSP Figure 6 “Local Street Connectivity Plan” shows street connections into the northeast area of the urban growth boundary (UGB) that includes the subject property:



TSP Figure 6

There are three blue arrows, one each from Cooley Road, June Way, and U.S. 99E, representing future street corridors.

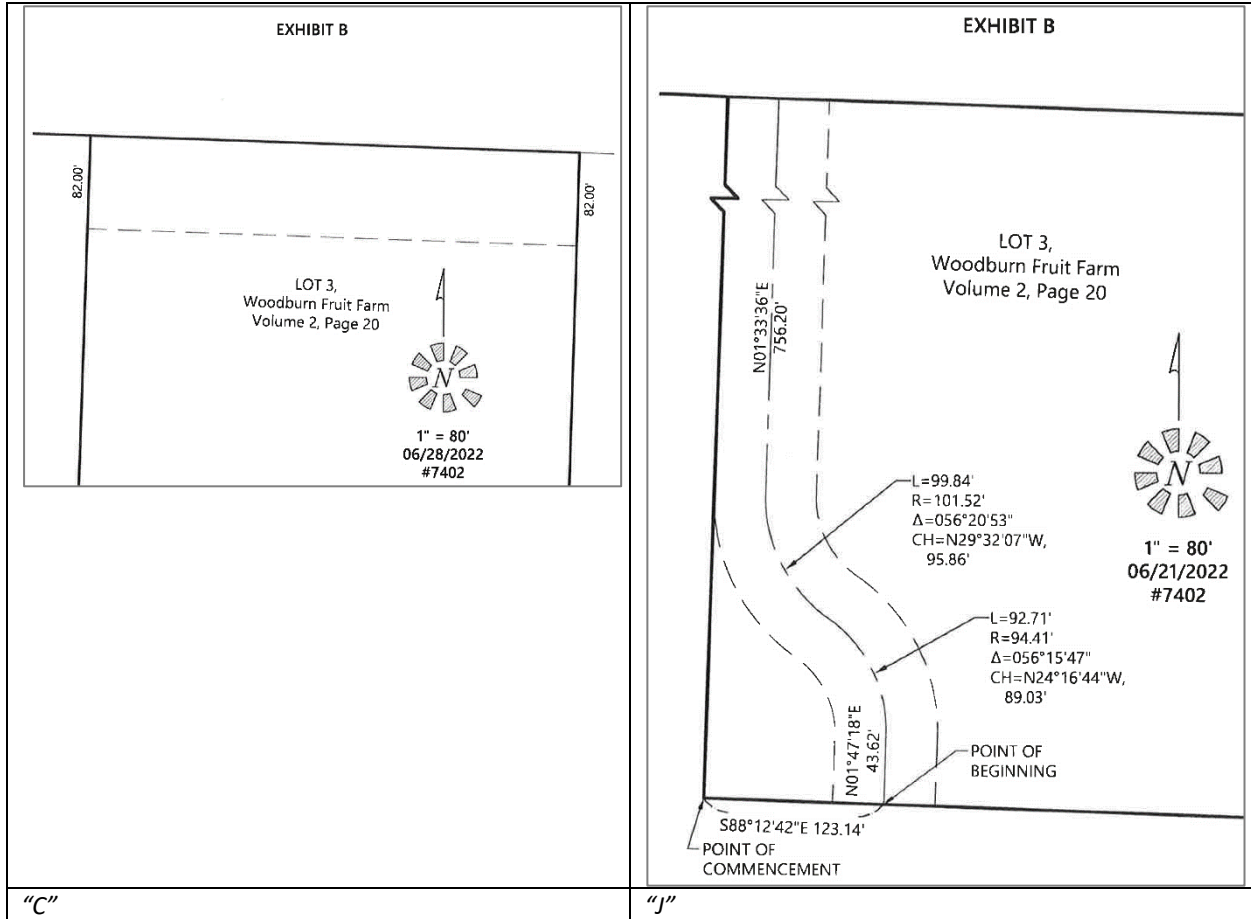
For this and additional reasons examined under conditional use criteria, staff conditions a public easement serving as a street reservation for what staff terms Street Corridor “C” (termed for the blue arrow from Cooley Road) for something like either a minor arterial or major collector as well as for what staff terms Street Corridor “J” (termed for the blue arrow from June Way) for a local class street. The developer’s proposal already includes such easements delineated on site plans and in the form of draft documents that would grant them based on City Public Works Department templates. They place Street Corridor “C” along the north

property boundary and “J” along the west boundary curving away southeasterly near the SE corner in order to align with and center on the T-intersection of OR 211 & June Way.

In this staff sketch from pre-application meeting PRE 22-02 on February 17, 2022, the north blue hatch area indicates “C” and the west blue outline indicates “J”:



Below are excerpts of the developer’s Exhibit B for each of the two draft documents that would grant the public easements:



▲ Staff applies conditions relating to ROW, sidewalk, planter strip, street trees, and Street Corridors "C" & "J" so that the provisions may be met.

3.02 Utilities & Easements

3.02.01

- A. The Director shall require dedication of specific easements for the construction and maintenance of municipal water, sewerage and storm drainage facilities located on private property.
- B. A five-foot wide public utility easement shall be dedicated along each lot line abutting a public street.
- C. As a condition of approval for development, including property line adjustments, partitions, subdivisions, design reviews, or Planned Unit Developments (PUDs), the Director may require dedication of public utility easements.

No roadside public utility easement (PUE) exists, and Figure 3.01B requires a 10-ft one.

Additionally, Public Works requires various on-site PUEs for on-site improvements such as fire suppression water lines and fire hydrants, and separate conditions establish a one or more public easements or other legal instrument to implement Street Corridors "C" & "J".

- ▲ Staff applies a *D condition* for the required roadside PUE and other conditions for other PUEs.

3.02.03 Street Lighting A. Public Streets

The appended Public Works comments (August 25, 2022; Attachment 102A) from the City Engineer identify street lighting as an issue, stating under comment 9, “Provide a street lighting plan/design for review and approval to the City, PGE, and ODOT.” Public Works will review separate from and after land use approval.

3.02.04 Underground Utilities. All permanent utility service to and within a development shall be underground, except where overhead high-voltage (35,000 volts or more) electric facilities exist.

- Because within adjacent ROWs there are no existing electric power line poles to remove (except a wood pole with a lateral line from across OR 211 to serve a demolished house that the developer will remove anyway as part of frontage improvements), the provisions are not applicable. (Out of caution, staff applies a CU condition confirming that the developer is to remove that pole.)

3.03 Setbacks and Open Space

3.03.02 Special Setbacks

- Because as examined earlier above for 3.01, there will be ROW dedication, the Special Setback – which is a setback to accommodate future street widening – is not applicable.

3.03.03 Projections into the Setback Abutting a Street

- A. Chimneys, flues, bay windows, steps, eaves, gutters, sills, pilasters, lintels, cornices, planter boxes and other ornamental features may not project more than 24 inches into the setback abutting a street.
- B. Covered, unenclosed porches, extending not more than 10 feet beyond the front walls of the building, shall maintain at least a 10 foot setback from the property line or Special Setback.
- C. A balcony, outside stairway or other unenclosed, unroofed projection may not project more than 10 feet into a front setback.
- D. Arbors, archways, pergolas and trellises shall be exempt from the setback abutting a street.

...

3.03.04 Projections into the Side Setback

- A. Chimneys, flues, bay windows, steps, eaves, gutters, sills, pilasters, lintels, cornices, planter boxes and other ornamental features may not project more than 18 inches into a side setback.

...

3.03.05 Projections into the Rear Setback

- A. Chimneys, flues, bay windows, steps, eaves, gutters, sills, pilasters, lintels, cornices, planter boxes and other ornamental features may project not more than 24 inches into the rear setback.
- B. A balcony, outside stairway or other unenclosed, unroofed projection may not project more than 10 feet into a rear setback. In no case shall such a projection come closer than 6 feet from any lot line or Special Setback.
- ...
- E. No permitted projection into a rear setback shall extend within ten feet of the centerline of an alley, or of a rear lot line if no alley exists, or within six feet of an accessory structure.
- F. Accessory structures are not considered projections into a rear setback, but have separate setback requirements listed in this Ordinance (Section 2.06).

The site plans illustrate no such projections.

✓ The provisions are met.

3.03.06 Vision Clearance Area; Figures 3.03A & B

The proposal includes the required vision clearance areas (VCA) – or sight triangles – at the driveway.

✓ The provisions are met.

3.04 Vehicular Access

3.04.02 Drive-Throughs

⊖ Because the proposal involves no drive-through, the provisions are not applicable.

3.04.03 Driveway Guidelines and Standards

A. Number of Driveways

1. For residential uses, the maximum number of driveways per lot frontage shall be one. For purposes of controlling driveway access, every 100 feet of frontage is considered a separate lot frontage.
2. A minimum of two driveways shall be provided in developments with: ...
 - b. 100 dwelling units in multiple-family dwellings (200 if all dwelling units are equipped with automatic fire sprinklers);

...

B. Joint Access

1. Lots that access a Major Arterial, Minor Arterial, or Service Collector should be accessed via a shared driveway.

2. A partition, subdivision, or PUD should be configured so that lots abutting a Major Arterial, Minor Arterial, or Service Collector have access to a local street. Access to lots with multiple street frontages should be from the street with the lowest functional classification.
3. Every joint driveway or access between separate lots shall be established by an access easement and maintenance agreement to the satisfaction of the Director and revocable only with the concurrence of the Director.

Access Management

Staff exercises the discretion per subsection B.1, to administer conventional access management. Restricting driveways along OR 211, an arterial class road, and maintaining looped circulation for fire truck and other vehicle access results in the driveway numbers and placements as proposed and a condition memorializes. Additionally, Comprehensive Plan Policy H-2.5 calls for inter-parcel circulation through crossover easements in this context.

Also, the TSP has access management policies in its AM table.

Lastly, the Highway 99E Corridor Plan (2012) emphasizes and describes access management for the corridor, which does include properties not directly on U.S. 99E including along both OR 214 and OR 211, which include the subject property.

Joint Driveway / Public Access Easement

The site plans propose public cross access easement(s) to the benefit of all adjacent tax lots (except the north – the youth prison outside city limits and the UGB) and to share the driveway as well as drive aisle stubs to property lines.

▲ In order to secure actual and correct dedication of a public cross access easement or easements that conform to 3.04.03B.3, and to limit driveways to two, staff applies conditions.

Access Requirements Table 3.04A		
		5 or More Dwelling or Living Units, School, or House of Worship ⁶
Paved Width of Driveway (feet) <small>^{3,4}</small>	1-way	12 minimum 20 maximum
	2-way	24 minimum 30 maximum (Add 8' if a turn lane is provided)
Curb Flare Radius (feet)		25 minimum

Access Requirements Table 3.04A		
Throat Length (feet) ⁵	Major Arterial, Minor Arterial, Service Collector	50 minimum
	Access or Local Street	20 minimum
Corner Clearance (feet) Guidelines ¹ (See Figure 3.04B)	Access or Local Street	30 minimum
	Service Collector	50 minimum
	Minor Arterial	245 minimum
	Major Arterial	300 minimum
Driveway Separation Guidelines (feet) ^{1, 2} (See Figure 3.04B)	Driveway on the same parcel	50 minimum
	Access or Local Street	none
	Service Collector	50 minimum
	Minor Arterial	245 minimum
	Major arterial	300 minimum
Turnarounds (See Figure 3.04C)	Access to a Major or Minor Arterial	Required
	Access to any other street	Requirements per the Woodburn Fire District

The site plans show the driveways that meet the minimum standards or exceed them where necessary to meet Oregon Fire Code (OFC) Appendix D.

✓ The minimum standards are otherwise met.

3.04.03A. Unused driveways shall be closed.

⊖ Frontage improvements will eliminate the remnant driveway of the demolished rural residence.

3.04.03C. Interconnected Parking Facilities.

Because the proposal is a single, integrated site development for one primary use – multiple-family dwelling – and not like a commercial strip mall, the provision would not have been applicable. However, because the development is a de facto expansion westward of Woodburn

Place Apartments at 2145 Molalla Road, which the City approved under the project name Woodburn Eastside Apartments, the developer intends it as such with the development reliant on Woodburn Place Apartments for its common building / leasing office and its trash and recycling enclosure, and given the context of access management per 3.04 that limits driveways for both developments, it's necessary and prudent to have drive aisles connect the two developments.

▲ Staff applies a condition for drive aisle connections between Woodburn Place Apartments and the subject development.

3.04.04 Improvement Standards

The site plans illustrate pavement that conforms.

✓ The requirement is met.

3.04.05 Traffic Impact Analysis

A. A Traffic Impact Analysis (TIA) may be required by the Director prior to the approval of a City access permit when the Director estimates a development proposal may generate either 100 or more additional, peak hour trips, or 1,000 or more additional daily trips, within ten years of a development application.

The applicant submitted a traffic impact analysis (TIA) dated April 21, 2022 on April 22, 2022 and a revised TIA dated June 30, 2022 on July 1, 2022. The TIA studied three intersections: OR 211 with each of Cooley Rd, June Way, and OR 99E.

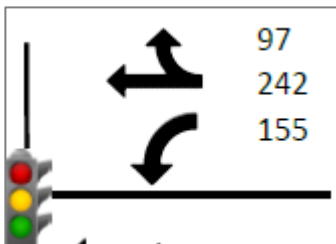
It states under Key Findings (p. 22) that, "The proposed development would generate 102 (23 in, 79 out) AM peak hour trips and 101 (62 in, 39 out) PM peak hour vehicle trips." It adds that the intersection of OR 99E / N. Pacific Hwy) & OR 211 / Molalla Rd would not meet "mobility standards under 2022 existing, 2024 background and 2024 total (with proposed development) traffic conditions", presumably referring to Oregon Department of Transportation (ODOT) standards for its roads.

The TIA, limiting its conventional analysis to vehicle traffic only, assumed citywide growth in background vehicle traffic through 2024, specifically 1.3% yearly (p. 12).

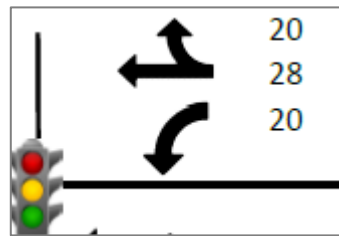
Through ANX 2019-01 & CU 2019-04 Woodburn Place Apartments at 2145 Molalla Road (final decision document dated May 24, 2021 and signed May 26, 2021), agency commentary came from the Oregon Department of Transportation (ODOT; April 6, 2020, pp. 1-2), which suggested as a traffic mitigation improvement widening OR 211 to add a right turn lane for those going west and then turning north onto U.S. 99E.

The City approved that development with the prime vehicular traffic mitigation condition being Condition T-A1 that required the developer to pay 10.1% towards a right turn lane. Staff derived that percentage from the increase of trips on OR 211 that were westbound/straight and northbound/right-turn.

For the subject development that is a de facto expansion of Woodburn Place Apartments, staff derives a percentage the same way:



Excerpt TIA Figure 2 "2022 Existing Volumes AM Peak Hour", Box 1 "99E / Molalla Rd"



Excerpt TIA Figure 6 "Site Generated Volumes AM Peak Hour", Box 1 "99E / Molalla Rd"

The TIA excerpts show during the AM peak hour at present $242 + 97 = 339$ trips. The right box shows that the traffic model predicts the development would generate $28 + 20 = 48$ additional westbound/straight and northbound/right-turn trips. $48/339$ equals a 14.2% increase.

The City contracts with a transportation consultant (from a company other than the one that prepared the applicant's TIA) to review the TIA, rebut or affirm its conclusions, and advise staff. The consultant's memo (August 1, 2022, p. 4) echoed 14.2% as the proportionate share and recommended that it equal the absolute amount of \$155,752.

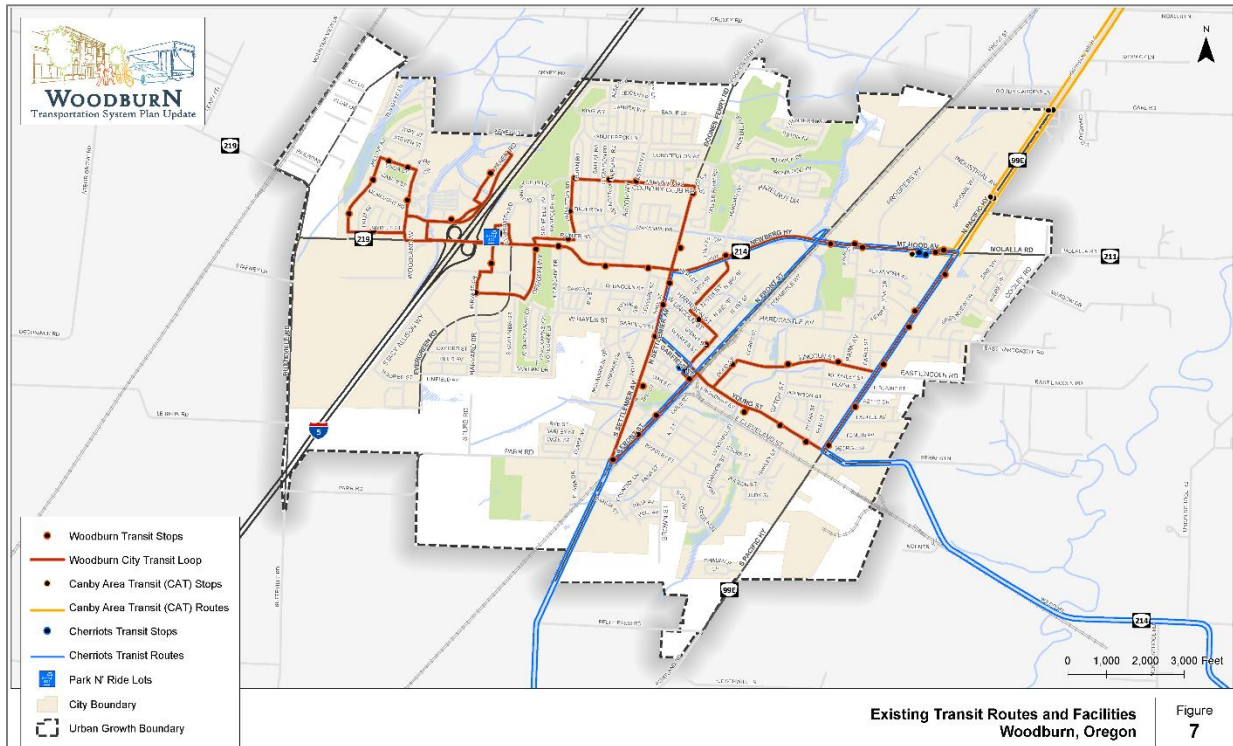
The TIA, consultant's memo, and ANX 2019-01 ODOT agency commentary as well as on-the-ground context informed staff applying a *transportation automotive (T-A) condition*.

Additional Issues: Walking & Cycling

Under the Conditional Use Provisions section farther below, staff cites Comprehensive Plan policies that – together with the TSP and Transit Plan Update (TPU) projects as described here in the Design Review Provisions section and below in the "Additional Issues: Bus Transit" subsection – advance walking, cycling, and vanpooling. Staff conditions accordingly.

Additional Issues: Bus Transit

Presently, the Woodburn Transit System (WTS) bus loops through east, central, and west Woodburn, but is yet to reach into the northeastern UGB:



TSP Figure 7 (2019)

Neither do Salem-Keizer Cherriots nor Canby Area Transit (CAT) serve it.

Below is an analysis of applicable projects from the [Transportation System Plan \(TSP\) 2019](#) major update, Table 4 “Transit Plan”:

Project Number	Location	Responsible Jurisdiction	Description	Priority	Cost Estimate
T1	Woodburn Fleet	Woodburn Transit/City	Coordinate with Woodburn Transit to deliver service enhancements funded through the STIF: Purchase of Category B and C vehicles (1 each) for use in the City's expanded transit services. (100% funding level 2020-21)	Medium	\$5,000
T2	Woodburn Fleet	Woodburn Transit/City	Coordinate with Woodburn Transit to deliver service enhancements funded through the STIF: Purchase a Category B vehicle that will replace the second oldest full-size vehicle in the WTS fleet; will be used for the City's existing local fixed route circulator. (130% funding level 2021)	Medium	\$5,000
T4	Woodburn Fixed Route	Woodburn Transit/City	Coordinate with Woodburn Transit to deliver service enhancements funded through the STIF: Modify the existing 60-minute fixed route loop; add an additional 30-minute route that will serve high frequency stops on weekdays (7am-7pm) within the Woodburn city limits. Total additional service will be up to 6,192 revenue hours (FY20-21). (100% funding level 2020-21)	Medium	\$5,000
T6	Woodburn Fixed Route	Woodburn Transit	Increase frequency of existing route to 30 minutes	Medium	\$0 ¹
T16	Woodburn	Cherriots/ City	Coordinate with Cherriots to provide a stop in Woodburn for SMART Route 1X, providing service to WES station in Wilsonville and downtown Salem	Medium	\$5,000
T18	City-wide	Woodburn Transit/ Cherriots	Evaluate all bus stops to verify static bus route information signage is visible and accessible and that bike racks are available at major bus stops	Medium	\$25,000
1. Project to be funded by others.					

(Note: STIF refers to the [ODOT Statewide Transportation Improvement Fund](#).)

A staff table below provides more method details as applied to TSP projects.

Project Number	Description	TSP Cost Estimate	Method
The TSP projects from Table 1 above except T18	See Table 1 above.	-	Similar as for ANX 2019-01 & CU 2019-04 Woodburn Place Apartments analyses and findings table, which yielded \$33.23 per dwelling total for the TSP projects from the Table 1 above except T18.
T18	Evaluate all bus stops to verify static bus route information signage is visible and accessible and that bike racks are available at major bus stops	\$25,000	<p>50 existing bus stops are WTS stops.</p> <p>Of these, the Woodburn Memorial Transit Center/Facility already has bike racks, and the DR 2019-05 Allison Way Apts. developer will install bike parking at Stop 11 (along Harvard Drive behind Walmart) at a cost of 25,000 by 49 = \$510.20.</p> <p>This leaves 48 stops, and an updated cost of $(\\$25,000 - \\$510.20) / 48 = \\$510.20$ per bus stop.</p> <p>Second, staff identified through conditions two bus stops roughly equidistant from the ANX 2019-01 & CU 2019-04 Woodburn Place Apartments site (north and west, or counterclockwise), and the one closest bus stop for east or clockwise travel, where no bike parking exists. 3 stops x \$510.20 = \$1,530.61 total.</p> <p>Staff conditioned ANX 2019-01 & CU 2019-04 Woodburn Place Apartments accordingly.</p> <p>For Woodburn Place West Apartments, there is no practical physical improvement left for the developer, who is the same developer as for Woodburn Place Apartments, to do. Therefore, there are no conditions for bus transit physical improvements.</p>

In order to induce more transit ridership, staff applies *transportation bus (T-T) condition* specifying how the developer will transit operational and service improvements.

Additional Issues: Safety Study Corridor

The TSP 2019 major update Figure 4 “Freight Routes” designates U.S. 99E as “Freight Route” and OR 211 & 214 as “Truck Route”, and Figure 5 “Traffic Safety Plan Elements” designates OR 211 & 214 as Safety Study Corridors. Staff applied a *transportation bicycle pedestrian (T-BP) condition* specifying how the developer will construct off-site sidewalk that contributes to safety along the south side of OR 211.

Additional Issues: Intercity Bus Transit

These trips are within the areas served by the Wilsonville South Metro Area Transit (SMART) transit agency, TriMet that serves the remainder of the Portland metro area and operates the Westside Express Service (WES) commuter rail line that has a terminal in Wilsonville and connects to the Metropolitan Area Express (MAX) light rail Blue and Red Lines at Beaverton Transit Center, and the Salem metro area Cherriots transit agency.

Having express busses to and from connections with SMART bus, TriMet rail, and Cherriots bus connections during morning and afternoon commutes would induce subject project residents to consider seriously riding these express busses, and were there midday service too, even more so. Additionally, Cherriots contracts with and oversees a vanpool service that serves Woodburn and both metro areas, [Valley VanPool](#).

In addition to the TSP, the City “Transit Plan Update Approved Final Report” (November 8, 2010) per its executive summary (p. ES-1) guides the provision of transit services and facilities in Woodburn through 2030 and supplements the TSP.

Below is an analysis of applicable projects:

Header	Service	Description	Annual Operating Cost Impact	Capital Needs	TPU Objective Addressed
11. Provide Peak-Only Intercity Service to Salem and Wilsonville (pp. 10-12 to 10-13)	Fixed Route	New intercity service offering three morning and three evening round trips between Woodburn and downtown Salem (weekday only)	\$150,000	\$300,000	3.4 (Transit image), 4.4 (Fixed route share), 5.2 (Expanded intercity service)
		New intercity service offering three morning and three evening round trips between Woodburn and WES station in Wilsonville (weekday only)	\$130,000	\$300,000	
12. Provide All-Day Intercity Service to Salem and Wilsonville (pp. 10-13 to 10-14)	Fixed Route	New midday service operating hourly between Woodburn and downtown Salem (weekday only)	\$130,000	None (assumes Strategy 11 implemented first)	3.4 (Transit image), 4.4 (Fixed route share), 5.2 (Expanded intercity service)
		New midday service operating hourly between Woodburn and WES station in Wilsonville (weekday only)	\$130,000	None (assumes Strategy 11 implemented first)	
20. Promote Regional Carpool/Vanpool Program (p. 10-19)	[n/a]	Promotion of existing rideshare programs to meet mobility needs that are not easy or cost effective to meet with transit.	[n/a]	[n/a]	5.3 Other travel options

Staff applies the same logic as applied to the TSP projects:

<i>Header</i>	<i>Description</i>	<i>Annual Operating Cost Impact</i>	<i>Capital Needs</i>	<i>Method</i>
The TPU projects from Table 3 above.	See Table 3 above.	-	-	<p>Similar as for ANX 2019-01 & CU 2019-04 Woodburn Place Apartments analyses and findings table, which yielded \$335.18 per dwelling total for the TPU projects from Table 3 above.</p> <p>From Tables 2 and this one, $\\$33.23 + \\$335.18 = \\$368.41$ per dwelling, which was required through ANX 2019-01 Condition T-T1.</p> <p>Staff adjusts that figure for inflation from January 2021 (the month of ANX 2019-01 first City Council hearing) to July 2022 using the U.S. Bureau of Labor Consumer Price Index (CPI) Inflation Calculator: \$417.27.</p>

As a concluding summary, based on the transportation problems that the TIA documented, the TSP, and the TPU, City objectives are for the development to contribute a fair share towards the objectives of:

- Initiation and continuation of regional express bus service
- Higher frequency service
- Bus shelter purchases
- Installation of bicycle parking where bus stops lack it
- Bus purchase(s)
- Increasing walking and cycling safety along and across OR 211, and
- Vanpooling.

▲ To address transportation problems, staff applies *transportation (T) conditions*.

3.05 Off-Street Parking and Loading

3.05.02 General Provisions

The site plans illustrate that the proposal meets the general provisions, including provision of wheel shops along head-in parking stalls adjacent to the access ways, i.e. the prime bicycle/pedestrian wide walkway route to and from sidewalk, and most if not all remaining walkways. Staff conditions on-site exterior light fixtures to be full cut-off and limit light encroachment.

3.05.03 Off-Street Parking

3.05.03 Off-Street Parking

A. Number of Required Off-Street Parking Spaces

1. Off-street vehicle parking spaces shall be provided in amounts not less than those set forth in this Section (Table 3.05A).
2. Off-street vehicle parking spaces shall not exceed two times the amount required in this Section (Table 3.05A).

B. Accessible parking shall be provided in amounts not less than those set forth in Table 3.05B. The number of accessible spaces shall be included as part of total required vehicle parking spaces.

C. A maximum of 20 percent of the required vehicle parking spaces may be satisfied by compact vehicle parking spaces.

D. Off-street vehicle parking spaces and drive aisles shall not be smaller than specified in this Section (Table 3.05C).

E. All uses that are required to provide 10 or more off-street parking spaces and residential structures with four or more dwelling or living units shall provide a bicycle rack within 50 feet of the main building entrance. The number of required rack spaces shall be one space per ten vehicle parking spaces

F. Garages ...

2. For multi-family dwellings, one-half of the parking spaces required by this Section (Table 3.05A) shall be in a garage or garages.

Off-Street Parking Ratio Standards Table 3.05A	
Use ¹	Parking Ratio - spaces per activity unit or square feet of gross floor area
RESIDENTIAL	
1. Dwellings, including manufactured homes	2/ dwelling unit
1. The Director may authorize parking for any use not specifically listed in this table. The applicant shall submit an analysis that identifies the parking needs, and a description of how the proposed use is similar to other uses permitted in the zone. The Director may require additional information, as needed, to document the parking needs of the proposed use.	

Accessible Parking Ratio Standards Table 3.05B			
Total Spaces	Minimum Total Accessible Spaces ¹	Minimum Van Accessible Spaces	Minimum "Wheelchair User Only" Spaces
301 to 400	8	[0]	1
501 to 1000	2% of total	[0]	1 in every 8 accessible spaces or portion thereof
1001 or more	20 plus 1 for each 100 spaces over 1000	[0]	
1. "Van Accessible Spaces" and "Wheelchair User Only" are included in "Total Accessible Spaces."			

The ratio yields (258 dwellings x 2 stalls) = 516 stalls. The site plan (plan sheet SDR4) notes 516 stalls, the minimum requirement.

The proposal has compact parking stalls equaling 14.5%, below the 20% maximum.

The proposal triggers subsection E. for bicycle parking, which requires $(516 / 10) = 51.6 \rightarrow 52$ bike stalls minimum; however, staff requested and the applicant opts to propose more as conditioned, both in the form of outdoor racks and, within each proposed outdoor storage closet – which are accessible from patios and balconies – a retractable hook made for wall-mounted stowage of a bike. A condition specifies details, the main idea being, “build it, and they will come.” This means if bicycle parking is plentiful, convenient, and secure, tenants would be more likely to cycle. Additionally, the City Council expanded the bicycle parking ratio through a new Table 3.05D and established standards by creating WDO 3.05.06 via Ordinance 2602 on May 9, 2022 (Legislative Amendment LA 21-01) effective June 8, 2022. The bicycle parking as proposed largely already conforms to the new WDO.

Garages / Carports

Regarding 3.05.03F.2, carports are permissible in lieu of garages. Relevant definitions are:

“1.02 Definitions

Carport: A permanent structure consisting of a roof and supports for covering a parking space which is not completely enclosed.

Garage: A building, or portion of a building, which is completely enclosed and designed for the storage or parking of a vehicle.”

Through a past multi-family development project, Woodland Crossing Apartments at 9065 Arney Lane (DR 2017-03), the Community Development Director interpreted that WDO 3.05.03F.2. allows for carports in lieu of garages. Staff applies the interpretation as the Director had:

“The language in this Section of the WDO seems to indicate that fully enclosed garages are required for even a large multi-family apartment complex that has a parking lot open to the public, such as this one. There is a distinction in the WDO between carports and garages in some areas, but it is not consistent throughout the Code. It’s important to note that when garages are required for single-family and duplex developments under the WDO, there is also an accompanying requirement to provide a ‘parking pad’ outside the garage, a minimum of 20 feet in depth (see the yellow area in the graphic above). This additional area is required to be on private property so that the driver of a car is not backing directly into traffic from their garage. In the case of parking lots – which are typical for commercial centers and multi-family parking areas - the opportunity for this critical ‘parking pad’ is impossible.

Staff has made an interpretation that the requirement for a garage in the case of multi-family dwelling units in a parking lot is satisfied with carports. This is a reasonable and practical interpretation for a variety of aesthetic and most importantly, safety factors. Having individuals opening garage doors and having cars backing into drive aisles from an enclosed garage would be dangerous and Staff would not approve it. Staff finds that the application meets the criteria and will process a clarification of this particular Code language during the next set of amendments to the WDO.”

Additionally, the City Council codified the Director’s interpretation by amending WDO 3.05.03F.2 via Ordinance 2602 on May 9, 2022 (Legislative Amendment LA 21-01) effective June 8, 2022.

The applicant proposes carports meeting the coverage provision.

Accessible/ADA/Handicap Parking

The proposal provides ADA-compliant stalls that meet or exceed the minimum provision.

- ▲ To provide for some of the bicycle parking being near sidewalk as guest parking, to ensure that it is clear the bicycle parking signage requirement applies to any stall(s) in a stairwell, to specify vertical clearance because 3.05 doesn’t, and to provide for some of the bicycle parking to be along the wide walkway that is the spine of the development, staff applies a condition.

Parking Space and Drive Aisle Dimensions Table 3.05C							
Parking Angle	Type of Space	Stall Width (feet)	Curb Length (feet)	Stripe Length (feet)	Stall to Curb (feet)	Drive Aisle Width (feet)	
						1-way	2-way
A		B	C	D	E	F	G
90°	Standard or Accessible	9.0	9.0	19.0	19.0	24.0	24.0
	Compact	7.5	7.5	15.0	15.0	22.0	
	Car Accessible Aisle	6.0	6.0	19.0	19.0	24.0	
	Van Accessible Aisle	8.0	8.0	19.0	19.0		
<ol style="list-style-type: none"> 1. A parking space may occupy up to two feet of a landscaped area or walkway. At least four feet clear width of a walkway must be maintained. 2. Space width is measured from the midpoint of the double stripe. 3. Curb or wheel stops shall be utilized to prevent vehicles from encroaching on abutting properties or rights-of-way. 4. The access aisle must be located on the passenger side of the parking space, except that two adjacent parking spaces may share a common access aisle. 5. Where the angle of parking stalls differ across a drive aisle, the greater drive aisle width shall be provided. 							

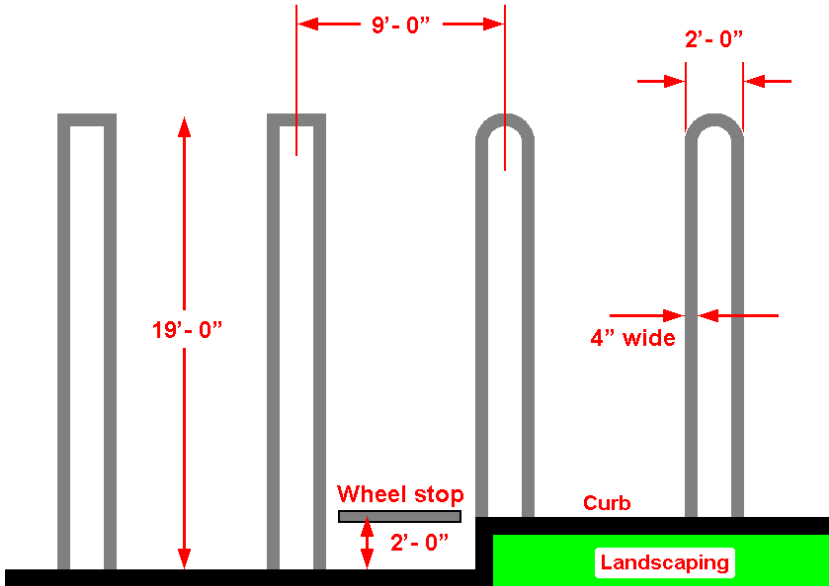


Figure 3.05C - Parking Space Striping

The applicant proposes all stalls, which are all at 90°, to the drive aisle with dimensions, double-striping, and curbing that meet or exceed the minimum.

✓ The requirement is met.

3.05.04 Off-Street Loading

B. The off-street loading facilities shall be on the same lot, or site, as the use or structure they are intended to serve. Required loading spaces and required parking spaces shall be separate and distinct, except that if authorized through a land use decision, a parking area may be used for loading during those times when the vehicle parking area is not in use.

Loading Space Requirements Table 3.05D				
Use and Area (square feet)	Minimum Number of Spaces	Minimum Size of Space (feet)		
		Width	Length	Height
Office				
0 – 4,999	0	12	30	14
5,000 – 41,999	1			
42,000 or more	2			
Nonresidential uses, except office, in the CO, CG, and NNC zones				
0 – 9,999	1	12	30	14
10,000 – 41,999	2			
42,000 – 81,999	3			
82,000 or more	4			

Because the use is residential, the table is not applicable.

⊖ The provisions are not applicable.

3.05.05 Shared Parking

⊖ The applicant opted not to exercise this option.

3.06 Landscaping

3.06.02 General Requirements

Because the application materials lack landscape plans, the requirement is not met.

▲ To secure landscaping, staff applies Condition G1.

3.06.03 Landscaping Standards

A. Street Trees

The landscape plans through Sheet L1.1 illustrates 8 street trees. 3.06.03A.1 required 1 tree per every entire 50 ft of street frontage, equaling $434 \text{ ft} / 50 = 8.6 \rightarrow 9$ trees. However, by conditional use staff applies 3.06.03A as the Council amended by Ordinance 2602 on May 9, 2022 (Legislative Amendment LA 21-01) effective June 8, 2022, which requires 1 tree per every 30 ft of street frontage within a block face, equaling $434 \text{ ft} / 30 = 14.4 \rightarrow 14$ trees.

The sheet illustrate also constraints in the form of the driveway and street lights such that 8 appears to be the feasible number within the frontage. Staff applies a condition that requires the 14, but sets a fee in-lieu for as many as 6 of the 14, establishing a de facto floor of 8 trees equal to what the sheet illustrates.

▲ To secure street trees per the amended WDO, staff applies a CU condition.

B. & Tables 3.06A & B

Planting Requirements Table 3.06A		
Location	Planting Density, Minimum	Area to be Landscaped, Minimum
Setbacks abutting a street	1 PU/15 square feet	Entire setback excluding driveways
Buffer yards	1 PU/20 square feet	Entire yard excluding off-street parking and loading areas abutting a wall
Other yards	1 PU/50 square feet	Entire yard, excluding areas subject to more intensive landscaping requirements and off-street parking and loading areas
Off-street parking and loading areas	<ul style="list-style-type: none"> • 1 small tree per 10 parking spaces; or¹ • 1 medium tree per 15 parking spaces; or¹ • 1 large tree per 25 parking spaces¹ and <ul style="list-style-type: none"> • 1 PU/20 square feet 	<ul style="list-style-type: none"> • RS, R1S, RSN, RM, RMN, P/SP, CO, CG and MUV zones: 20% of the paved surface area for off-street parking, loading and circulation • DDC, NNC, IP, IL, and SWIR zones: 10% of the paved surface area for off-street parking, loading and circulation • Landscaping shall be within or immediately adjacent to paved

Planting Requirements Table 3.06A		
Location	Planting Density, Minimum	Area to be Landscaped, Minimum
	excluding required trees ²	areas
Common areas, except those approved as natural common areas in a PUD	3 PU/50 square feet	Entire common area
<p>1. Trees shall be located within off-street parking facilities, in proportion to the distribution of the parking spaces.</p> <p>2. Required landscaping within a setback abutting a street or an interior lot line that is within 20 feet of parking, loading and circulation facilities may also be counted in calculating landscaping for off-street parking, loading and circulation areas.</p>		

The landscape plans appear to show the requirement is met.

✓ The requirement is met.

Plant Unit (PU) Value Table 3.06B		
Material	Plant Unit (PU) Value	Minimum Size
1. Significant tree ¹	15 PU each	24" Diameter
2. Large tree (60-120 feet high at maturity) ¹	10 PU each	10' Height or 2" Caliper
3. Medium tree (40-60 feet high at maturity) ¹	8 PU each	10' Height or 2" Caliper
4. Small tree (18-40 feet high at maturity) ¹	4 PU each	10' Height or 2" Caliper
5. Large shrub (at maturity over 4' wide x 4' high) ¹	2 PU each	3 gallon or balled
6. Small to medium shrub (at maturity maximum 4' wide x 4' high) ¹	1 PU each	1 gallon
7. Lawn or other living ground cover ¹	1 PU / 50 square feet	
8. Berm ²	1 PU / 20 lineal feet	Minimum 2 feet high
9. Ornamental fence ²	1 PU / 20 lineal feet	2½ - 4 feet high

Plant Unit (PU) Value Table 3.06B		
Material	Plant Unit (PU) Value	Minimum Size
10. Boulder ²	1 PU each	Minimum 2 feet high
11. Sundial, obelisk, gnomon, or gazing ball ²	2 PU each	Minimum 3 feet high
12. Fountain ²	3 PU each	Minimum 3 feet high
13. Bench or chair ²	0.5 PU / lineal foot	
14. Raised planting bed constructed of brick, stone or similar material except CMU ²	0.5 PU / lineal foot of greatest dimension	Minimum 1 foot high, minimum 1 foot wide in least interior dimension
15. Water feature incorporating stormwater detention ²	2 per 50 square feet	None
<p>1. Existing vegetation that is retained has the same plant unit value as planted vegetation.</p> <p>2. No more than twenty percent (20%) of the required plant units may be satisfied by items in lines 8 through 15.</p>		

The landscape plans through Sheet L1.3 in the legend tabulates trees and their sizes at planting. Because a few are indicated as something other than either 10-ft height or 2-inch caliper at planting, staff applies a condition.

▲ To secure landscaping, staff applies a DR condition.

3.06.05 Screening & Table 3.06D

A. Screening between zones and uses shall comply with Table 3.06D.

Architectural Wall

⊖ Because two of the lots adjacent to the subject property, at the southwest 051W08A004400 and at the north 051W090000100 (2630 N. Pacific Hwy – the youth prison), are unincorporated, they have no City zoning. Regarding the two at the west and northwest, 051W08A004800 & 5000 (2200 & 2400 N. Pacific Hwy), they are zoned CG, the same zoning as the subject property. The one at the east, 051W09B000900 (2145 Molalla Rd) also is zoned CG and is approved for development into Woodburn Place Apartments. Therefore, staff interprets that Table 3.06D is not applicable to the site perimeter and so no Architectural Wall is required at the site rear or sides.

Architectural Wall: Recycling and Trash Enclosures

Such enclosures are required per Table 3.06D for any outdoor storage of “refuse and recycling collection facilities ...”, and the applicant proposes no such outdoor storage on the subject

property because the development will make use of the facilities in Woodburn Place Apartments.

3.06.05B.

All parking areas, except those for single-family and duplex dwellings, abutting a street shall provide a 42-inch vertical visual screen from the abutting street grade. Acceptable design techniques to provide the screening include plant materials, berms, architectural walls, and depressed grade for the parking area. All screening shall comply with the clear vision standards of this ordinance (Section 3.03.06).

Because there is parking closer to OR 211 than the closest building – Building X – the provision is applicable.

The landscape plans through Sheet L1.1 illustrates screening of the west dead-end north-south drive aisle, but not of the east north-south drive aisle because the landscape plans are outdated by showing a second driveway when the site plans propose no such second, east driveway. The requirement is not met here. For this reason, staff applies a conditional use condition.

▲ To secure screening landscaping at the east, staff applies a CU condition.

3.06.06 Architectural Walls

B. Design Standards and Guidelines

Architectural Wall

None is proposed or required.

Architectural Wall: Recycling and Trash Enclosures

None are proposed.

— Because no Architectural Walls are proposed, the standards are not applicable.

C. Retaining walls should/shall meet the texture and color requirements of architectural walls in or abutting residential districts, where the texture and color requirements apply to the visible face of the retaining wall.

1.02

Abutting: Touching on the edge or on the line, including at a corner. It shall include the terms adjacent, adjoining and contiguous.

Adjacent: Near, close or bordering but not necessarily contiguous with; adjoining but separated by a right-of-way.

- Because the proposal includes no retaining walls, the provisions are not applicable.

3.06.07 Significant Trees on Private Property

The existing conditions site plan (Sheet SDR 2) illustrates 7 Significant Trees. The arborist report justifies their removal, and the constraints of conventional garden apartment development allow too little leeway to shift buildings and parking to save any. For these reasons, staff applies a condition to assess a fee for removal of each tree to fund City tree plantings elsewhere in town. The fee is based on the Planning Division fee schedule “tree credit” and adjusted for inflation from June 2008 (when Council last updated the fee schedule) to July 2022 using the U.S. Bureau of Labor Consumer Price Index (CPI) Inflation Calculator.

- ▲ To secure mitigation of Significant Tree removal, staff applies a condition.

3.07 Architectural Design

3.07.05 Standards for Medium Density Residential Buildings

Note: A medium density residential building is any building where the predominant use is multiple-family dwelling, nursing care or group care facility.

- A. At the time of application, the applicant shall choose whether the Design Review shall be conducted as a Type I, II, or III review (Section 5.01, 5.02, 5.03). For a Type I review, the criteria of this Section shall be read as “shall” and shall be applied as standards. For a Type II or III review, the criteria of this Section shall be read as “should” and shall be applied as guidelines.**
- B. Open Space**
 - 1. Private Open Space**
 - a. Ground Level Courtyard**
 - (1) Units within five feet of the finished grade, should/shall have at least 96 square feet of private open space, with no dimension less than six feet.**
 - (2) Ground level private open space should/shall be visually and physically separated from common open space, through the use of perimeter landscaping or fencing.**
 - b. Balcony**

Units more than 5 feet from the finished grade should/shall have at least 48 square feet of private open space in a balcony, with no dimension less than six feet.
 - 2. Common Open Space and Facilities**
 - a. Common open space and facilities consist of the site area and facilities not devoted to dwellings, parking, streets, driveways or storage areas that are available for use by all**

residents of a development.

- b. Required yard setbacks should/shall be included as common open space.
- c. Open Space and Facility Design Guidelines and Standards.
 - (1) A minimum of 30 percent of the net site area of each medium density residential development should/shall be permanently designated for use as common open space and facilities.
 - (2) The common area should/shall include at least one open space containing 2,000 square feet, with a minimum width of 36 feet.
 - (3) Facilities to accommodate children's or adult's recreation, meeting or education activities should/shall be provided at a ratio of 36 square feet of outdoor, or 12 square feet of indoor, common area per dwelling unit or living unit. The minimum improved common area for this purpose should/shall be 720 square feet of outdoor or 240 square feet of indoor space. The space for such improvements may be counted as part of the common area required by Section 3.07.05.B.1.c.2 at a 1:1 ratio for outdoor space and 3:1 ratio for indoor space.

C. Architectural Design Guidelines and Standards

1. Building Mass and Facade

- a. Buildings should/shall have no dimension greater than 150 feet.
- b. Every two attached dwelling or living units should/shall be offset by at least four feet in depth.
- c. Individual buildings located within 28 feet of a property line should/shall have a varied setback at least four feet.
- d. A flat roof, or the ridge of a sloping roof, should/shall not exceed a horizontal length of 100 feet without providing a difference in elevation of at least four feet.
- e. Buildings should/shall incorporate a porch or recessed entry for each ground-level dwelling or living unit. Covered porches and entries should be at least 30 square feet, with no dimension less than six feet. This provision does not apply to buildings for residential care.
- f. All habitable rooms, except bathrooms, facing a required front yard should/shall incorporate windows.
- g. Staircases providing access above the first floor level should/shall not be visible from a street.

2. Building Materials, Texture and Color

- a. The exterior finish for at least 90 percent of the facade should/shall be:
 - (1) Either siding, brick or stucco. Plain concrete, corrugated metal, plywood and sheet press board should/shall not be used as exterior finish material; and
 - (2) Either white, tinted with a minimum of 10 parts per 100 of white, or shaded with a minimum of 10 parts per 100 of black or brown. Shading colors with brown or black to create earth tones or tinting colors with white to soften the appearance.

- (3) Fluorescent, “day-glo,” or any similar bright color shall not be used on the facade.**
- b. The roofing material should/shall be either composition shingles; clay or concrete tile; metal; or cedar shingles or shakes. Composition shingles should/shall be architectural style, with a certified performance of at least 25 years.**


3. Pedestrian Circulation

- a. The internal pedestrian system in medium density residential developments should/shall connect to other areas of the site, to other building entrances and to adjacent streets.**
- b. When a residential building is sited within 24 feet of a street right-of-way, the building should/shall contain entrances directly accessible from the street.**

The site plans and building elevations show largely what the guidelines describe. By conditional use, staff applies conditions for larger patios and balconies – the same as for Woodburn Place Apartments – and specifies their delineation from common area and fencing or railings in a way that makes them more useful for tenants, such as having flat caps wide enough to set a coffee mug on and, for railings, having a bottom or near bottom horizontal member and a second horizontal member in addition to the top railing to allow residents to affix to, hang from, and thread through elements from them instead of the top of the railings, things like flower boxes, hanging plants, and holiday lights.

 To secure better private outdoor space for tenants, staff applies a CU condition.

3.07.06 Standards for Non-Residential Structures in Residential, Commercial and Public/Semi Public Zones

 Because the development includes no common building and will rely on that of Woodburn Place Apartments, the non-residential standards are not applicable.



Conditional Use Provisions

The conditional use per WDO Table 2.03A, header E Residential, row 4 is multiple-family dwellings, specifically the proposed Woodburn Eastside Apartments. (Table footnote 9 about the Gateway and Interchange Management Area Overlay Districts is not applicable to the subject property.)

Conditional Use Criteria

5.03.01 Conditional Use

B. Criteria:

1. The proposed use shall be permitted as a conditional use within the zoning district.
2. The proposed use shall comply with the development standards of the zoning district.
3. The proposed use shall be compatible with the surrounding properties.

Relevant factors to be considered in determining whether the proposed use is compatible include:

- a. The suitability of the size, shape, location and topography of the site for the proposed use;
- b. The capacity of public water, sewerage, drainage, street and pedestrian facilities serving the proposed use;
- c. The impact of the proposed use on the quality of the living environment:
 - 1) Noise;
 - 2) Illumination;
 - 3) Hours of operation;
 - 4) Air quality;
 - 5) Aesthetics; and
 - 6) Vehicular traffic.
- d. The conformance of the proposed use with applicable Comprehensive Plan policies; and
- e. The suitability of proposed conditions of approval to ensure compatibility of the proposed use with other uses in the vicinity.

CU criteria and factors executive summary

1. The proposed use of multiple-family dwellings is permitted as a conditional use within the CG zoning district.
2. The proposed use does comply with the development standards both as proposed and through conditions of approval.
3. The proposed use shall be compatible with the surrounding properties because of ...

Table CU-3 below as well as the following:

- a. The subject property, a parcel of 9.62 acres that is orthogonal, roughly rectangular, and flat, is sized, shaped, and topographically suited for a conventional new construction apartment complex.

It is located within both the Woodburn urban growth boundary (UGB) and city limits along a public right-of-way, a road improved to a rural state. The Comprehensive Plan land use map designates the northeastern corner of the UGB, the whole area east of U.S. 99E and north of OR 211, as Commercial. Most land within city limits that is zoned commercial is specifically Commercial General (CG), and most of it is along two corridors: I-5 and U.S. 99E, with bulges near the interchange of I-5 and OR 214 and the intersection of U.S. 99E and OR 214. Much of this land remains undeveloped or underdeveloped, and this is particularly so in the northeastern UGB. This appears to signal a lack of sufficient market demand, while the proposal – coupled with recent applications to the City for at least four other apartment projects that are all on CG-zoned land (DR 2019-03 Pacific Valley Apartments at 1310 N. Pacific Highway, DR 2019-05 Allison Way Apartments at Stacy Allison Way and Hooper Street, CU 2020-01 Templeton Apartments at 1430 E. Cleveland Street, and ANX 2019-01 & CU 2019-04 Woodburn Place Apartments at 2145 Molalla Road) – indicate stronger demand for multiple-family dwellings. In short, the subject property has public access, sat and sits idle, helps to meet City multiple-family housing need, and is located near the intersection of U.S. 99E and Highways 211 & 214, bringing multiple-family housing and commercial services in close proximity.

- b. The capacity of public water, sewerage, drainage, street and pedestrian facilities serving the proposed use for any given facility is either sufficient or will be after the developer upgrades either as conditioned or as the Public Works Department directs at the permit stage. The applicant’s annexation narrative (p. 4) states:

“The submitted plans show that the proposed buildings can be serviced by the infrastructure to support the development and will be designed to City standards.

Prior to construction of the site, the applicant has provided plans that identify all existing and proposed utilities. The plans show how all required utilities will be connected to existing or relocated to provide services to the proposed development.

The applicant will obtain all required permits prior to construction.”

- c. Regarding the impact of the proposed use on the quality of the living environment:

1) Noise:

None from the site development; but, to protect residents from the noise of what will remain more a highway than a street, staff conditions more street trees than usual in a planter strip wider than usual, and additional buffer space is provided through a modest minimum front setback for the two closest buildings, a hedge or row of shrubbery, and a front yard row of trees that complement the row of street trees.

2) Illumination:

The City Council adopted Ordinance No. 2602 on May 9, 2022 effective June 8, 2022 that amended the WDO by creating new Chapter 3.11 that goes a long way in properly regulating exterior lighting for development generally. Staff applies a condition for the development to conform with it and adds modest limits on lights in the front yard, between the highway and the facades of the buildings closest to the highway.

3) Hours of operation:

Because the proposal is an apartment complex, and so there are no commercial, industrial, or institutional uses, there are no hours of operation to regulate.

4) Air quality:

The City Council adopted Ordinance No. 2602 on May 9, 2022 effective June 8, 2022 that amended the WDO to create new Section 3.04.06 establishing a requirement that there be a walkway between sidewalk and a building main entrance, with standards, as well as that at least one walkway must be a wide walkway for walking and cycling. Staff applies a condition for the development to conform with this section.

Ordinance No. 2602 amended WDO 3.05 to add an electric vehicle (EV) parking requirement with standards. Staff applies a condition for the development to conform with it and distribute EV parking across the development.

Ordinance No. 2602 amended WDO 3.05 also to improve the bicycle parking requirement and set standards. Staff applies a condition for the development to conform with it largely.

Ordinance No. 2602 amended WDO 3.06.03A about street trees to increase the ratio from 1:30 to 1:50 and improve standards. Staff applies a condition for the development to conform with this section and explicitly sets a fee in-lieu were not all trees to manifest.

Staff applies conditions for additional trees on-site, a wider sidewalk as a public bicycle pedestrian path, and off-site sidewalk along 2010 Molalla Road – the remaining south side sidewalk gap between June Way and OR 99E. EVs directly help air quality, and the remaining conditions serve as transportation demand management (TDM) by inducing residents to drive less often, especially for nearby destinations in the commercial area around the intersection of U.S. 99E & OR 211, and with fewer driving trips comes better air quality.

For localized air quality, staff conditions that all operable windows have insect screens, so that residents feel free to open windows, be it to drive out stagnant or humid air or bring in fresh air, without also bringing in pests like fruit flies, flies, and stinging insects.

5) Aesthetics:

Staff applies conditions for aesthetics mostly at the site plan scale (instead of the building scale), mostly to bring buildings and common area closer together and to place surface parking away and along the site perimeter. Staff also conditions buildings generally to have a window at every typical room next to an outside wall. The 22 apartment buildings are arranged on three distinct “blocks” or “islands” of landscaping defined and surrounded by drive aisles and driveways. This, with the conditioned on-site bicycle pedestrian path serving as the central spine of the site plan, helps residents find their way within the complex. Staff conditions more street trees than usual in a planter strip wider than usual. A condition sets as standard the larger patios and balconies the site plans propose above and beyond the WDO and interprets an unclear WDO provision about how to delineate a patio space from common open space by requiring shrubbery with either fence or railings.

6) Vehicular traffic.

Staff applies conditions relating to:

- Mitigating the effect of additional vehicle trips generated by the site development through any of off-site improvements, transportation study, and/or fees in-lieu.

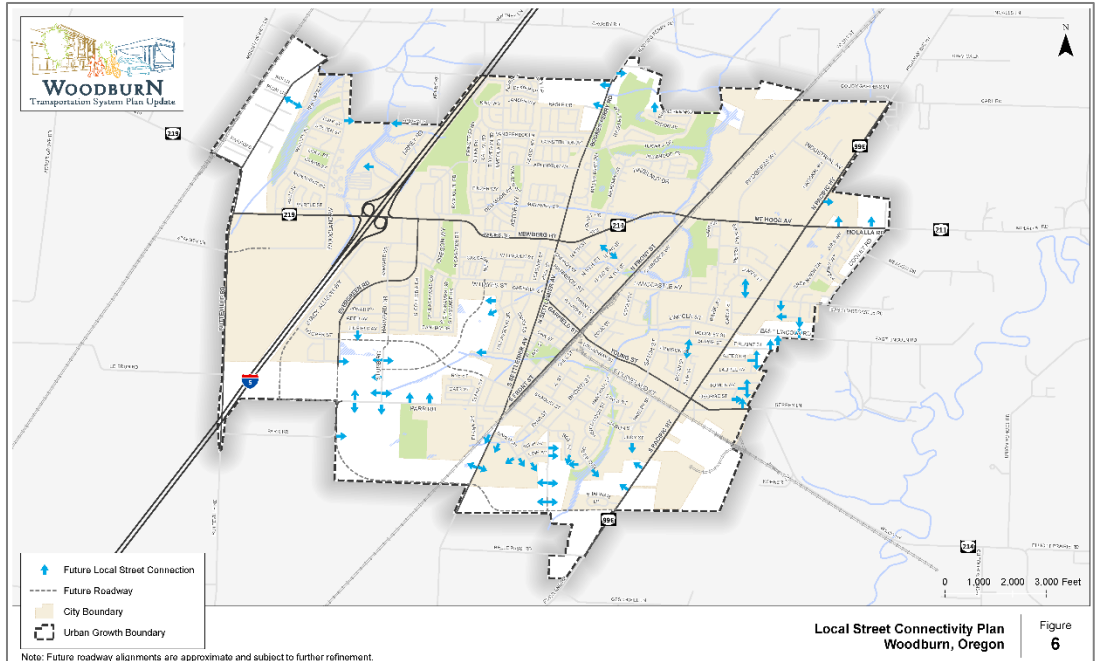
- Inducing more walking, cycling, bus ridership, and vanpooling instead of driving, including through off-site improvements, mitigation fees to improve regional and local bus transit, lots of bike parking and a bicycle/pedestrian path on-site, and vanpooling funds.
 - Surveying on-site parking usage for staff to better understand how parking is used in conventional new construction apartment complexes;
 - Granting the Community Development Director authority to require the property manager to implement a car share service with one or two dedicated parking spaces on site; and
 - Limiting driveways and reducing conflict points among turns into and out of OR 211.
- d. Regarding the conformance of the proposed use with applicable [Comprehensive Plan](#) policies, staff addresses this factor through both factor c. above (“impact of the proposed use” with six subfactors 1) through 6) and this factor d.:

<i>Table CU-3d</i>		
<i>Policy</i>	<i>Page No.</i>	<i>What Related Conditions Address</i>
D-1.3	14	Creative design, sufficient landscaped area and open space, and residential higher density near jobs, shopping and potential transit services – specifically requiring perimeter parking overhangs to shrink pavement, more and greater variety of trees, fees to mitigate tree preservation, an on-site bicycle/pedestrian path, a wide sidewalk as a public bicycle/pedestrian path, and a wide planter strip with more than usual street trees.
D-1.5	14	Creative design, slow moving traffic, and landscaping and tree planting to enhance the livability and aesthetics of the neighborhood – specifically on-site bicycle/pedestrian path speed tables and markings of drive aisle crossings.
G-1.1	27	Expansion areas of the City are served by public facilities and services with adequate capacity. Consideration of proposals that vary from City capacity standards and facility master plans shall include mitigating measures determined to be appropriate the Public Works Department – specifically to improve surface and subsurface improvements and allow walkers and cyclists to span the distance from improved frontage across OR 211 to existing sidewalk and improved City street network. This includes off-site sidewalk across 2010 Molalla Rd.
G-1.3	28	Provide an interconnected street system to improve the efficiency of movement by providing direct linkages between origins and destinations – specifically to provide in the northeast UGB area north of OR 211 and east of U.S. 99E a future street that would connect Cooley Road and either or both June Way

<i>Table CU-3d</i>		
<i>Policy</i>	<i>Page No.</i>	<i>What Related Conditions Address</i>
		<p>and U.S. 99E and that would be ROW sufficient for a minor arterial or major collector.</p> <p>For the subject property, reservations of Street Corridor “J” and a segment of Street Corridor “C” begin to implement both G-1.3 and Transportation System Plan (TSP) Figure 6 (2019; Attachment 106) and influences the overall alignment of streets.</p>
H-1.2	33	Encourage alternative travel options between Woodburn, Portland and Salem by implementing a carpool/vanpool parking program and coordinating WTS with other regional service provided by Cherrlots Regional and Canby Area Transit – specifically by conditioning fees to improve regional and local bus service (per the TSP and the Transit Update Plan of 2010) and to fund a time-limited vanpool subsidy for a modest percentage of future households within the development.
H-1.3	34	Develop a low stress network of bicycle lanes and routes that link major activity centers such as residential neighborhoods, schools, parks, commercial areas and employment centers. Identify off-street facilities in City greenway and park areas. Ensure all new or improved collector and arterial streets are constructed with bicycle lanes – specifically conditioning lots of on-site bike parking, duly requiring frontage improvements, and also conditioning a wide sidewalk as a public bicycle/pedestrian path, a wide planter strip with more than usual street trees, and off-site sidewalk across 2010 Molalla Rd.
H-1.4	34	Develop a comprehensive network of sidewalks and off-street pathways. Identify key connections to improve pedestrian mobility within neighborhoods and link residential areas to schools, parks, places of employment and commercial areas. Ensure all new collector and arterial streets are constructed with sidewalks. Specifically, to do so by conditioning lots of on-site bike parking, duly requiring frontage improvements, and also conditioning a wide sidewalk as a public bicycle/pedestrian path, a wide planter strip with more than usual street trees, and off-site sidewalk across 2010 Molalla Rd. These are all to raise the attractiveness, ease, safety, and potential cyclists’ perception of safety of cycling.
H-1.5	34	Maintain adequate intersection and roadway capacity on the key east-west and norths south arterials, in this case U.S. 99E and OR 211, both by conditioning construction and/or fees for automotive improvements but also by implementing Street Corridors “C” & “J” as examined above for Policy G-1.3 and by requiring improvements that induce more walking and cycling to and from the site development.

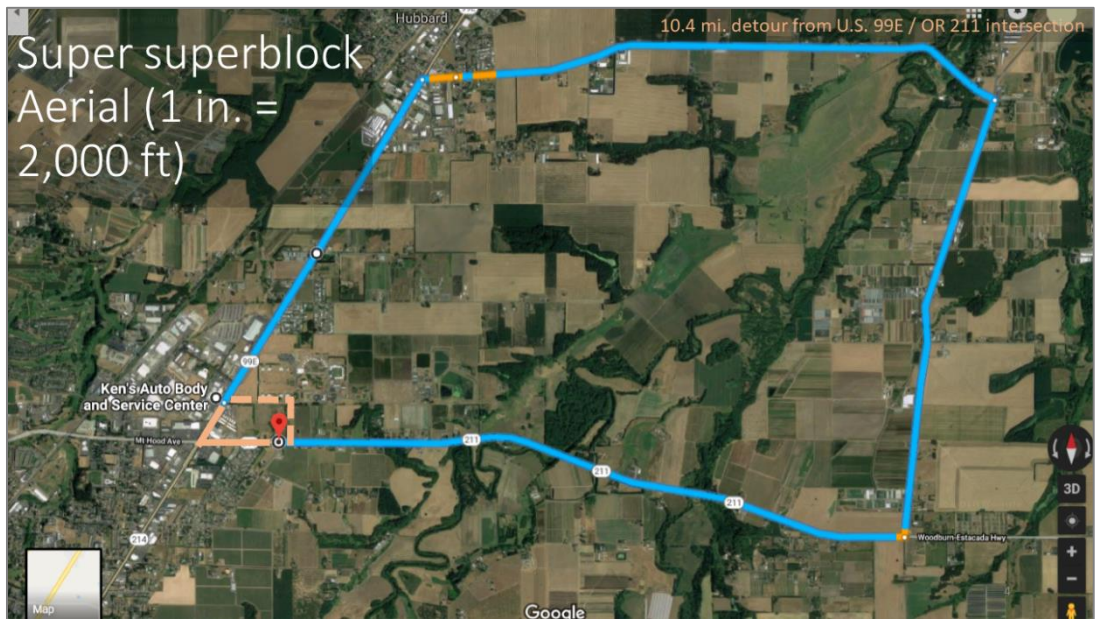
<i>Table CU-3d</i>		
<i>Policy</i>	<i>Page No.</i>	<i>What Related Conditions Address</i>
H-2.2	34	Maintain and enhance new east-west and north-south collector/minor arterial streets within the City to relieve traffic demands on OR 214 & 211 and U.S. 99E -- specifically by implementing Street Corridors “C” & “J” as examined above for Policy G-1.3.
H-2.3	34	Encourage multi-modal transportation options, including park-and-ride facilities, carpooling, and use of transit services – specifically by conditioning lots of bike parking on site, allowance for the Community Development Director to require that the property manager have a car share service – this having a basis also in TSP Projects TDM 1, 2, & 3 – and construction and/or fees to improve walking, cycling, bus ridership, and vanpooling.
H-2.5	35	Provide inter-parcel circulation through crossover easements--specifically by conditioning accordingly.
H-3.1	35	Continue coordination with ODOT to improve safety on state facilities within the City and citywide access management strategies – specifically by conditioning to allow for fees in lieu of construction, more than one way to construct an improvement, and for ODOT and/or the City Engineer to make decisions for the developer about which way to construct an improvement and how.
H-3.2	35	Implement strategies to address pedestrian and bicycle safety issues, specifically for travel to and from local schools, commercial areas, and major activity centers – specifically by as examined above for Policies D-1.3, G-1.1, H-1.3, H-1.4, & H-2.3.
H-5.1	36	Implement, where appropriate, a range of potential Transportation Demand Management (TDM) strategies that can be used to improve the efficiency of the transportation system by shifting single-occupant vehicle trips to other models and reducing automobile reliance at times of peak traffic volumes – specifically as examined above for Policies D-1.3, D-1.5, G-1.1, H-1.2, H-1.3, H-1.4, & H-2.3.
M-1.2	49	The City shall increase its commitment to energy conservation, including alternative energy vehicles, increased recycling, and reduction in out-of-direction travel – specifically Policies and conditioning the development to have a number of electric vehicle (EV) parking stalls with charging stations.

Below are images providing further context for Street Corridors “C” & “J” in relation to Comprehensive Plan Policies G-1.3 and H-2.2:



TSP Figure 6

As examined under the Design Review Provisions section for 3.01, TSP Figure 6 calls for street connections among U.S. 99E and OR 211 at Cooley Road and June Way.



“Superblock” exhibit staff produced for Pre-App PRE 2019-01 that was February 13, 2019
 (Note: Ignore the scale indication within the image.)

The superblock exhibit shows that looking beyond the UGB, the subject property is part of a gigantic superblock through which there are no streets or rural roads such

In the exhibit, solid purple is the segment on the subject property of what staff later termed Street Corridor “C”, dashed purple indicates a sensible way to connect the three blue arrows that TSP Figure 6 shows, solid yellow line indicates provision on public cross access easements on the subject property (except that the thinner north-south yellow line most to the east actually is the UGB), and dashed yellow line indicates public cross access easements on more lots along OR 211. (Background colored lines represent subsurface utilities and bluish-gray polygons are building footprints.)

From the beginning, staff advised the applicant based on the above concept that staff drafted to bring greater detail to blue arrows of TSP Figure 6. The concept is of course a diagram and not at the detailed level of a site plan, civil engineering plan, or construction drawing.

The developer, who also developed ANX 2019-01 Woodburn Place Apartments at the east at 2145 Molalla Road, established the first segment of Street Corridor “C” through street reservation by granting an off-street PUE. The developer proposes for the subject development to extend the same width corridor as an off-street PUE across the subject property to the west lot line.

- e. Regarding the suitability of proposed conditions of approval to ensure compatibility of the proposed use with other uses in the vicinity, staff addresses these under other provisions through which staff introduces given issues and their related conditions of approval.

Capacity of public facilities

The City Engineer through Attachment 102A did not identify any deficiencies of or threats to public infrastructure in regards to factor b. of the third CU criterion – subsection B.3b – and the proposal indicates that the applicant will have constructed required street improvements, details to be determined in concert with ODOT as well as the City Engineer.

<i>Table CU-3</i>		
<i>Condition</i>	<i>CU Criteria/Factors</i>	<i>Reasons</i>
CU1	3a, 3b, 3c, 3c5), 3e	<ul style="list-style-type: none"> • The property is large and deep. • To encourage walking and cycling, particularly for cyclists afraid to ride on-street. • To continue the landscape strip and sidewalk widths conditioned for Woodburn Place Apartments.
CU2	3b, 3c, 3c5), 3c6), 3e	<ul style="list-style-type: none"> • Same as WDO 3.04.06A. • To mimic the wide walkway in Woodburn Place Apartments. • To encourage walking and cycling with shade trees along the wide walkway and crossings of drive aisles that feel comfortable and are and feel safe. • To calm traffic at walkway crossings. • To eliminate walkway dead-ends. • To integrate the two apartment complexes.
CU3	3c, 3c1)	<ul style="list-style-type: none"> • To prevent damage to front undercarriage of low-rise vehicles from noisy scraping of high wheel stops. • To lessen pavement and the urban heat island effect • To encourage walking and cycling with vehicles not overhanging walkways.
CU4	3c, 3c3)	<ul style="list-style-type: none"> • To maximize available parking for residents through mail carrier stall outside postal hours.
CU5	3c, 3c4), 3c6)	<ul style="list-style-type: none"> • Same as WDO 3.05.06A. • To encourage cycling by making bicycle parking plentiful, easy to find, and convenient, lowering vehicle trips and improving air quality.
CU6	3c, 3c4)	<ul style="list-style-type: none"> • Same as WDO 3.05 purpose statement and based on new Table 3.05E. • To encourage EV over gasoline vehicle driving by making parking sufficient, easy to find, and convenient, lessening the urban heat island effect from lowered emissions and improving air quality, and • Contribute to state policy goals regarding EVs.
CU7	3c, 3c5)	<ul style="list-style-type: none"> • To ensure that landscape areas are just that and mostly green, not mostly bark dust. • To reduce the urban heat island effect. • To screen at-grade electrical transformers and other equipment.

<i>Table CU-3</i>		
<i>Condition</i>	<i>CU Criteria/Factors</i>	<i>Reasons</i>
CU8	3a, 3c, 3c5)	<ul style="list-style-type: none"> • The property is large and deep, with the development in the south block/island having one large common area large enough for the root zones of mature evergreen trees without interfering with buildings or parking. • To provide for variety of trees, specifically to have a few evergreens that can grow large for habitat and for visual wayfinding.
CU9	3a, 3c1), 3c5)	<ul style="list-style-type: none"> • The property is large and deep, and the development has a front yard. • To help visually and acoustically buffer highway traffic from residents of the nearest buildings. • To regulate front yard fencing for the residential development as if residentially zoned.
CU10	3c, 3c5)	<ul style="list-style-type: none"> • Same as WDO 3.06 purpose statement and based on new WDO 3.06.03C. • To encourage walking and cycling with shade trees mitigating the urban heat island effect of parking areas. • To calm traffic at walkway crossings.
CU11	3c, 3e	<ul style="list-style-type: none"> • To ensure conformance with WDO 3.06.06B & D regarding recycling and trash enclosures.
CU12	3c, 3c5)	<ul style="list-style-type: none"> • To mitigate removal of Significant Trees.
CU13	3c, 3c5), 3e	<ul style="list-style-type: none"> • To mimic the patio and balcony minimum areas in Woodburn Place Apartments. • To provide patio and balcony areas width and large enough to be inviting and encourage actual use and sociability.
CU14	3c, 3c5)	<ul style="list-style-type: none"> • To mimic the common area improvements in Woodburn Place Apartments. • To have most benches with backs and not have any benches and the legs of those sitting on them be too close to walkers and cyclists on walkways • Shelter some outdoor space to encourage outdoor dining and sociability. • Through signage, to inform tenants that the parking area can fill with water during major storms.
CU15	3c, 3c2), 3c4), 3c5), 3e	<ul style="list-style-type: none"> • To mimic the architecture of Woodburn Place Apartments developed by the same developer.

<i>Table CU-3</i>		
<i>Condition</i>	<i>CU Criteria/Factors</i>	<i>Reasons</i>
		<ul style="list-style-type: none"> • To provide some window standards as baselines based on wall-mounted utility conflicts that arose after approval of Woodburn Place Apartments for that project, that is, to prevent loss of window area moving into the building permit and construction stages. • Maximize daylight into living and dining areas of units. • To have insect screens to remove conflicts between tenants wanting fresh, cooler air without air conditioning and not wanting insects. • To not have cascades of rainwater dumped onto walkways from building scuppers.
CU16	3c, 3c5), 3e	<ul style="list-style-type: none"> • To regulate signage, especially in the front yard, for the residential development as if residentially zoned, to prevent property manager making use of more liberal commercial regulations.
CU17	3b, 3c, 3e	<ul style="list-style-type: none"> • To mimic the parking management conditioned for Woodburn Place Apartments.
CU18	3c, 3c2), 3c5), 3e	<ul style="list-style-type: none"> • To mimic conditioned exterior lighting for Woodburn Place Apartments . • Same as WDO 3.11.01A. • To not have too much artificial light come through windows facing the highway that will have its own street lights.
CU19	3a, 3c, 3c5), 3e	<ul style="list-style-type: none"> • For privacy, to provide reasonable minimum distances between: <ul style="list-style-type: none"> ○ Apartment windows and sidewalk. ○ Apartment windows and common walkways and provide room for landscaping such as shrubbery. ○ Patios and common walkways and provide room for landscaping such as shrubbery.
CU20	3c, 3c5)	<ul style="list-style-type: none"> • To have address plates or mounted numerals conform to City address assignment and make this explicit for the developer/contractor. • To ensure the postal service is not unduly aggravated by tenants failing to perceive and use their correct, full apartment numbers. • To apply lessons learned from previous apartment developments.

<i>Table CU-3</i>		
<i>Condition</i>	<i>CU Criteria/Factors</i>	<i>Reasons</i>
D7	3c, 3c5), 3e	<ul style="list-style-type: none"> • To provide details for patio and balcony visual separation that the WDO 3.05.06C.7 & 3.07.05B.1a do not detail, such as confirming patios are to be paved. • Where railings provided, to ensure they are useful for mounting and threading things like flower boxes, hanging plants, and holiday lights. • To ensure that fence/railings have flat caps wide enough to set something down on them like a coffee mug. • To limit fence/railings height along the wide walkway to increase informal, standing-height surveillance of resident children and strangers.

✓ The conditional use criteria are met. Staff recommends approval with conditions of the request.

Recommended Conditions of Approval

Staff recommends approval of the consolidated applications based on the findings in the staff report and attachments, which are incorporated by this reference, as well as applying the following conditions of approval:

General

G1. As part of building permit application, the applicant shall submit revised site plans meeting the WDO and conditions of approval and obtain Planning Division approval through sign-off on permit issuance.

G2. The applicant or successors and assigns shall develop the property in substantial conformance with the final plans submitted and approved with these applications, except as modified by these conditions of approval. Were the applicant to revise plans other than to meet conditions of approval or meet building code, even if Planning Division staff does not notice and signs off on building permit issuance, Division staff retains the right to obtain restoration of improvements as shown on an earlier land use review plan set in service of substantial conformance.

G3. References: Attachment 201 serves as a dictionary or glossary defining certain abbreviations, acronyms, phrases, terms, and words in the context of the conditions of approval. The 200 series of attachments are as binding as the conditions of approval in the main body of the final decision.

G4. Due dates / public improvements:

- a. When public street improvements, and any fees in lieu of public improvements, are due shall be per WDO 3.01.02E and 4.02.12 unless if and where a condition of approval has more restrictive timing. By this condition, there is more restrictive timing: In any case, the improvements are due no later than by Building Division issuance of certificate of occupancy (C of O) for the first of any of the buildings.
- b. Where a Phasing Plan per WDO 5.03.05 is relevant, building permit issuance means issuance for the phase in which the conditioned improvement is located.
- c. Where changes to street addresses are necessary, the developer shall apply through the Planning Division for and obtain approval of an [Address Assignment Request](#). This is due prior to building permit application, and if and where land division is relevant, then also after recordation with County.

G5. Recordation due dates: The applicant shall apply to the County for recordations of items that the City requires no later than 6 months prior to expiration of the land use approval as WDO 4.02.04B establishes, and shall complete recordations no later than three years past the land use “final decision” date. The due date to complete recordations shall not supersede when recordations are due relative to the building permit stage.

G6. Administration:

- a. Conformance: That a land use approval does not reiterate any and each particular detail, provision, requirement, rule, spec, or standard from any of the WDO, other ordinances, resolutions, public works construction code, or department policies does not exempt development from conformance with them.
- b. Copies: The developer, including any succeeding contract, shall provide copies of documentation that a City staff person requests regardless if the documentation source is another City staff person or department.
- c. Fees: The developer shall pay fees per Attachment 203.



Conditional Use 22-01

CU1. Frontage/street improvements: These shall be:

- a. Bicycle lane: 6 ft wide min.
- b. Planter strip: 8 ft wide min, exc. curb dimension.
- c. Street trees: Per WDO 3.06.03A.1 (1:30), equaling 14 trees per frontage. For max 6 of the min trees required along the frontage, the developer may pay a fee in-lieu per Attachment 203. This fee provision shall substitute for the applicant invoking WDO 3.06.03A.3 (Director modification/relocation).
- d. Sidewalk: 8 ft wide min, which may overlap the PUE with granting of public access via either the PUE or separate easement, and with the gap between its west dead-end and the OR 211 shoulder connected with ADA-compliant pavement min width 5 ft.
- e. Electric power pole: Removal of the electric power pole at June Way and burial of lateral electric line(s) serving the development.

CU2. Walkways:

- a. Wide walkway: Per WDO 3.04.06B and:
 - (1) Extent: Min as far north as the south side of the northernmost east-west drive aisle and follow a route among the common building and min 14 of the apt buildings.
 - (2) Decorative paved areas: At least 1,500 sq ft of wide walkway shall be either paved with any of decorative bricks or pavers or paved with concrete that is scored, stamped, or otherwise treated to have a pattern. (The intent is for the developer to apply this to the proposed three circular bulges along the wide walkway and excludes the square footage of unpaved holes in the doughnut shapes of these areas.)
 - (3) Speed tables: Per WDO 3.04.06D.1.
 - (4) Trees: At each of the landings on the west side (min 5), a landscaped island min width 7.5 ft between insides of curbing and extending 14.5 ft min, exc. curb dimension. Each island shall have a tree. Along the length of the wide walkway within 10 ft of each side, total min 22 trees.
 - (5) VCA: Each crossing shall have two small VCAs, one each at the north landing, east side and the south landing, west side. The VCA triangles shall measure from 6 by 6 ft from the intersecting edges of wide walkway and drive aisle, and no parking stall shall overlap VCA.
- b. Walkways: Per WDO 3.04.06d.2, applied such that per Exhibit CU2b below min 4 crossings shall be extension of walkway poured concrete:

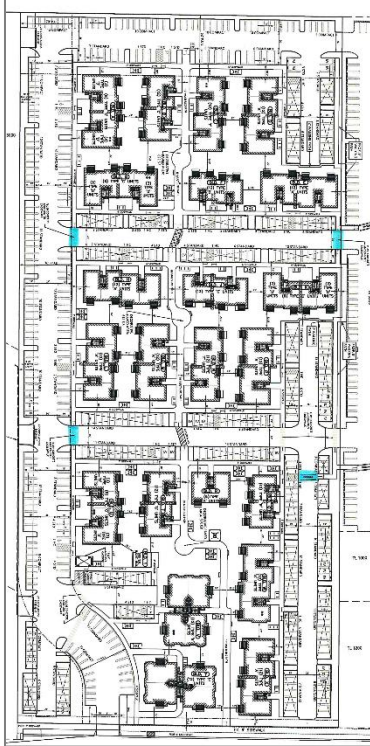


Exhibit CU2b (blue indicates extension of walkway poured concrete)

- c. Connections with Woodburn Place Apts.: Min 2 walkways shall connect east across the east lot line and across the west north-south drive aisle of Woodburn Place Apts. (2145 Molalla Rd) with the walkway system of that apartment development. The crossings of the drive aisle shall be striped per WDO 3.04.06D.2.
- d. Nominal walkways: At each of the east dead-end north-south drive aisles, a walkway min width 2 ft shall extend from the dead-ends of standard width walkways east across the east lot line to the curb of the Woodburn Place Apts. (2145 Molalla Rd) west north-south drive aisle.
- e. East walkway: The walkway that dead-ends near the site SE corner shall extend south to sidewalk.
- f. Mail shelter: The walkway that dead-ends east of the mail shelter shall extend northeasterly to the wide walkway bulge.
- g. Outermost drive aisle: A walkway shall serve the southeastern outermost drive aisle per WDO 3.05.02N.

CU3. Overhang / wheel stops:

- a. Overhang: In parking aisles along the rear and sides of the subject property, standard size stalls may be per WDO Table 3.05B as amended by Ordinance No. 2602 (18 ft instead of 19) and shall overhang curbing and landscaping by min 0.5 ft, as WDO Figure 3.05C allows up to 1.5 ft max.

- b. Wheel stops: Wheel stops are required along walkways. Wheel stops anywhere within the site development based on WDO 3.05.02H as amended by Ordinance No. 2602 shall be max 4.25 inches high.

CU4. Maximizing available parking for residents / mail carrier stall: The proposed stall designated for mail carrier parking (near Building U) shall be available for resident parking on official postal holidays, Sundays, and remaining days outside the hours of 8 a.m. to 6 p.m. A sign 1½ by 1 ft min shall note the range of hours when a space is limited to mail carrier parking and specify that it is available for resident parking outside the specified hours.

CU5. Bicycle parking:

- a. Based on WDO Table 3.05D, except at a lesser rate of 1.05 per dwelling [270 stalls], & 3.05.06.
- b. Guest: Of the stalls outdoors, min 2 stalls w/in 10 to 20 ft of ROW and along or near the wide walkway.
- c. Stairwell signage: WDO 3.05.06C.4 applies such that each stairwell that has bicycle parking requires a sign.
- d. Vertical clearance: 8 ft min from floor or grade or, where a stall is under stairs, 6 ft min.
- e. Wide walkway: Min 60 stalls distributed along the wide walkway.

CU6. EV: Electric vehicle parking shall be:

- a. Per WDO Table 3.05E [26 stalls] and 3.05.03I.
- b. Grouping/distribution: Per Exhibit CU6b below, min 2 groups per block/island. For example, the 4 along the north block/island north face may be one group of 4 or two of 2.

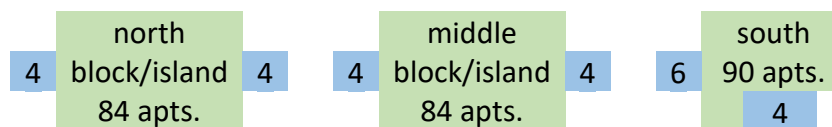


Exhibit CU6b EV distribution/grouping: max number per group

CU7. General:

- a. Bark dust: 5.0% max of landscaped area may be bark dust.
- b. Screening: Evergreen hedge or shrubbery shall be screen at-grade electrical and mechanical equipment along their sides, excepting the side intended for technician access.

CU8. Evergreen: Min 3 trees. The 3 shall be 1 min of the following coniferous or evergreen species:

Cedar, Western Red	Madrone, Pacific
Douglas-Fir	Oak, Oregon White
Fir, Grand	Pine, Ponderosa; and
Hemlock, Western	Yew, Pacific

CU9. Front yard:

- a. Trees: The front yard shall have a loose row of trees that complements the row of street trees. 12 min, with trees new to the site placed min 3.5 ft from edge of sidewalk and max 20 ft from ROW.
- b. Hedge/shrubbery: Besides that parking area screening which WDO 3.06.05B requires, the remainder of the front yard also shall have a hedge or row of evergreen shrubbery that can mature to 3 ft height. Such shall be planted and pruned to be min 2 ft from sidewalk, wide walkway, and walkways and max height 3.5 ft. Min number equal to 1 plant per 3 ft of length.
- c. Fence/fencing/free-standing wall: If any proposed in the front yard between the east and west walkways, the height limits shall be the same as if the property was residentially zoned, that is, per 2.06.02A and Figure 2.06A.

CU10. Parking area trees:

- a. South and west caps of parking aisles: Conform to WDO 3.06.03C.1.
- b. Middle: Each parking aisle shall have between the ends of the aisle at least one landscaped island that is 6 ft wide min between insides of curbing and extends 14 ft min, exc. curbing, into aisles with perpendicular or angled stalls and at least 7 ft, exc. curbing, into aisles with parallel parking. A drive aisle with parking on both sides has two parking aisles, and the wide walkway crossing landscaped islands conditioned elsewhere do not count towards this condition. Each island shall have a tree.

CU11. Trash enclosure: If any later proposed, per WDO 3.06.06B & D.

CU12. Tree preservation:

- a. ROW: Street improvements, including both frontage and off-site improvements, shall preserve any existing alive trees, including through meandering sidewalk.
- b. On-site: The developer shall pay a fee for Significant Tree removal per Attachment 203.

CU13. Balconies and patios:

- a. Patios: 8 ft min narrowest dimension and 96 sq ft min.
- b. Balconies: 8 ft min narrowest dimension and 80 sq ft min.

See also Condition D7.

CU14. Common area improvements: They shall include:

- a. Benches: 9 min, each 6 ft wide min, 75.0% min with backs, color(s) other than black or charcoal. A concrete or masonry seat wall may substitute for a backless bench for each segment that is 6 ft wide min, 1.5 ft high and deep min, and includes a cap of smoother concrete. Place 8 min benches along the access way preferably near the three circular bulges, 1 at or near the wide walkway bend that is east of the south bulge, and 1 w/in 20 ft of sidewalk. Benches shall be set back 1½ ft min from edge of access way or walkway.
- b. Picnic benches: 2 min, each square. 1 min ADA-compliant (with one of the four sides omitting a bench seat) on a paved pad. Place 1 min in the shelter.
- c. Shelter: At least 2 gazebos, pavilions, or shelters with pad (pavement options same as WDO 3.04.04), narrowest dimension of 14 ft, 225 sq ft min, ceiling height 10 ft min measured to underside of roof cross beams, and placed in the south island/block of apt buildings along or near the wide walkway. Set back columns, piers, or posts min 1.5 ft from any walkway.
- d. Parking area flood signage: Because the proposed parking area is proposed as itself the stormwater management facility such that during the 25-year storm, water would pool within curbing two inches or higher, the same as for Woodburn Place Apts., there shall be on-site signage that alerts residents:
 - (1) Min 8 signs each at a different location within both the subject property (4 min) and Woodburn Place Apts. (4 min), each location within max 10 ft of drive aisle or parking stall boundaries. Locations shall include the south and middle east-west drive aisles of both properties and near the Woodburn Place Apts. common building.
 - (2) Face min size 12 by 18 inches, mounted min 2 ft above grade and max the same as whatever the state rules are for ADA parking signage, and text size min 4 inches high. An example of acceptable text is: "Parking Lot Floods During Heavy Rain".

CU15. Buildings: It shall be:

- a. Windows:
 - (1) Area/size: Min of a simple majority of all windows proposed shall be min 20 sq ft.
 - (2) Proportion: All windows shall be square or vertically proportioned, except that horizontally proportioned windows are allowed if they have grilles or muntins dividing lights or panes so as to be vertically proportioned. Exception per (3) below.

- (3) Per room: Within apts, every habitable room abutting a building exterior wall shall have min 1 window. Each ground floor corner room proposed through land use review to one on each elevation shall continue to have 2; however, where the developer to propose additional location(s) of exterior wall-mounted utilities and meters, then for each room, the second window may shrink to min 6 sq ft and be horizontally proportioned.

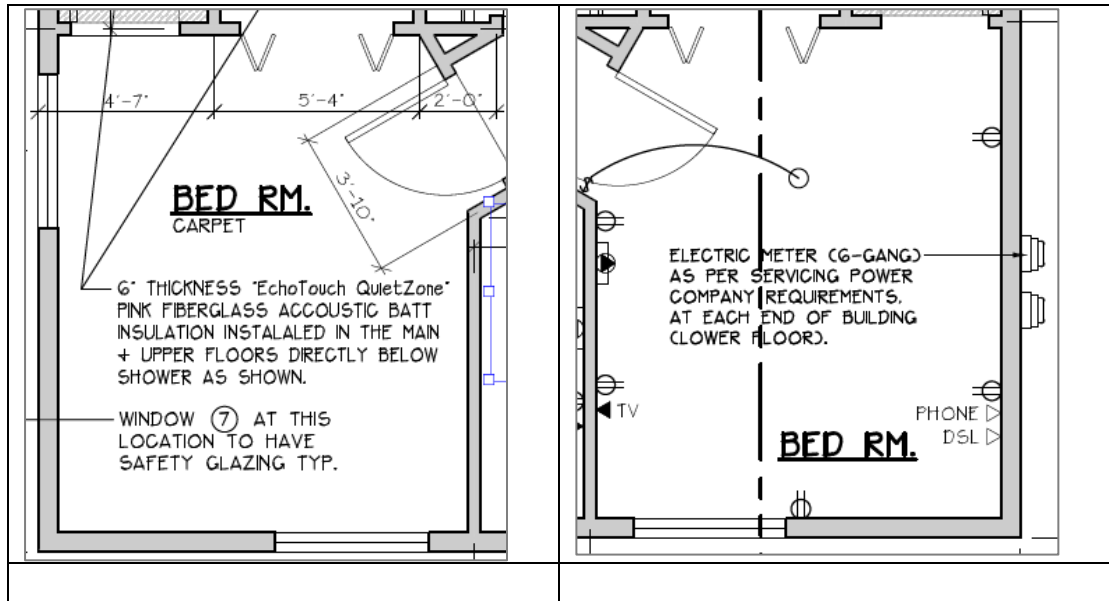


Exhibit CU15a(3)

- (4) For units with floor plans resembling Exhibit CU15a(4) below with 3 ft or more offset, each rear in the wall dividing dining/living area and patio or balcony shall have a window min width 1.5 ft and min area 7.5 sq ft approximately where the blue letter “B” indicates:

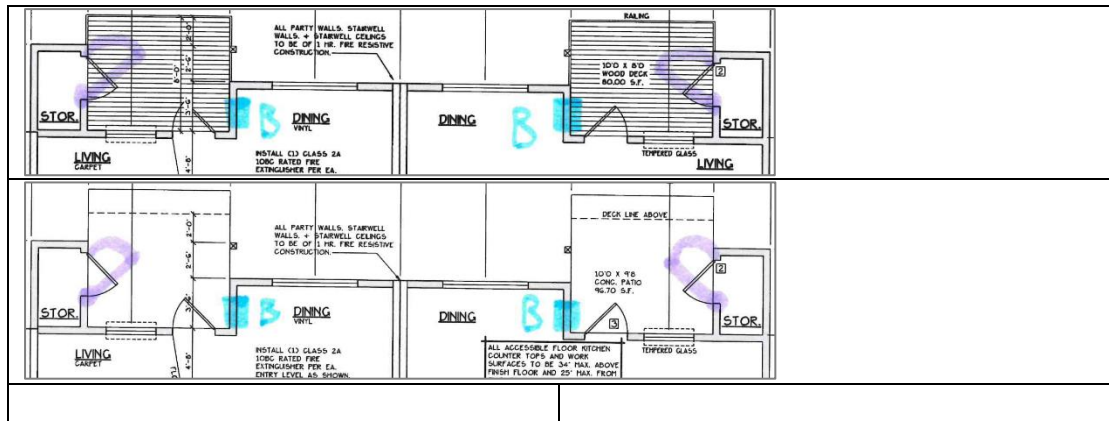


Exhibit CU15a(4)

- (5) Insect screens: All operable windows shall have insect screens.
- b. Scuppers: Any building rainwater scuppers shall not to dump onto the pavement of an access way or walkway.

CU16. Signage: If any permanent signage, particularly in the front yard, is later proposed, the allowed signage for both the subject property and Woodburn Place Apts. shall be as if the property was residentially zoned, that is, per Table 3.10.10A.

CU17. Parking management: This shall be as follows (the same as ANX 2019-01 Condition CU17):

- a. Survey: The applicant or any successor and assigns such as a property manager shall collect data about off-street parking usage or allocation and provide it to the City to the attention of the Director.
 - (1) Reporting period: Collect data by each half of a year – January through June and July through December. Submit each biannual report by the last City business day in the last month of the next quarter of a year and that is not a federal holiday. (For example, a report for January through June 2022 would be due by September 30, 2022.)
 - (2) First report: The first report shall cover whatever irregular length of time would pass between phase occupancy and the end of the next half of a year ending June or December.
 - (3) Attributes: Collect and report on:
 - (a) Geography: Report numbers divided between Phases 1 & 2 (as defined in Condition G3).
 - (b) The number of off-street spaces/stalls that are available and how many, if any, are closed due to occasional events such as parking area resurfacing, temporary outdoor events, outdoor storage, or the stationing of large trucks or truck trailers.
 - (c) Track stalls and usage by type: regular standard size, regular compact, accessible/ADA/handicap, EV, and any other type (such as those designated for visitors, leasing office employees, staff golf cart, or mail carrier).
 - (d) Collection: The property manager shall do field counts as per condition subpart (4 “field count”) below and also provide a separate set of assumed counts based on lease agreements, i.e. what tenant households are allocated a stall or stalls and for what periods, assumed that stalls are occupied as lease agreements describe.
 - (e) Usage: Report how many stalls are used and allocated. For vacant apartments in the context of assumed counts, record stalls associated with vacant apartments as unallocated.
 - (f) If and when a parking area resurfacing project were to happen, provide written notice to the Director of approximate start date and duration, location, and number of stalls involved.
 - (g) Format: Use tables to report by phase absolute numbers and percentages of stall type occupancies. Include phase and sitewide totals.

- (4) Field count: The property manager shall do at least two field counts per reporting period, meaning to travel the project and count in real time occupied and vacant stall types such as by marking a project site plan. Each count shall be on Tuesday, Wednesday, or Thursday that is neither a federal holiday nor within a week (7 days) of a federal holiday. One count shall be daytime starting no earlier than 9:30 a.m. and concluding no later than 4:30 p.m., and one count shall be nighttime starting no earlier than 10:00 p.m. and concluding no later than 12:30 a.m. Report when on a given date the counts were done and how long it took, for example, from 11:30 a.m. to noon.
 - (5) Bicycle parking: For outdoor bicycle parking stalls, including those within stairwells but excluding outdoor closets, the property manager shall also do field counts the same way as per condition subpart (4) above and as part of the larger report confirm the total number of existing outdoor bicycle stalls.
 - (6) Parking demand management: The reporting that a parking demand management condition requires, if it exists, may be incorporated with the parking usage data collection report.
 - (7) Context: In each report, cite the project name, phases, street addresses, master/parent case file number ANX 2019-01 and child case file number CU 2019-04, and the condition identification(s), state what period the report covers, state the number of vacant apartments and when and how the number was determined given fluctuation over six months, and provide an employee name and direct contact information for questions City staff might have.
 - (8) Intent: It is not the express intent of this condition to police property management or punish tenants or management for perceived misuse of parking, but instead without judgment to collect data on how parking is actually used in a conventional large apartment complex.
 - (9) Change of ownership: If and when property ownership were to change, the property manager shall pass along record of the conditions of approval to the contract purchaser and successive property manager.
- b. Expiration: This parking usage/allocation data collection condition becomes optional as of July 1, 2031. If reporting were to cease, the last report – for the January to June 2031 period – would be due September 30, 2031.

CU18. Lighting: Exterior lighting shall be:

- a. Per WDO 3.11 and, for permanent signage, per WDO 3.10.06G.
- b. Front yard: Of the south elevations of the two buildings closest to the ROW, max one exterior wall-mounted fixture and limited to Building Y, and the first 20 ft of front yard are limited to one pole-mount.

CU19. Privacy buffer:

- a. Apt building setback from highway shall be whichever is greater, either (1) 12 from ROW or (2) 10 ft from sidewalk. For accessory buildings, if any, min equal to streetside PUE.
- b. The distance between a walkway and an apt building main wall plane shall be min 6 ft.
- c. The distance between a walkway and a patio slab shall be min 3 ft.

CU20. Addressing: Addresses on apartment doors shall contain the full unit numbers per City address assignment. For example, in Building A, Apt. A101 shall read "A101", not "101".

CU21. Carports in sewer forced main easement: Regarding the existing sewer forced main that runs north-south near the west lot line, the existing easement for it that runs along the west property line (County Reel 143, Page 1685, dated 1978), and the carports proposed within the south end of the easement, at the subject property SW corner, the developer shall do one of the following:

- a. Remove or relocate the proposed carports while maintaining conformance with WDO 3.05.03F.2, due by building permit application;
- b. Through PW, rework a sewer forced main south segment to be east, sufficiently within and following a new proposed off-street PUE serving as Street Corridor "J", due the same as Condition G4; or
- c. Through PW, either obtain a revocable permit or enter into a written agreement, either legal instrument allowing for the carports to remain on terms set by PW, due by either when PW establishes or building permit issuance (of the structural permit for whatever is the first building or structure in the development), whichever is earlier.

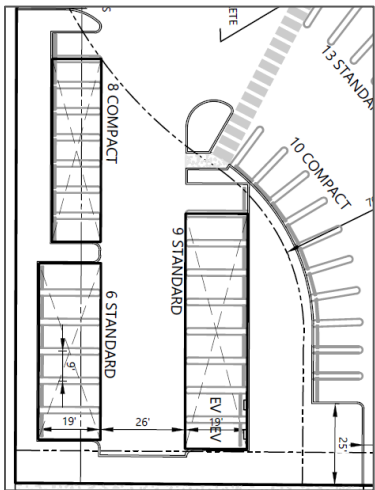


Exhibit CU21

Design Review 22-08

D1. ROW: To meet WDO Figure 3.01B, as part of recordations and regarding OR 211, the applicant shall dedicate (a) variable width ROW resulting in half-street ROW that is uniform 50 ft wide min measured from road centerline, and (b) a streetside PUE min 10 ft.

D2. Street corridors: To meet WDO 3.01.05F, as proposed as part of recordations and regarding Street Corridors "C" & "J", the developer shall reserve each future street corridor per Attachment 202.

D3. Driveways:

- a. Number: To meet WDO 3.04.03B.5 regarding access management, the number of driveways shall be limited as follows:
 - (1) 1 west, max width 30 ft, aligned with the T-intersection of the highway and June Way; and
 - (2) 1 east, max width 24 ft, towards the east lot line, but only if ODOT were to later permit a second driveway.
 - (3) If the Woodburn Fire District were to later desire a fire or emergency access driveway and ODOT to permit it, the area of driveway throat outside ROW shall be grass paving surface ("grasscrete") max width 24 ft.
- b. Approach / apron / curb cut: Driveways shall conform to PW SS&Ds, Section [4150](#), unless overridden by ODOT choosing to apply its standards.

D4. Cross access: To meet WDO 3.04.03B, the developer shall:

- a. Extend a drive aisle stub conforming to WDO 3.04.03C.4b to the property line along each of the following properties:

<i>Tax Lot</i>	<i>Address</i>	<i>Description</i>
051W09B000900	2145 Mollalla Rd	Woodburn Place Apts.*
051W08A005000	2400 N. Pacific Hwy	vacant; La Morenita Tortilleria Inc.
051W08A004800	2200 N. Pacific Hwy	Mini or self-storage; FCS Investment Group
051W08A004400	none	Industrial yard; Truss T Landholding LLC

*Regarding Woodburn Place Apts., the min is 3 stubs aligned with east-drive aisles on that property.


- b. At the interface of a property line and a drive aisle stub, fixed obstructions including curbing is prohibited. (The developer may instead place signed barricades atop the pavement.)
- c. To meet WDO 3.04.01A.2 and 3.04.03B.5, C.1, C.4a, & D.2, establish one or more public cross access easements revocable only with the written concurrence of the Director and that grants or grant public access to and from the main driveway at OR 211 and connects or connect with each of the min 6 drive aisle stubs.

D5. Parking striping: To meet WDO 3.05.02J, the developer shall designate compact stalls "COMPACT" in lettering one min 1 ft high.

D6. Landscaping: trees: To meet WDO Table 3.06B regarding tree minimum size at planting, the developer shall revise the landscape plan tree legend(s) to indicate for each tree species either 10-ft height or 2-inch caliper.

D7. Patios: Visual separation shall conform to WDO 3.05.06C.7 last sentence & 3.07.05B.1a as follows:

- a. Pavement: Patios shall be paved with brick, poured concrete, concrete pavers, or square or rectangular cobblestone pavers.
- b. Patio railings/fencing & gate: The outermost edges of patio concrete slabs that do not abut building walls shall have either metal or wood railings or cedar wood fencing at least 3 ft high. If the latter, then opacity shall be full, but if fencing is higher than the minimum height, it shall be no more than 90% opaque, such as by being fully opaque from grade but from the top having a lattice pattern. Gates per WDO 3.05.06C.7.
- c. Balcony railings – and where installed on patios, railings instead of fencing – shall have a bottom or near bottom horizontal member and a second horizontal member in addition to the top railing to allow residents to affix to, hang from, and thread through elements from them instead of the top of the railings. The cap or top member of balcony and patio railings, as well as the cap or top member of patio wood cedar fencing, flat and 3 inches wide min.
- d. Height maximum: The fencing or railings max height shall be either 5 ft or, where a patio faces and is within 10 ft of the wide walkway, 3.5 ft.
- e. Shrubbery: Evergreen shrubbery shall line fully the outermost edges of patio concrete slabs, except along the gated opening.



Design Review 22-08: Transportation

T-A1. OR 99E & OR 211/214:

- a. Mitigation: Similar as was for ANX 2019-01 Woodburn Place Apts. developed by the same developer, the developer shall pay a proportionate share contribution towards the same mitigation project, specifically a westbound dedicated right-turn lane or pocket that leads to OR 99E northbound based on ANX 2019-01 ODOT agency commentary (April 6, 2020).
- b. The proportionate share shall be 14.2% that equals an absolute amount per Attachment 203.

T-BP1. Off-site sidewalk improvements: To further TDM through walking, in addition to the required half-street sidewalk, the developer shall:

- a. Fill the highway south sidewalk gap within the block face between June Way and OR 99E. It is located almost wholly along the frontage of 2010 Molalla Rd (051W08DA00400) and approximately 400 to 450 ft long.
- b. Such sidewalk is not required to conform fully with WDO Figure 3.01B regarding siting relative to highway centerline and min width, and ROW dedication and streetside PUE granting are neither required nor expected. ADA compliance is expected.
- c. Sidewalk may be either curb-tight or the spatial equivalent were ODOT not to direct that there be curb, and min width along any given segment shall be any of the following:
 - (1) 3 ft, or
 - (2) Whatever min(s) ODOT establishes; andwith the proviso that along any given segment sidewalk shall be 5 ft wide where it can fit.
- d. The developer shall relocate or remove any utility boxes, cabinets, pedestals, and poles, excepting electric power poles, to accommodate sidewalk whether as necessary or as ODOT directs.
- e. Were ODOT to specify in writing standards such that sidewalk would have to meet them or not exist at all, and field constraints would prevent sidewalk from compliance, then the conditioned requirement for the off-site sidewalk would no longer be applicable. The burden of proof remains on the developer to make use of this condition part.

T-BP2. Wayfinding: To further TDM, the developer shall do one of the following:

- a. Install 1 min devices, such as signage, that provides wayfinding to bicycle routes, multi-use paths, parks, schools, and other essential destinations. If the developer were to opt for signage and assuming pole signage, sign face min dimensions shall be 2 ft by 1 ft and the placement shall be one at or near the junction of the wide walkway and sidewalk.

(Note: The developer may mimic the typical wayfinding signage the City approved for the Mill Creek Greenway as Smith Creek Development [ANX 2017-05] adapted from the City of Tualatin, Oregon greenway trail system signage which it in turn had adapted from the Regional Trails Signage Guidelines of The Intertwine Alliance, a trails coalition in the Portland metro area.)

- b. Pay a fee in-lieu per Attachment 203. [TSP B40 “wayfinding”/P62]

T-T1. Bus transit and vanpool fee: To further TDM through bus transit and vanpooling, the developer shall pay a mitigation fee per Attachment 203. [This condition relates to TSP projects T1, 2, 4, & 16, TDM1, TSP Fig. F5, and TPU projects 1, 2, 3, 11, 12, 13, 15, & 20.]

T-T2. School bus shelter: To further TDM through bus transit, the developer shall build or install a school bus shelter close to wherever the Woodburn School District bus would stop within or next to the development. Follow the direction of the school district facilities manager, or absent that default to min 48 sq ft, 6 ft narrowest dimension, and 8 ft min height clearance. Affix a min 1½ by 1 ft sign face with text min 4-inch high indicating that the shelter is a school bus stop. The shelter may be in ROW and/or a streetside PUE if ODOT allows and if set back min 1.5 ft from sidewalk. Another shelter that a developer provides for another purpose may do double duty as the school bus top shelter if it continues to meet all the other requirements, whatever they may be, for that purpose as well as this condition, and the shelter is within 240 ft of ROW. The developer may place the shelter on the Woodburn Place Apts. property (2145 Molalla Rd) as a means of meeting this condition.

T-TDM1. Car share: The same as ANX 2019-01 Condition T-TDM1, until July 1, 2025, the Director may invoke as a requirement that the property management team shall contract with a car share company or service, designate and mark a minimum number of parking spaces – which shall be at least one – for one or more shared vehicles for tenant use, and follow program details that the Director establishes as necessary to implement the requirement.

Applicant Identity

<i>Applicant</i>	Jeff Bolton, Senior Project Manager, Multi/Tech Engineering
<i>Applicant's Representative</i>	n/a
<i>Landowner(s)</i>	Church of Jesus Christ of Latter-day Saints (Jay Jones, local representative); Contract purchaser: Woodburn West LLC (Patricia Jones, registered agent) (Note: The developer is I & E Construction .)

Notes to the Applicant

The following are not planning / land use / zoning conditions of approval, but are notes for the applicant to be aware of and follow:

1. Records: Staff recommends that the applicant retain a copy of the subject approval.
2. Fences, fencing, & free-standing walls: The approval excludes any fences, fencing, & free-standing walls, which are subject to WDO 2.06 and the permit process of 5.01.03.
3. Signage: The approval excludes any private signage, which is subject to WDO 3.10 and the permit process of 5.01.10.
4. Inspection: The applicant shall construct, install, or plant all improvements, including landscaping, prior to City staff verification. *Contact Planning Division staff at least three (3) City business days prior to a desired date of planning and zoning inspection of site improvements.* This is required and separate from and in addition to the usual building code and fire and life safety inspections. Note that Planning staff are not primarily inspectors, do not have the nearly immediate availability of building inspectors, and are not bound by any building inspector's schedule or general contractor convenience.
5. Mylar signature: The Community Development Director is the authority that signs plat Mylars and not any of the mayor, City Administrator, Public Works Director, or City Engineer. Only one City signature title block is necessary.
6. PLA Plat Tracker: Marion County maintains a plat tracking tool at <http://apps.co.marion.or.us/plattracker/>. Use it to check on the status of a recordation request to the County. City staff does not track County plat recordation.
7. Technical standards:
 - a. Context: A reader shall not construe a land use condition of approval that reiterates a City technical standard, such as a PW standard, to exclude remaining standards or to

assert that conditions of approval should have reiterated every standard the City has in order for those standards to be met.

- b. Utilities: A condition involving altered or additional sidewalk or other frontage/street improvement that would in the field result in displacement or relocation of any of utility boxes, cabinets, vaults, or vault covers does not exempt the developer from having to move or pay to move any of these as directed by the City Engineer and with guidance from franchise utilities.
8. Other Agencies: The applicant, not the City, is responsible for obtaining permits from any county, state and/or federal agencies, which may require approval or permit, and must obtain all applicable City and County permits for work prior to the start of work and that the work meets the satisfaction of the permit-issuing jurisdiction. The Oregon Department of Transportation (ODOT) might require highway access, storm drainage, and other right-of-way (ROW) permits. All work within the public ROW or easements within City jurisdiction must conform to plans approved by the Public Works Department and must comply with a Public Works Right-of-Way permit issued by said department. Marion County plumbing permits must be issued for all waterline, sanitary sewer, and storm sewer work installed beyond the Public Right-of-Way, on private property.
 9. Stormwater management: The storm sewer system and any required on-site detention for the development must comply with the City Storm Water Management Plan, Public Works storm water practices and the Storm Drainage Master Plan.
 10. Public Works Review: Staff performs final review of the civil plans during the building permit stage. Public infrastructure must be constructed in accordance with plans approved by the City, as well as current [Public Works construction specifications, Standard Drawings, Standard Details](#), and general conditions of a permit type issued by the Public Works Department.
 11. ROW:
 - a. Dedication: The Public Works Department Engineering Division has document templates for ROW and easement dedications that applicants are to use.

ROW – and public utility easement (PUE) – dedications are due prior to building permit issuance per Public Works policy.
 - b. Work: All work within the public ROWs or easements within City jurisdiction must require plan approval and permit issuance from the Public Works Department. All public improvements construction work must be performed in accordance with the plans stamped “approved” by the City, and comply with the City’s Standard Specifications and Standard drawings.
 12. Franchises: The applicant provides for the installation of all franchised utilities in any required easements.

13. Water: All water mains and appurtenances must comply with Public Works, Building Division, and Woodburn Fire District requirements. Existing water services lines that are not going to be use with this new development must be abandoned at the main line. The City performs required abandonment of existing water facilities at the water main with payment by the property owner. All taps to existing water mains must be done by a “Hot Tap” method and by approved City of Woodburn Contractors. The applicant shall install the proper type of backflow preventer for all domestic, lawn irrigation and fire sprinkler services. The backflow devices and meters shall be located near the city water main within an easement, unless approved otherwise by Public Works. Contact Byron Brooks, City of Woodburn Water Superintendent, for proper type and installation requirements of the backflow device at (503) 982-5380.
14. Grease Interceptor/Trap: If applicable, a grease trap would need to be installed on the sanitary service, either as a central unit or in a communal kitchen/food preparation area. Contact Marion County Plumbing Department for permit and installation requirements, (503) 588-5147.
15. Fire: Fire protection requirements must comply with Woodburn Fire District standards and requirements, including how the District interprets and applies Oregon Fire Code (OFC). Place fire hydrants within the public ROW or public utility easement and construct them in accordance with Public Works Department requirements, specifications, standards, and permit requirements. Fire protection access, fire hydrant locations and fire protection issues must comply with current fire codes and Woodburn Fire District standards. See City of Woodburn Standard Detail No. 5070-2 Fire Vault. The fire vault must be placed within the public right-of-way or public utility easement.
16. SDCs: The developer pays System Development Charges prior to building permit issuance. Staff will determine the water, sewer, storm and parks SDCs after the developer provides a complete Public Works Commercial/Industrial Development information sheet.
17. Public Improvements Civil Plan Review: The process by which to receive, review, and approve drawings and other documents related to public improvements required by these conditions of approval may be paired with or incorporated into building permit review, or, if directed by the City Engineer, through a civil engineering plans (CEP) review process led by the Engineering Division. If opting for CEP, the applicant shall not only follow the direction of the Engineer Division, but also take some actions to facilitate tracking by Planning staff and coordination with Engineering:
 - a. Cover letter: Upon submitting application to the Engineering Division, simultaneously alert the Planning Division through a cover letter to the attention of the Planning Division referencing the intended or, if known, actual submittal date as well as the project name, project phase, tax lot number(s), street address(es), and the land use / planning / zoning final decision conditions of approval that require the public improvement that is the subject of the civil engineering plans. Referencing conditions may be by quotation or

- citing the identification numbers (e.g., T-A1). Identify the specific sheet (by number) or document page number that illustrates or notes how each subpart of a condition is met.
- b. Contact information: State the applicant's name, company, phone number, e-mail, and desired date for City staff to respond with review comments. The cover letter may include these.
 - c. Plan copies: Submit to the attention of the Planning Division at least two plan size copies of plan sets (24 by 36 inches). Within the cover sheet title block(s), include the phrase "civil engineering plans" or "public improvements civil plans". Submit also Adobe PDFs using a fileshare service.
 - d. Re-submittal fee: If there are multiple re-submittals, beginning with a third submittal / second revised submittal and continuing with each subsequent submittal, the applicant must pay through the Planning Division into City general revenue a fee of \$100.

Where public improvements involve the jurisdiction of an outside agency such as the Oregon Department of Transportation (ODOT), the developer must account for that when interacting with the City Engineer and City Public Works Department process.



**Woodburn Place West Apartments
2045 Mollala Road
CU 2022-01, DR 2022-08, & VAR 2020-05 Public Works**

August 25, 2022

CONDITIONS OF LAND USE APPROVAL:

1. The Applicant, not the City, is responsible for obtaining permits from state, county and/or federal agencies that may require such permit or approval. All work within the Oregon Department of Transportation (ODOT) right-of-way requires the applicant to obtain approval and permits from ODOT.
2. The Applicant shall obtain approval from the Oregon Department of Transportation (ODOT) for the storm drainage analysis that will impact ODOT's system. Provide a final storm drainage analysis for detention and conveyance system. The storm drainage hydraulic analysis shall comply with both ODOT and City's requirements.
3. The applicant shall obtain approval from the Oregon Department of Transportation (ODOT) for all required improvements along Highway 211, included but not limited to street improvements, striping, Highway signage, construction of new sewer line, construction of new water line, and additional improvements as required by ODOT's review/approval of the applicants' traffic study.
4. Applicant to provide for the installation of all franchise utilities and shall provide any required easements for these facilities. All permanent utility services to the development shall be underground.
5. Carports or permanent structures are not allowed on public utility easements.
6. Department of Environmental Quality Erosion Control 1200C permit will need to be obtained for this development prior to City issuance of permit.
7. Final review of the Civil Plans will be done during the building permit application. Public infrastructure will be constructed in accordance with plans approved by public works and ODOT.
8. Provide and record the required right-of-way dedication, public utility easements, and waterline easements prior to building permit issuance.
9. Provide a street lighting plan/design for review and approval to the City, PGE, and ODOT.
10. Applicant to provide plans indicating the construction of a new gravity sanitary sewer main in Hwy211 – Woodburn-Estacada Highway. As indicated on previous meetings, the existing sewer main in Hwy 211 is an abandoned force main. Applicant is responsible for constructing a public gravity sewer main to the proposed development. **Attachment 102A**

10. Fire hydrants locations and fire protection requirements shall be as per the Woodburn Fire Districts and City of Woodburn requirements. Actual fire hydrant locations and inline valving locations shall not be determined until the construction final plan review.
11. Based on WDO 3.04.05, & 5.03.01B.3b, and need for intergovernmental coordination, required improvements to ODOT's intersections and roads shall be determined prior to land use approval.
12. System Development fees shall be paid at the time of building permit issuance.
13. All public easements and right-of-way dedications shall be properly recorded prior to building permit issuance.

Engineering & Project Delivery

190 Garfield Street • Woodburn, Oregon 97071

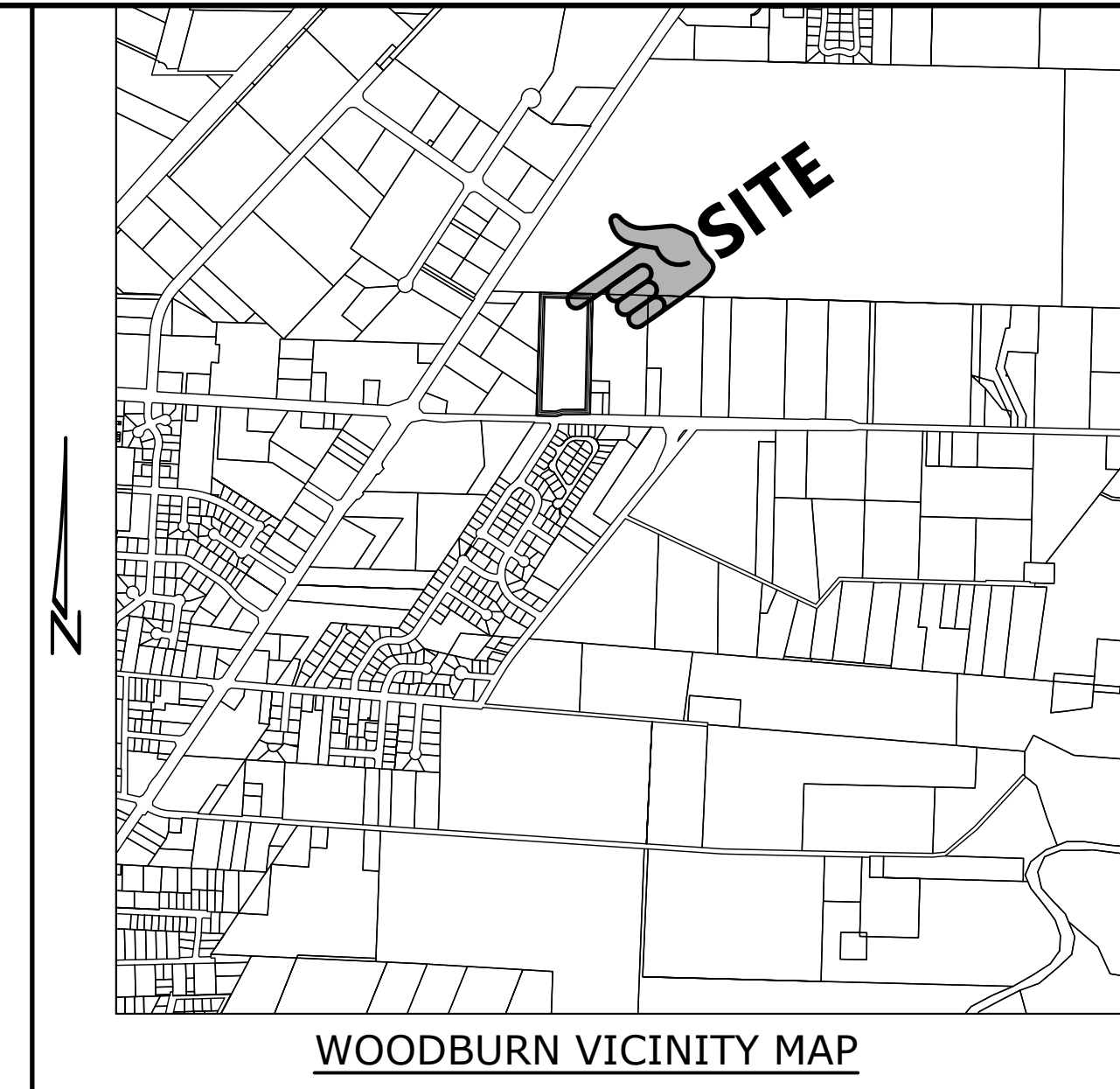
Ph. 5030-982-5240 • Fax 503-982-5242

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 Drawn: R.P.H.
 Checked: J.C.B.
 Date: JUNE 2022
 Scale: AS SHOWN

REGISTERED PROFESSIONAL ENGINEER
 JULY 4, 1984
 MARK D. GRANT
 EXPIRES 06-30-2023

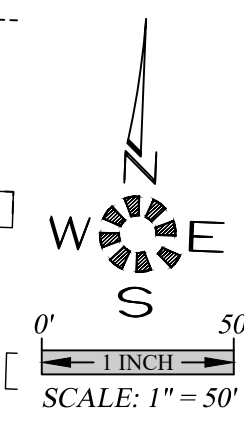
JOB # 7402
SDR1



WOODBURN PLACE WEST

SEC. 08, T. 5 S., R. 1 W., W.M.
 CITY OF WOODBURN
 MARION COUNTY, OREGON

Owner / Developer:
WOODBURN PLACE APARTMENTS, LLC.
27375 SW PARKWAY AVE
WILSONVILLE, OR, 97070
PHONE: (503) 655-7933



ABBREVIATIONS	
A.C.	ASPHALTIC CONCRETE
ACMP	ALUMINIZED CMP
ASSY.	ASSEMBLY
B.O.	BLOW OFF
B.F.V.	BUTTERFLY VALVE
C & G	CURB & GUTTER
CATV	CABLE TELEVISION
C.B.	CATCH BASIN
C.B.C.O.	CATCH BASIN CLEANOUT
C.B.I.	CATCH BASIN INLET
C.L.	CENTERLINE
CMP	CORRUGATED METAL PIPE
C.O.	CLEANOUT
CONC.	CONCRETE
CONST.	CONSTRUCT
D.I.	DUCTILE IRON
DIA.	DIAMETER
DWG.	DRAWING
EASMT.	EASEMENT
E.G.	EXIST. GRADE / GROUND
EOP, E.P.	EDGE OF PAVEMENT
ELEC.	ELECTRIC
ELEV. or EL.	ELEVATION
EX. or EXIST.	EXISTING
FT.	FEET
F.F.	FINISH FLOOR
F.G.	FINISH GRADE
F.H.	FIRE HYDRANT
F.M.	FORCE MAIN
GUT. or GTR.	GUTTER
G.V.	GATE VALVE
IMP.	IMPROVEMENT
INST.	INSERT
INV. or I-	INVERT
L	LENGTH, LINE
L.P.	LIGHT POLE
M	METER, MAIN
M.H.	MANHOLE
MTL.	METAL
O.H.	OVERHEAD
P.C.	POINT OF CURVE
P.C.C.	POINT OF CONTINUING CURVE
PED.	PEDESTAL
PRC.	POINT OF REVERSE CURVE
PROP.	PROPOSED
PT	POINT OF TANGENCY
PUB.	PUBLIC
PUE	PUBLIC UTILITY EASMT.
PVC	POLYVINYL CHLORIDE
PVT.	PRIVATE
P.P.	POWER POLE
P.L.	PROPERTY LINE
R	RADIUS
R	RIM
RD	ROOF DRAIN
R.O.W.	RIGHT-OF-WAY
SAN.S. or S.S.	SANITARY SEWER
S	SLOPE
STA.	STATION
STD.	STANDARD
STL.	STEEL
STM.DRN. or S.D.	STORM DRAIN
SVC.	SERVICE
SW	SIDEWALK
T.C.	TOP OF CURB
TEL.	TELEPHONE
TYP.	TYPICAL
U.G.	UNDERGROUND
VL.T.	VAULT
W.M.	WATER MAIN

SYMBOLS	
EXIST. PROP.	EXIST. PROP.
○	○ BLOW OFF ASSY.
□	□ CATCH BASIN
▣	▣ CATCH BASIN CLEANOUT
▤	▤ CATCH BASIN INLET
△	△ CATV PED. / BOX
○	○ CLEANOUT
▣	▣ ELEC. PED. / BOX
○	○ FIRE HYDRANT
○	○ GAS LOCATION MARKER
⊗	⊗ GAS VALVE
□	□ MAIL BOX
○	○ MANHOLE SAN. SEWER
○	○ MANHOLE STORM DRAIN
○	○ 2' DIA. C.O. / M.H.
○	○ MANHOLE TELEPHONE
○	○ MANHOLE WATER
▽	▽ REDUCER / INCREASER
▣	▣ TEL. PED. / BOX
▣	▣ TRAFFIC PED. / BOX
○	○ UTILITY / POWER POLE
□	□ WATER METER
⊗	⊗ WATER VALVE
---	--- SANITARY SEWER EXIST.
---	--- SANITARY SEWER PROP.
---	--- STORM DRAIN EXIST.
---	--- STORM DRAIN PROP.
---	--- WATER MAIN EXIST.
---	--- WATER MAIN PROP.

SHEET INDEX	
SDR1	COVER SHEET
SDR2	EXISTING CONDITIONS PLAN
SDR3	SITE PLAN
SDR4	SITE PLAN DETAILS
SDR5	OPEN SPACE PLAN
SDR6	GRADING PLAN
SDR7	PRIVATE STORM DRAIN PLAN
SDR8	PRIVATE SANITARY SEWER PLAN
SDR9	DOMESTIC WATER PLAN
SDR10	PRIVATE FIRE SERVICE PLAN
SDR11	PHOTOMETRIC PLAN
SDR11A	PHOTOMETRIC PLAN DETAILS
SDR12	PRELIMINARY FRONTAGE IMPROVEMENT PLAN
SDR13	FIRE HYDRANT COVERAGE PLAN
L1.1	PRELIMINARY LANDSCAPE PLAN
L1.2	PRELIMINARY LANDSCAPE PLAN
L1.3	PRELIMINARY LANDSCAPE PLAN

BUILDING A- TYPE C-D UNIT
 BUILDING B,J,T- TYPE C UNIT
 BUILDING C- TYPE F UNIT
 BUILDING D,E,H,C,M,P,X- TYPE A UNIT
 BUILDING F,Q- TYPE B UNIT
 BUILDING K- TYPE C-D UNIT
 BUILDING N- TYPE F UNIT
 BUILDING R- TYPE A UNIT
 BUILDING U- TYPE C-D UNIT
 BUILDING V- TYPE E UNIT
 BUILDING W- TYPE E UNIT
 BUILDING Y- TYPE A UNIT

Attachment 103
Sheet 1 of 11

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EXISTING CONDITIONS PLAN

WOODBURN PLACE WEST

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Drawn:	R.P.H.
Checked:	J.C.B.
Date:	JUNE-2022
Scale:	AS SHOWN



EXPIRES 06-30-2023

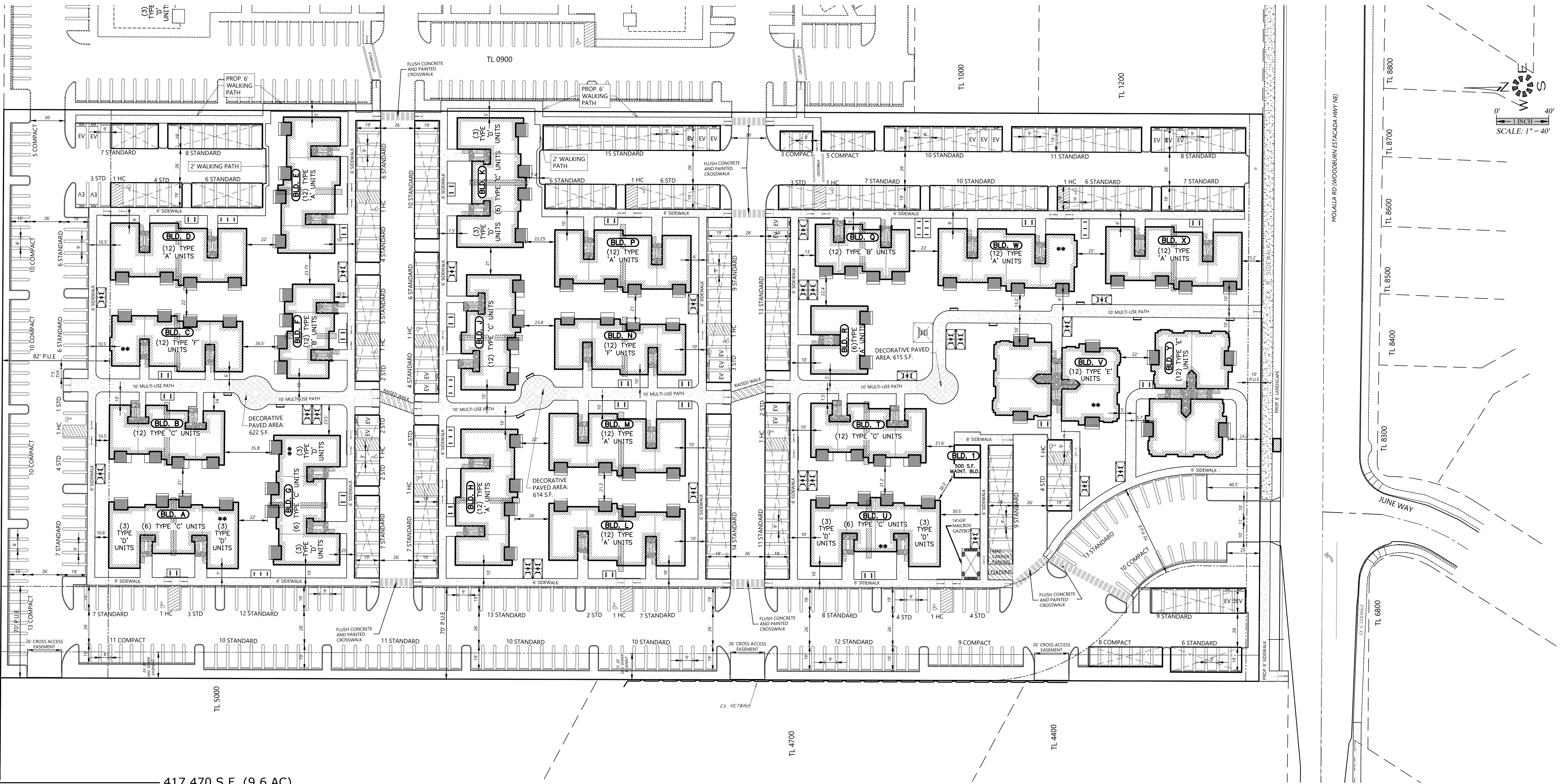
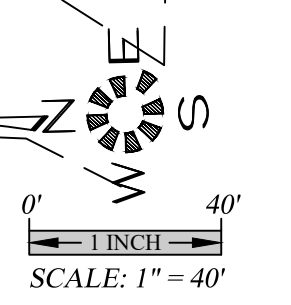
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SDR2

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SITE PLAN

WOODBURN PLACE WEST



SITE AREAS BOUNDARY — 417,470 S.F. (9.6 AC)

TOTAL HARDBPACE 300,828.85 S.F.
BUILDING COVERAGE 76,888 S.F.
PARKING, DRIVEWAY, SIDEWALK, CURB & MULTI-USE PATH- 223,940.85 S.F.

LANDSCAPE AREA- 115,681 S.F. (28%)

MULTI-FAMILY PARKING

- 516 TOTAL PARKING STALLS
- 406 STANDARD STALLS
- 26 ELECTRIC VEHICLE STALLS
- EV STATIONS TO BE MINIMUM LVL 2 CHARGING
- 94 COMPACT STALLS (TO BE PAINTED "COMPACT ONLY")
- 16 HANDICAP STALLS
- 248 COVERED STALLS (DUE TO PUBLIC EASEMENTS)
- 262 BICYCLE SPACES (132 COVERED) (SEE DETAIL PAGE)

- ⊖ - WALL PACK LIGHT - 8 FEET MAX ABOVE WALKWAYS
STAIRWELL LIGHTING TO BE WITHIN AND FLUSH WITH THE CEILING OR SHIELDED ON ALL FOUR SIDES.
- ⊕ - POLE LIGHT MAXIMUM 14' TALL
- ⊙ - POST LIGHT MAXIMUM 5' TALL
- ▨ - LOCATION OF ELECTRICAL SEPARATION WALL
- ① - MAXIMUM 1:12 SLOPE ON SIDEWALK END RAMPS
- ||| - BICYCLE SPACES (226 REQUIRED)
- ▭ - BENCHES (ALL BENCHES TO BE 6' LONG, 12 OF 15 BENCHES TO HAVE BACKS)
- - WHEEL CURB STOP (NO MORE THAN 4" IN HEIGHT)

BUILDINGS	
BUILDING A	11,508 S.F.
BUILDING B	8,736 S.F.
BUILDING C	10,128 S.F.
BUILDING D	11,424 S.F.
BUILDING E	11,424 S.F.
BUILDING F	6,588 S.F.
BUILDING G	11,508 S.F.
BUILDING H	11,424 S.F.
BUILDING J	8,736 S.F.
BUILDING K	11,508 S.F.
BUILDING L	11,424 S.F.
BUILDING M	11,424 S.F.
BUILDING N	10,128 S.F.
BUILDING P	11,424 S.F.
BUILDING Q	6,588 S.F.
BUILDING R	5,712 S.F.
BUILDING T	8,736 S.F.
BUILDING U	11,508 S.F.
BUILDING V	13,944 S.F.
BUILDING W	13,944 S.F.
BUILDING X	11,424 S.F.
BUILDING Y	11,424 S.F.
BUILDING 1	500 S.F.

258 TOTAL APARTMENT UNITS

- 102 TYPE 'A' 2-Bd, 2-Ba (952 S.F.) UNITS
- 24 TYPE 'B' STUDIO (549 S.F.) UNITS
- 60 TYPE 'C' 1-Bd, 1-Ba (728 S.F.) UNITS
- 24 TYPE 'D' 3-Bd, 2-Ba (1,190 S.F.) UNITS
- 24 TYPE 'E' 2-Bd, 2-Ba (1,162 S.F.) UNITS
- 24 TYPE 'F' 2-Bd, 1-Ba (844 S.F.) UNITS

- 1 U.S. MAIL AREA
- 1 500 S.F. MAINTENANCE FACILITY

LANDSCAPE ISLANDS AT 10' MULTI-USE PATH ARE REQUIRED TO BE MINIMUM 84 S.F. THIS IS AT ALL THREE DRIVE AISLES AS SHOWN.

TRASH AND RECYCLING TO BE DEPENDENT ON EXISTING WOODBURN PLACE APARTMENT FACILITY AT 2145 MOLALLA RD

ADA HANDICAP ACCESSIBILITY NOTES:

1. ALL ON-SITE WALKWAYS, PEDESTRIAN CONNECTIONS TO THE PUBLIC SIDEWALK AND ROUTES TO BUILDING ENTRANCES ARE ACCESSIBLE WITH RUNNING SLOPES LESS THAN 5% AND CROSS SLOPE LESS THAN 2% MAX. LANDINGS AT BOTTOM OF STAIRS AND EXT. FACE OF ENTRANCE DOORS SHALL HAVE A SLOPE IN THE DIRECTION OF TRAVEL NOT TO EXCEED 2%.
2. HANDICAP PARKING STALLS AND ACCESS AISLES ARE TO HAVE SLOPES IN ANY DIRECTION OF LESS THAN 2% MAX. GRAPHIC MARKINGS & SIGNAGE FOR HANDICAP AND VAN ACCESSIBLE STALLS WILL BE PER OSSC 2010 CHPTR. 11 AND ORS. REQUIREMENTS.
3. HANDICAP ACCESSIBLE CURB RAMPS SHALL HAVE A RUNNING SLOPE NOT TO EXCEED 1:12 MAX. AND A CROSS SLOPE NOT TO EXCEED 1%.
4. THE COMMUNITY BUILDING & ON-SITE LAUNDRY FACILITIES WILL BE FULLY HANDICAP ACCESSIBLE IN ACCORDANCE WITH ANSI A117.1 AND CHAPTER 11 OF THE 2010 OSSC.
5. 2% OF THE LIVING UNITS OR (3) UNITS WILL BE TYPE 'A' HANDICAP ACCESSIBLE. THESE INCLUDE A 1, 2 AND 3 BEDROOM UNIT AS INDICATED ON THIS SITE PLAN. THE BALANCE OF THE GROUND FLOOR LIVING UNITS WILL BE TYPE 'B' ADAPTABLE UNITS IN ACCORDANCE WITH ANSI A117.1.

NOTE:
GROUND FLOOR PATIOS ARE REQUIRED TO HAVE RAILING OR FENCING AROUND THEM. THE OUTERMOST EDGES OF PATIO CONCRETE SLABS THAT DO NOT ABUT BUILDING WALLS SHALL HAVE EITHER METAL OR WOOD RAILINGS OR CEDAR FENCING THREE AND A HALF (3.5) FEET HIGH. FENCING SHALL BE NOT FULLY OPAQUE. THE RAILINGS OR FENCING SHALL HAVE A GATED OPENING AT LEAST TWO (2) FEET AND SIX (6) INCHES WIDE AND NO MORE THAN 4 FEET WIDE. FENCING OR RAILING WITH TOP MEMBER FLAT AND MIN 3 INCHES WIDE. A SECOND HORIZONTAL MEMBER BELOW THE TOP MEMBER (TO ALLOW AFFIXING, HANGING, OR THREADING ITEMS BELOW THE TOP MEMBER).

BUILDING PERMIT REQUIRED FOR ALL BIKE SHELTERS GREATER THAN 200 SQ. FT.

FOR RAISED PEDESTRIAN CROSSINGS ONSITE. SEE DETAIL ON SHEET C1.4

"ELECTRICAL VEHICLE CHARGING" TO BE PAINTED IN 1" HIGH LETTERING AT EACH STALL. SEE SIGNAGE DETAIL ON SHEET C1.4.

BARBEQUE GRILL REQUIRED AT COVED BARBEQUE AREA.

DECORATIVE PAVED AREAS SHALL BE PAVED WITH CONCRETE THAT IS SCORED, STAMPED, OR OTHERWISE TREATED TO HAVE A PATTERN. SUBMITTAL REQUIRED TO CITY PLANNING DEPARTMENT FOR APPROVAL.

ALL OUTDOOR CLOSETS AT PATIO OR DECK SHALL HAVE WALL MOUNTED FOLDING OR RETRACTABLE BIKE HOOKS.

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 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

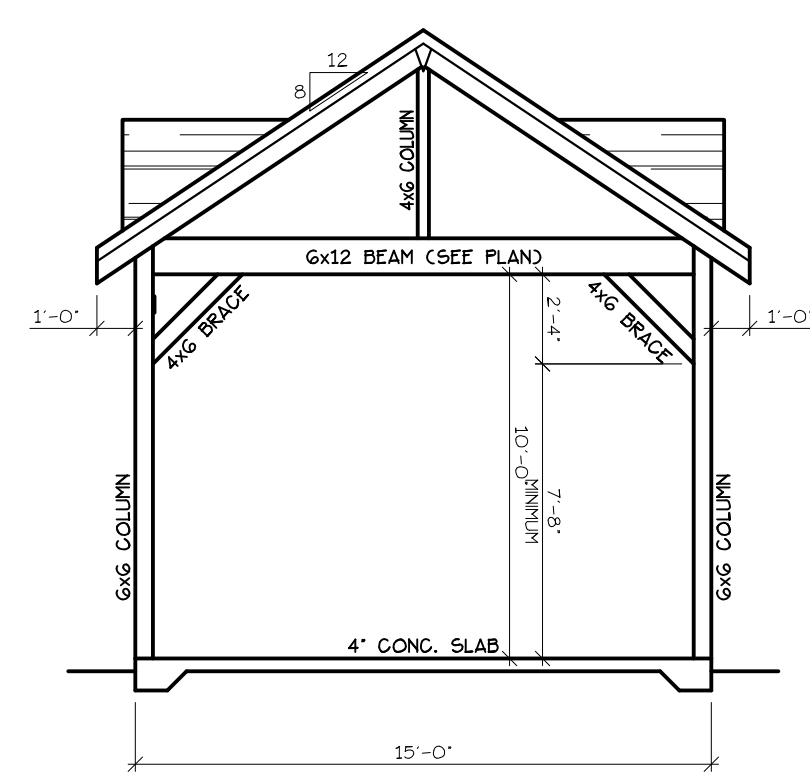
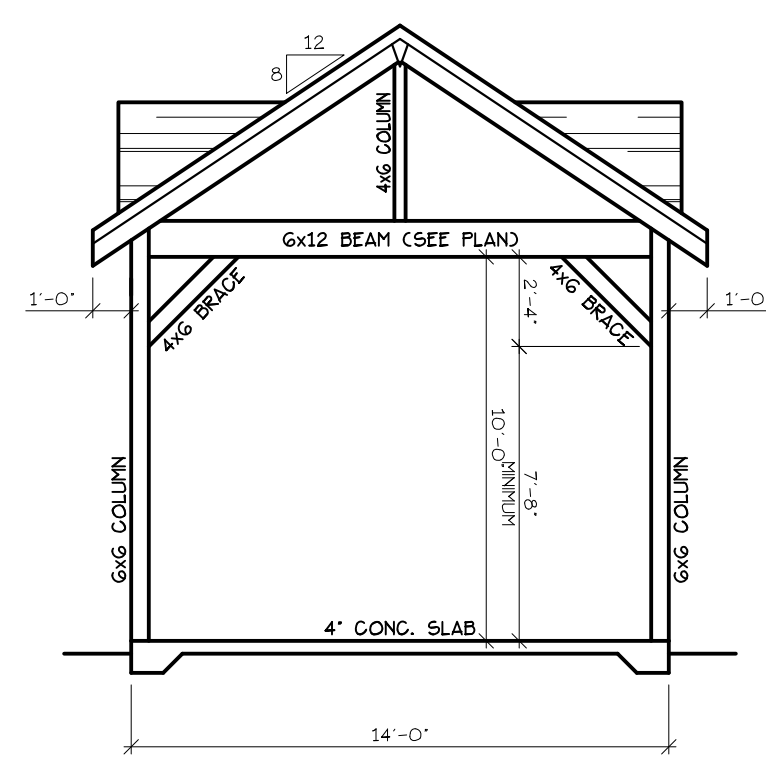
Design: M.D.G.
 Drawn: R.P.H.
 Checked: J.C.B.
 Date: JUNE 2022
 Scale: AS SHOWN



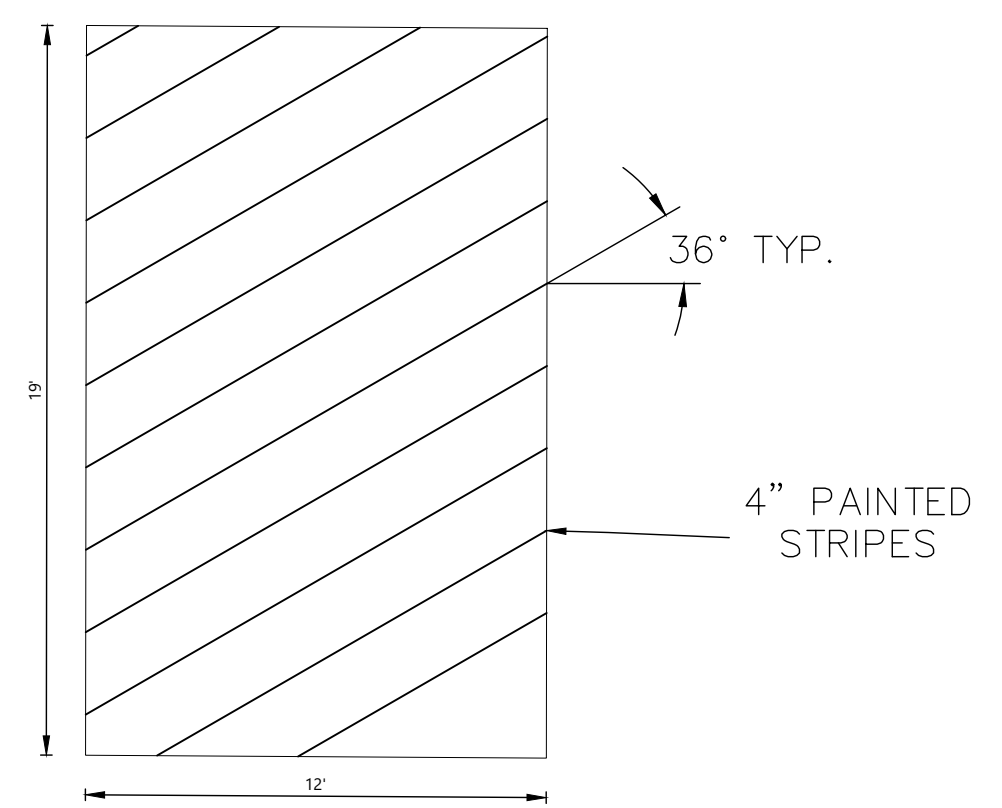
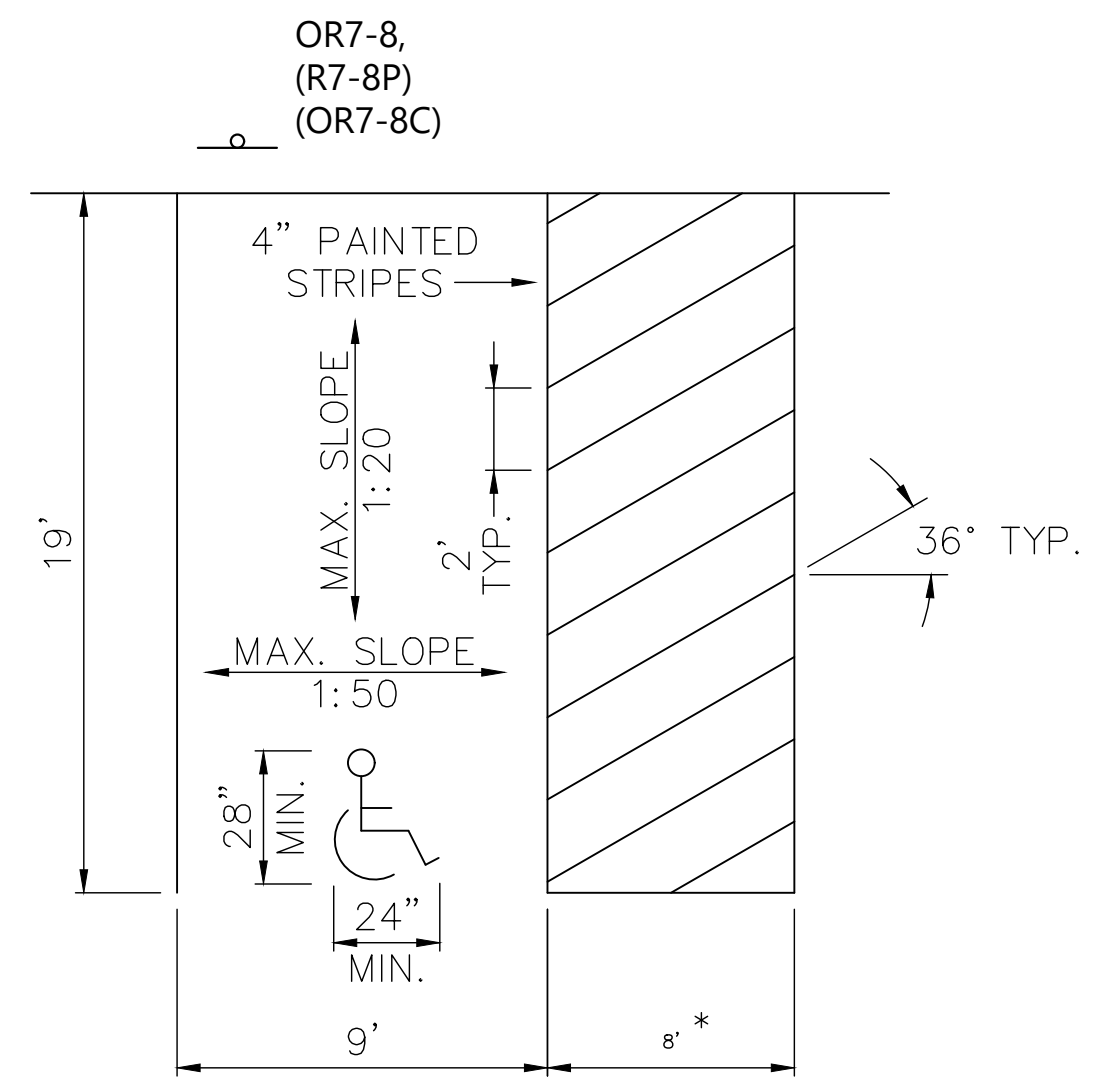
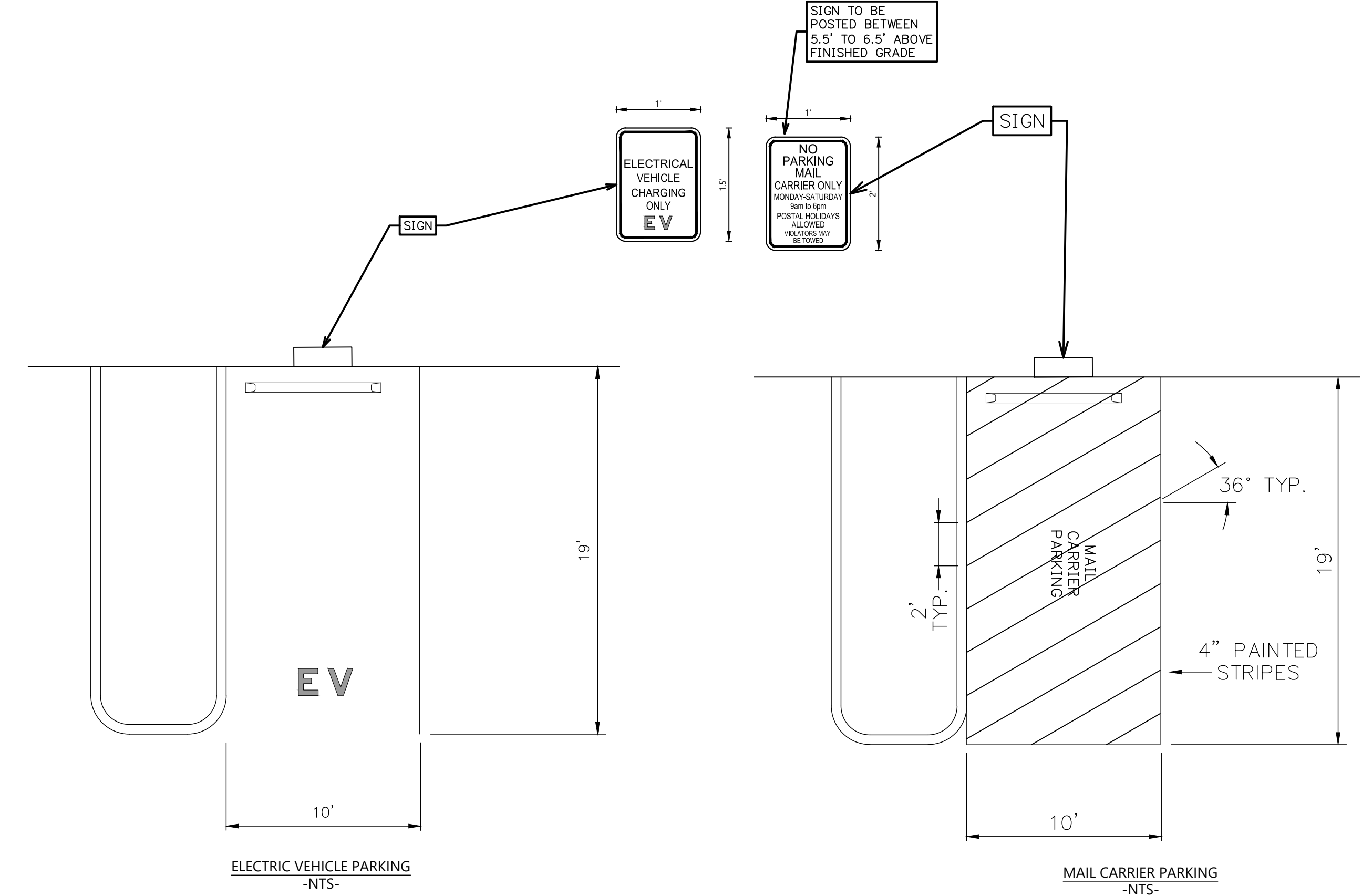
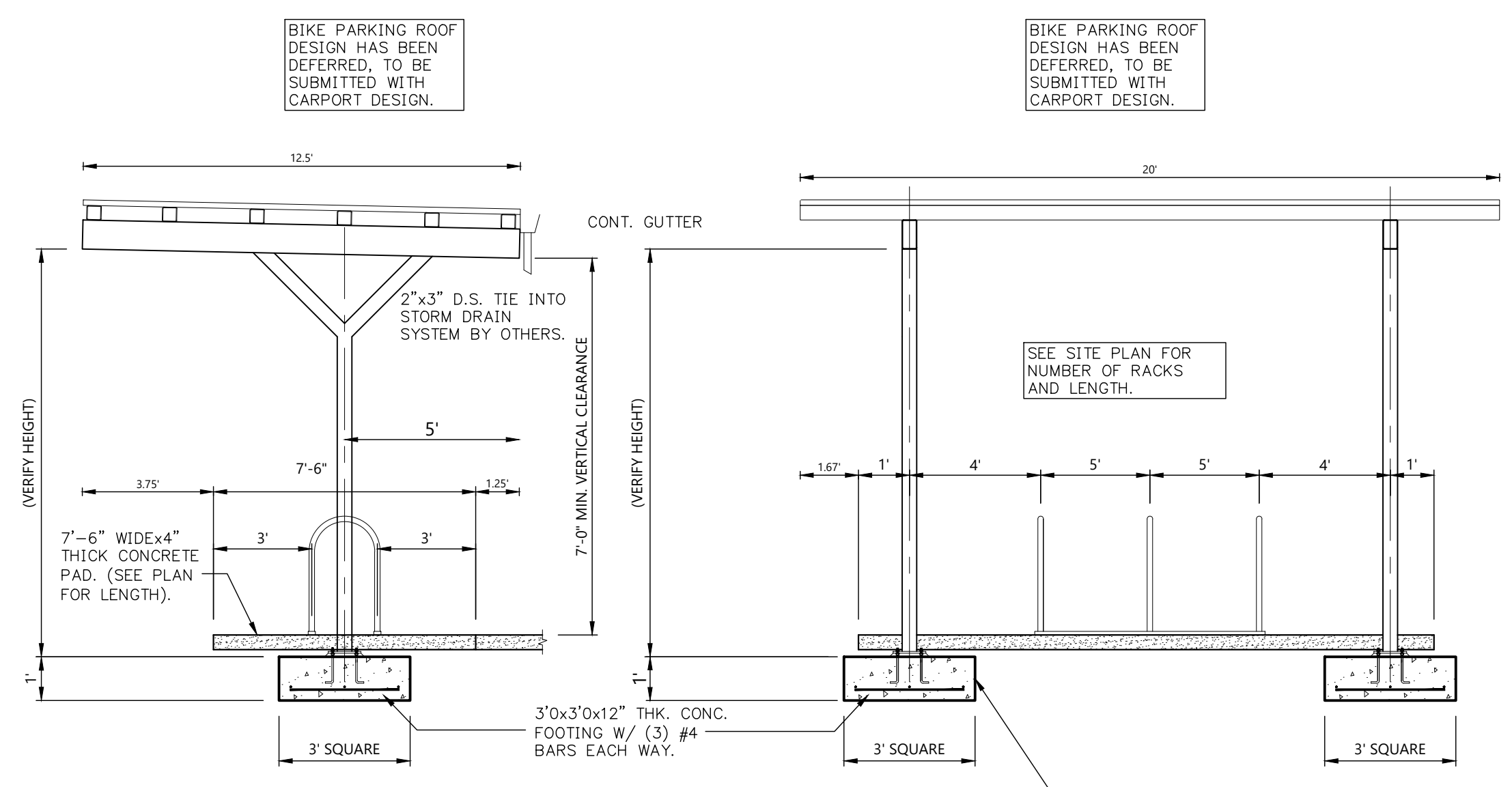
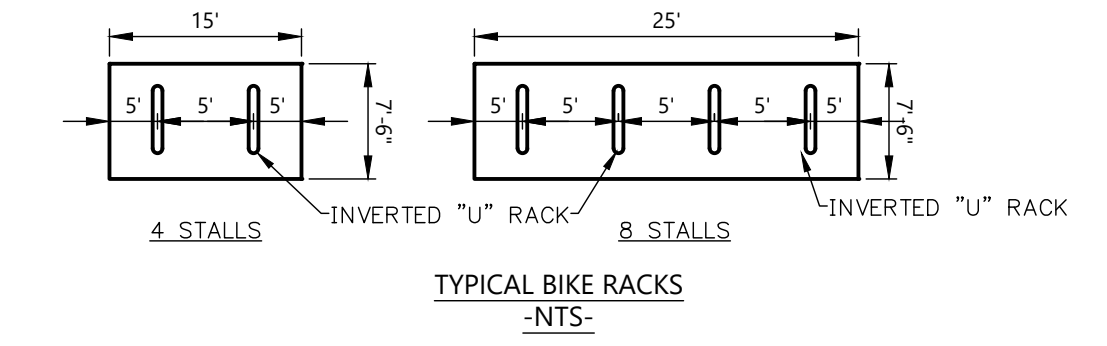
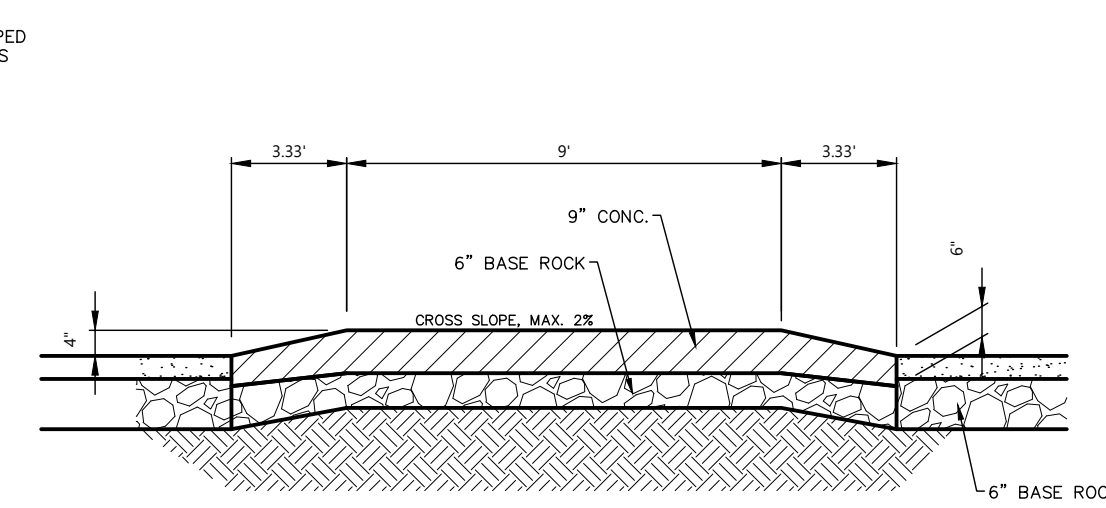
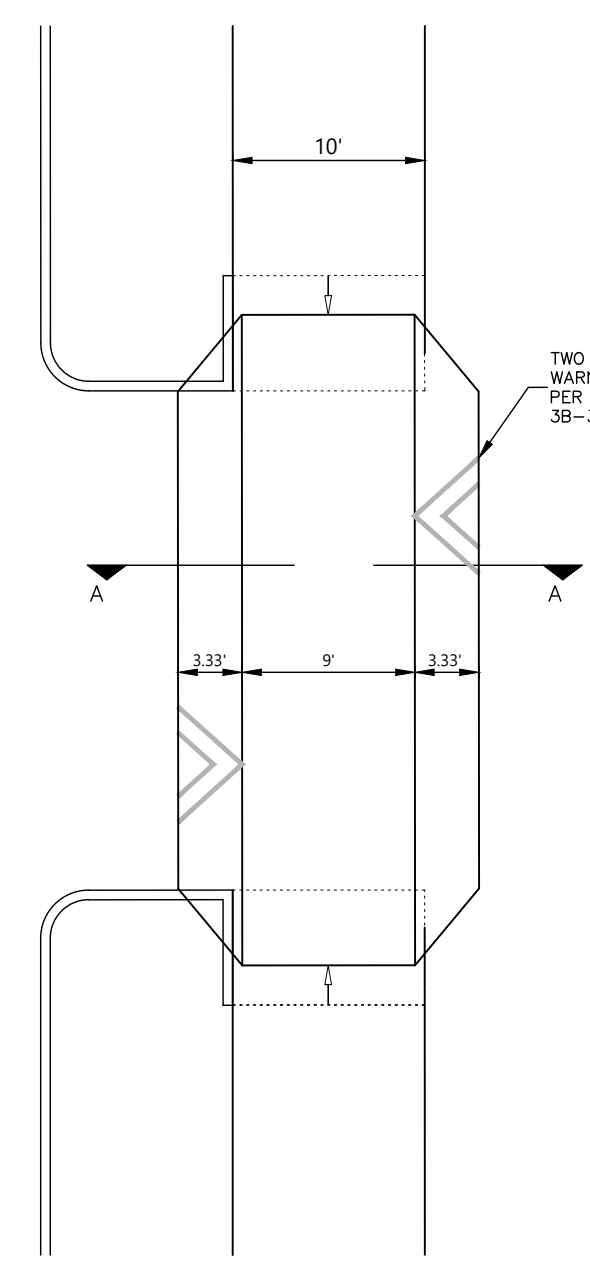
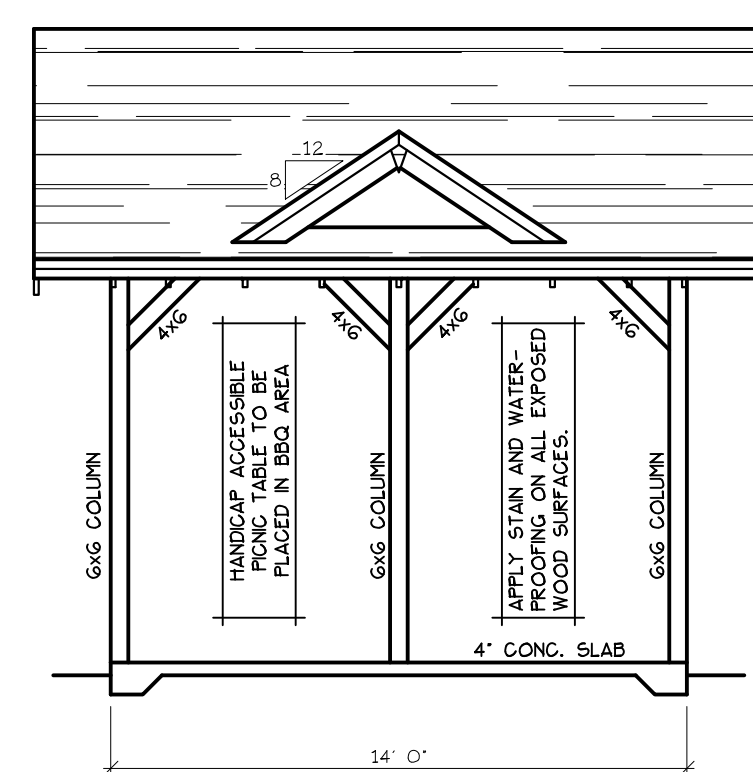
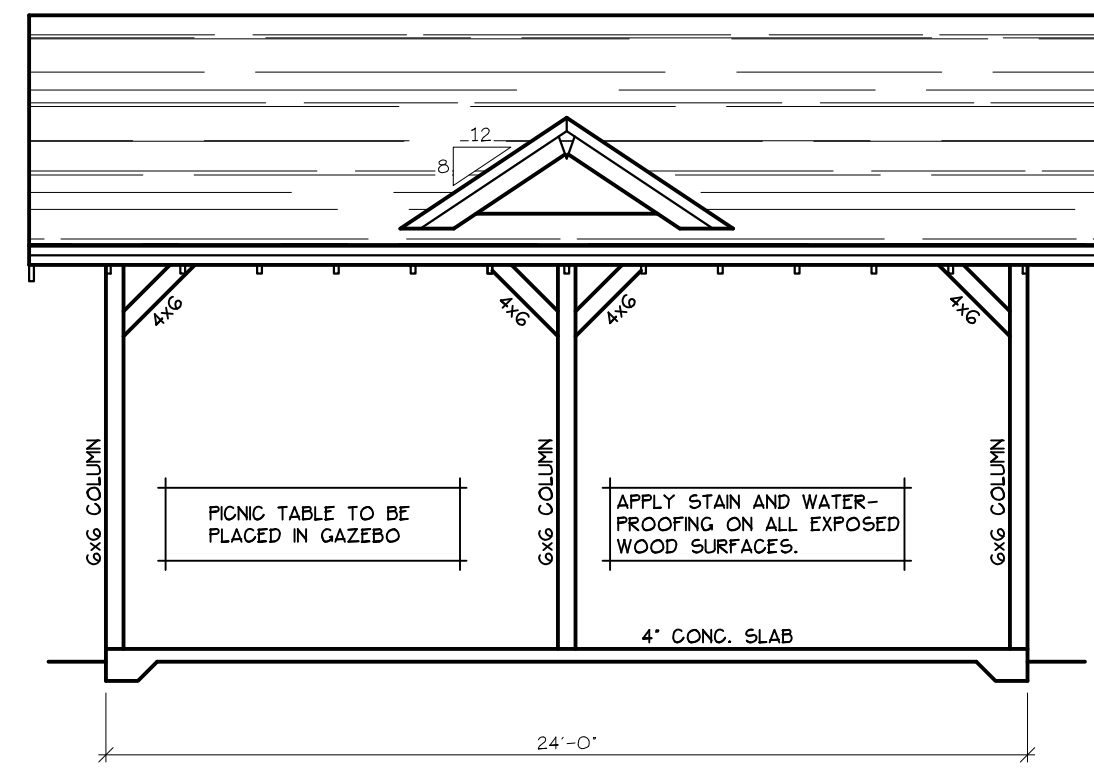
EXPIRES 06-30-2023

JOB # 7402

SDR3



BUILDING PERMIT REQUIRED FOR EACH GAZEBO ONSITE.



NOTES:
 1. PAVEMENT STENCIL WHITE PAINT FOR SYMBOL
 2. BLUE BACKGROUND & BLUE PAINTED CURB OPTIONAL
 * 8' FOR VAN-ACCESSIBLE (INCLUDE R7-8P)
 * 8' FOR WHEELCHAIR USER ONLY (INCLUDE OR7-8C)

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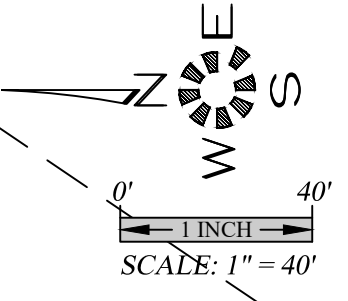
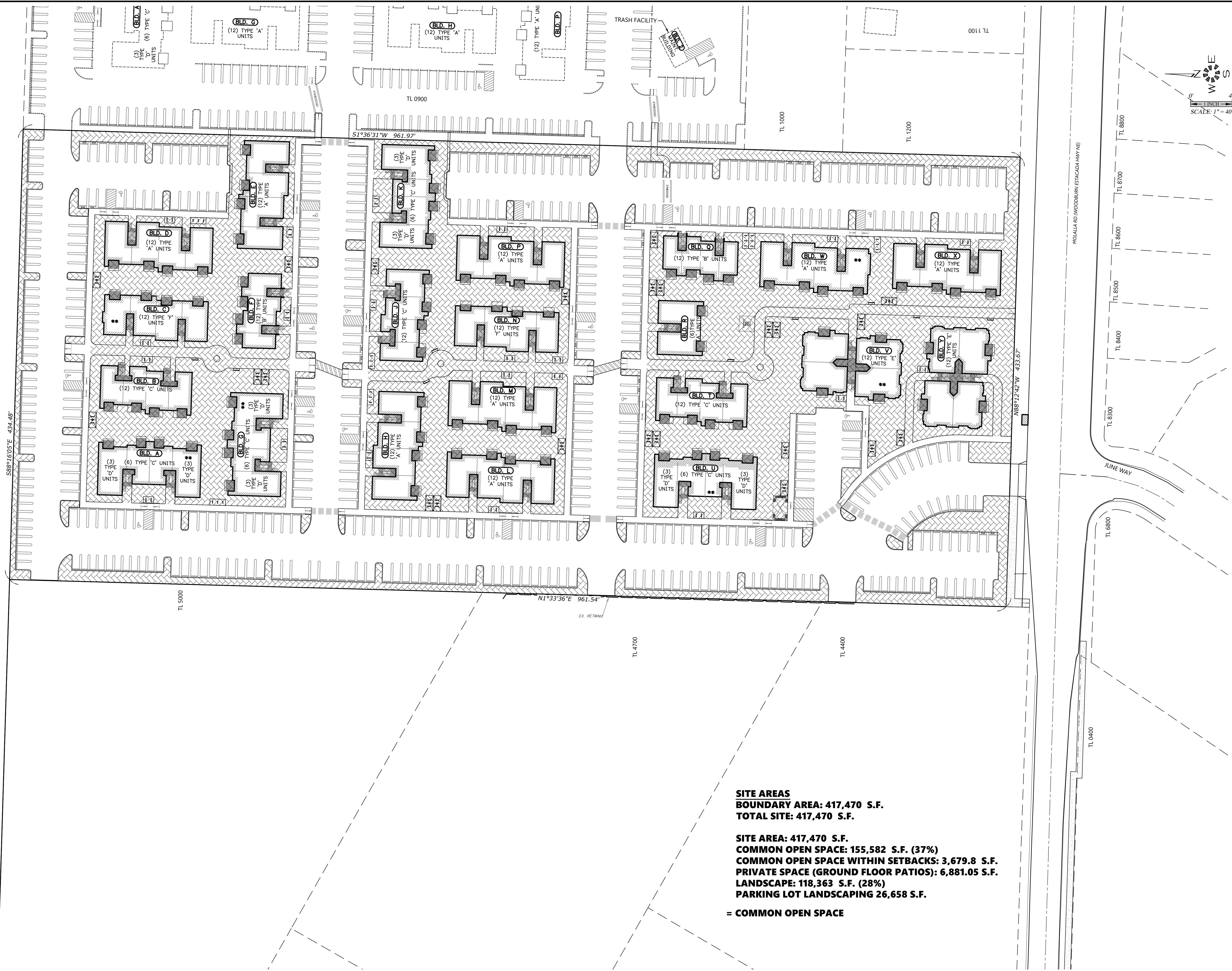
7402b - SHOW SITE DTL
 Design: M.D.G.
 Drawn: R.P.H.
 Checked: J.C.B.
 Date: JUNE 2022
 Scale: AS SHOWN

REGISTERED PROFESSIONAL ENGINEER
 MARK D. GENTILE
 JULY 14, 1998
 EXPIRES 06-30-2023

JOB # 7402

SDR4

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SITE AREAS
BOUNDARY AREA: 417,470 S.F.
TOTAL SITE: 417,470 S.F.

SITE AREA: 417,470 S.F.
COMMON OPEN SPACE: 155,582 S.F. (37%)
COMMON OPEN SPACE WITHIN SETBACKS: 3,679.8 S.F.
PRIVATE SPACE (GROUND FLOOR PATIOS): 6,881.05 S.F.
LANDSCAPE: 118,363 S.F. (28%)
PARKING LOT LANDSCAPING 26,658 S.F.

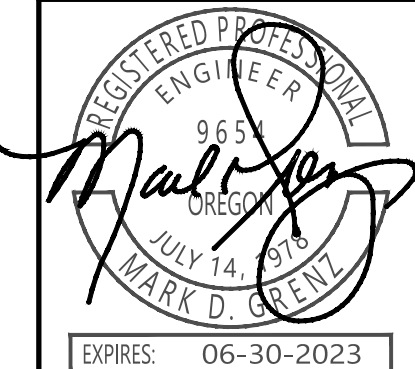
= COMMON OPEN SPACE

OPEN SPACE PLAN

WOODBURN PLACE WEST

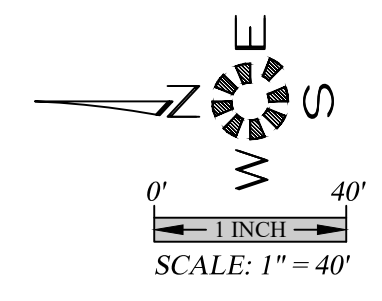
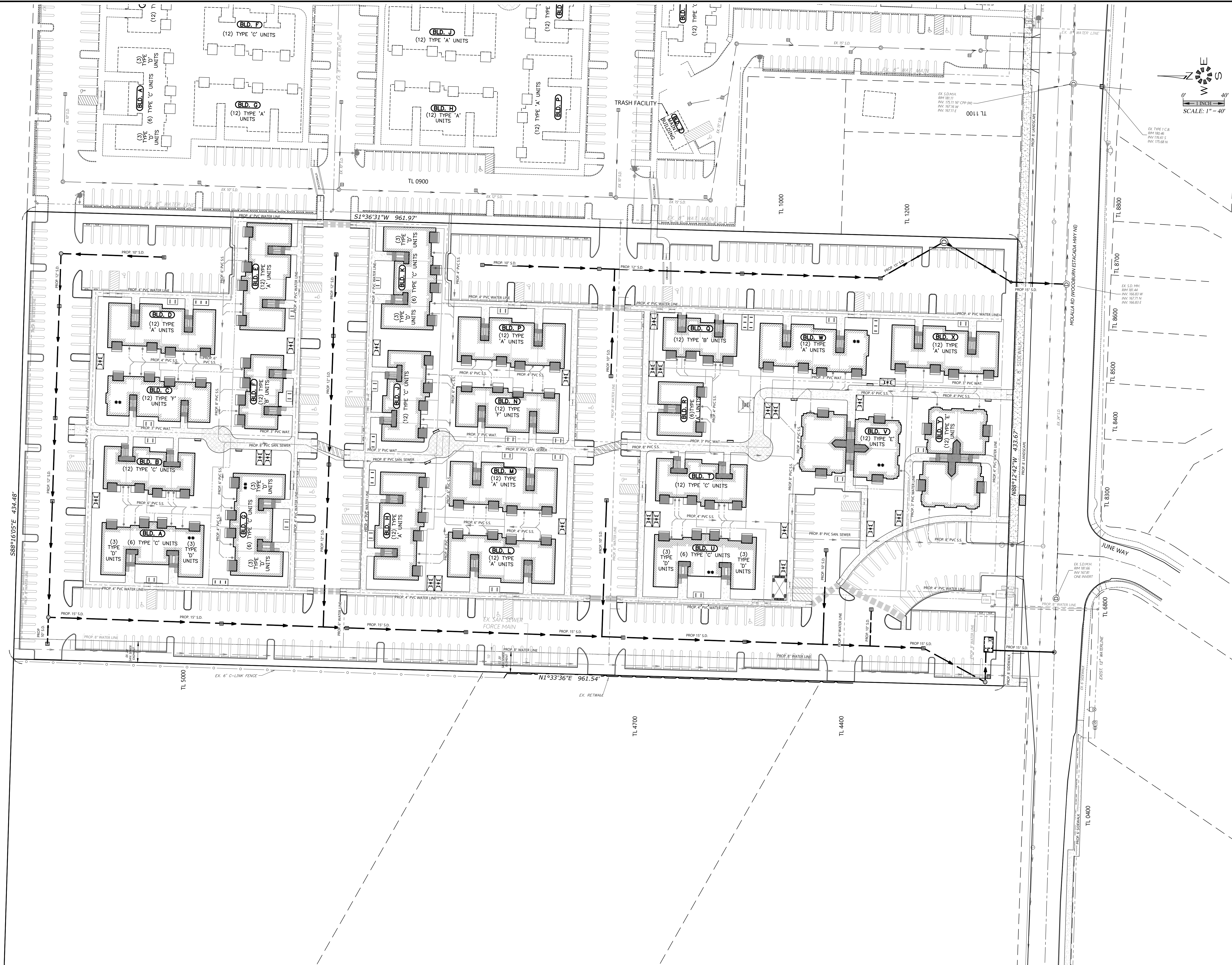
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7402B SDR5-05
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Drawn: R.P.H.
Checked: J.C.B.
Date: JUNE-2022
Scale: AS SHOWN



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JOB # 7402

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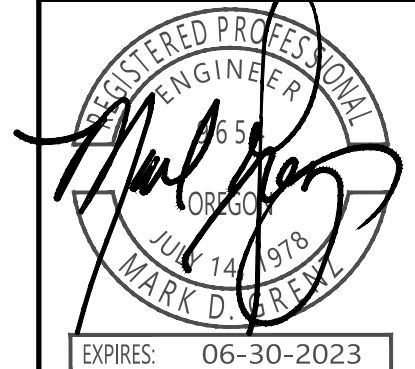
MULTI/TECH
 ENGINEERING SERVICES, INC.
 11555 13th St., S.E. Salem, OR, 97302
 PH: (503) 365 - 9227 FAX: (503) 364-1260
 www.mtengineering.net office@mtengineering.net

**PRIVATE
 STORM DRAIN PLAN**

WOODBURN PLACE WEST

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 Checked: J.C.B.
 Date: JUNE-2022
 Scale: AS SHOWN



EXPIRES 06-30-2023

JOB # 7402

SDR7

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 ENGINEERING SERVICES, INC.
 1155 13th St., S.E. Salem, OR, 97302
 PH: (503) 365-9227 FAX: (503) 364-1260
 www.mtengineering.net office@mtengineering.net

**PRIVATE
 SANITARY SEWER PLAN**

WOODBURN PLACE WEST

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 AUTHORIZATION FROM THE
 DESIGN ENGINEER.
 DIMENSIONS & NOTES TAKE
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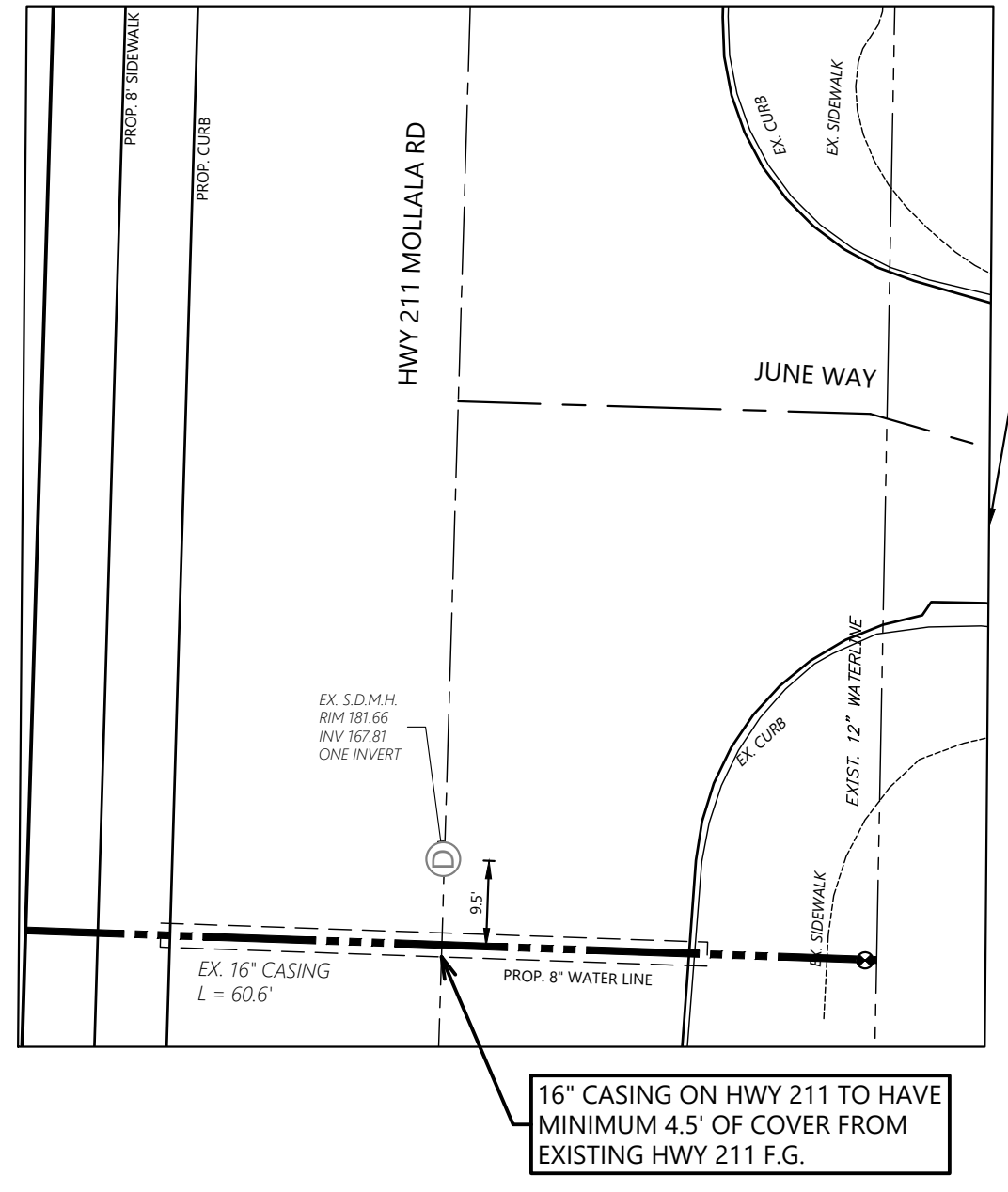
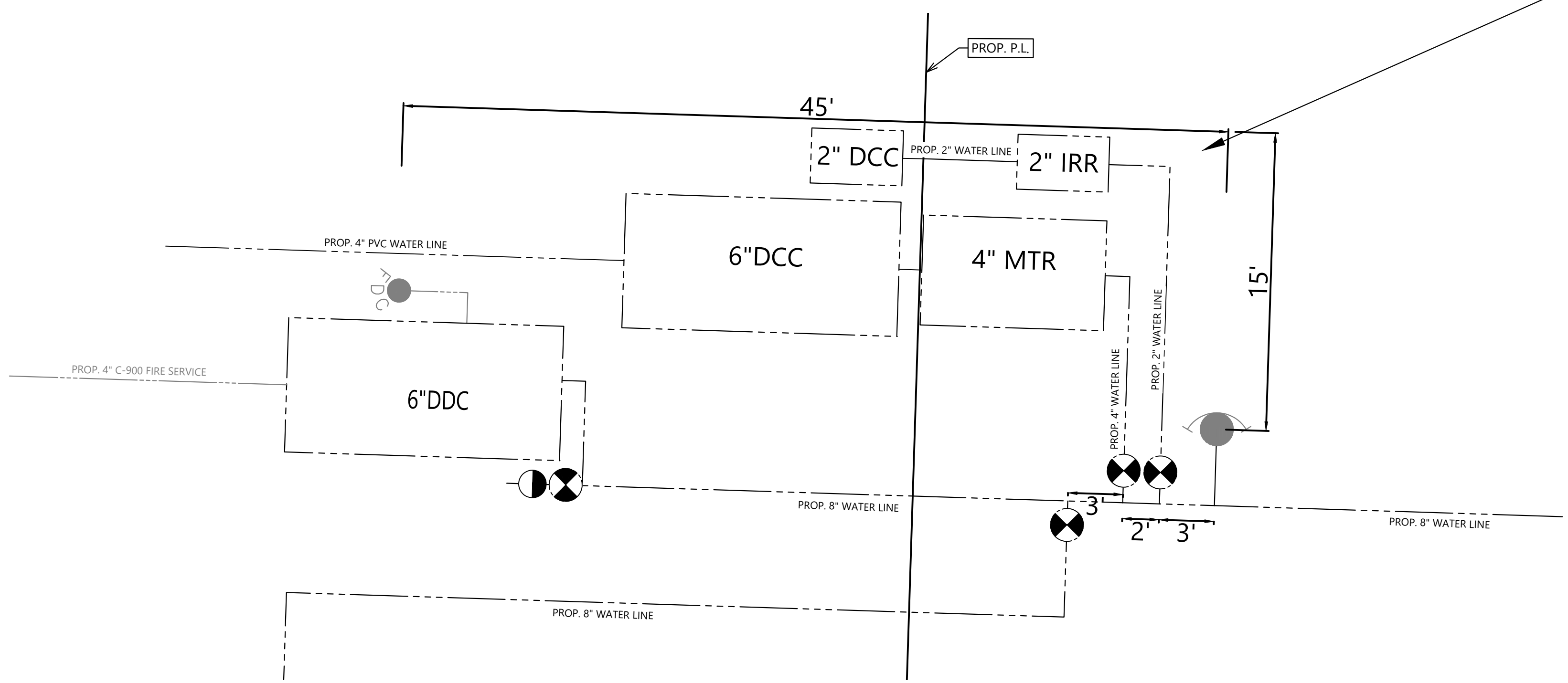
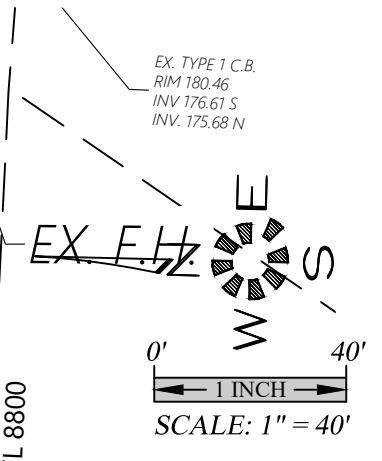
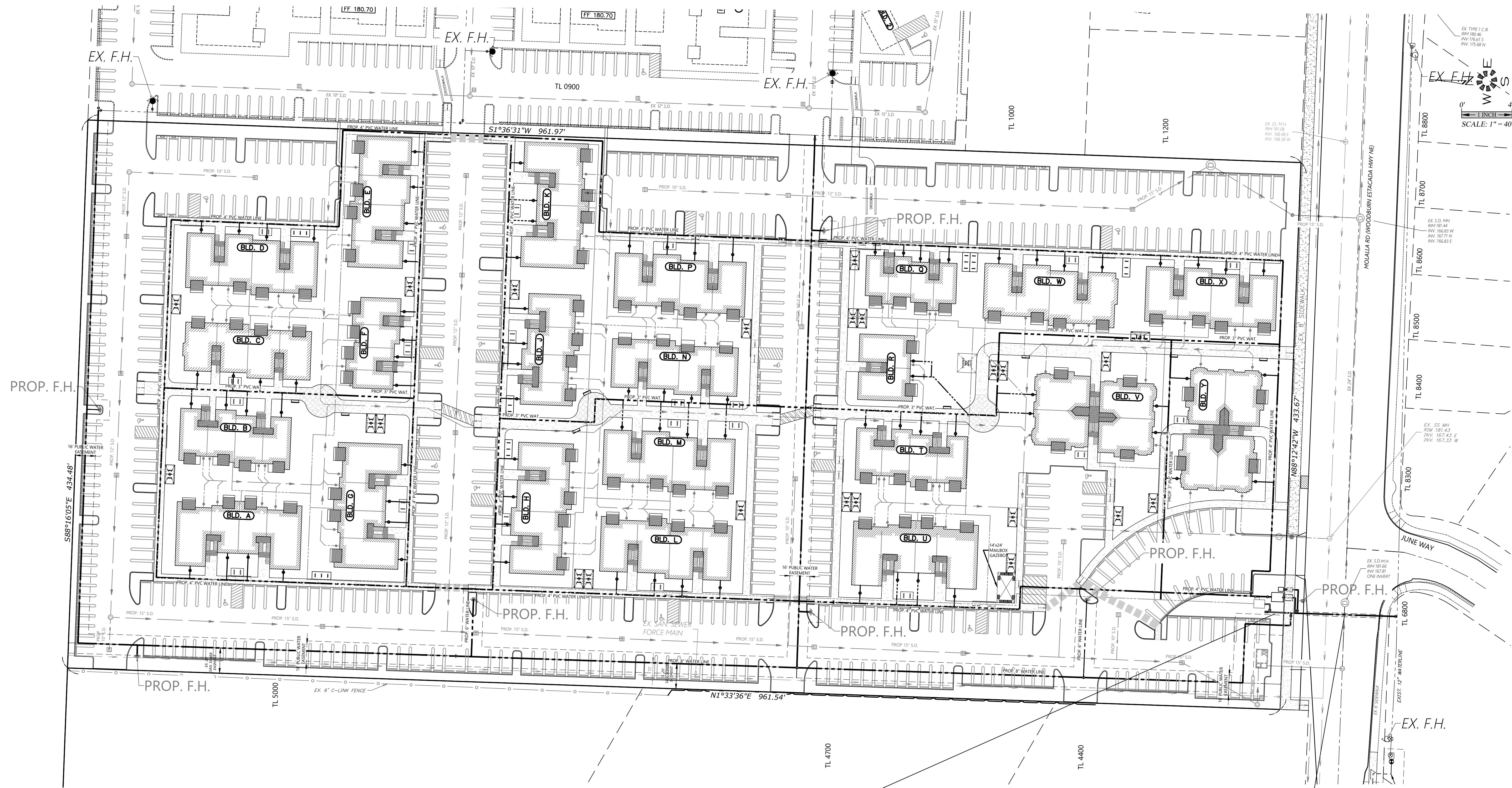
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 Checked: J.C.B.
 Date: JUNE-2022
 Scale: AS SHOWN



JOB # 7402

SDR8

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7402b SDR9-WH
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 Date: JUNE-2022
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EXPIRES 06-30-2023

JOB # 7402

SDR9

GLADETINO D824-LED Low Profile Gasmetri Luminaire

Order Form: Client Name, Project Name, Order #, City, State

Performance Data

Model	Beam Angle	Power	Output (lm)	Temp (°C)
8200K	30°	3000K	4000K	3000K
8200K	30°	3000K	4000K	3000K

Description: The Gladetino luminaire provides an aesthetically pleasing addition to any hallway or parking lot. It is made of die-cast aluminum with a powder coat finish. The luminaire is made of die-cast aluminum with a powder coat finish. The luminaire is made of die-cast aluminum with a powder coat finish.

Dimensions: 10" H, 10" W, 10" D

Mounting Options: Pole Mount, Wall Mount, Flush Mount

Options: Photocell, DALI, DMX, 0-10V, 1-10V, 0-10V, 1-10V

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

GLADETINO D824-LED Low Profile Gasmetri Luminaire

Digitize your light

Dimensions: 10" H, 10" W, 10" D

Length with Mounts: 10" H, 10" W, 10" D

Mounting Options: Pole Mount, Wall Mount, Flush Mount

Options: Photocell, DALI, DMX, 0-10V, 1-10V, 0-10V, 1-10V

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

GLADETINO D824-LED Low Profile Gasmetri Luminaire

Digitize your light

Stand-Alone Scheduled Dimming: Schedule Setup, 10 Position Selector Switch

Options: Pole Mount, Wall Mount, Flush Mount

Other Views: Top View, Side View, Front View, Back View

Mounting Hole Spacing: 10" H, 10" W, 10" D

GLADETINO D824-LED Low Profile Gasmetri Luminaire

Digitize your light

Mounting Options: Pole Mount, Wall Mount, Flush Mount

Options: Pole Mount, Wall Mount, Flush Mount

Other Views: Top View, Side View, Front View, Back View

Mounting Hole Spacing: 10" H, 10" W, 10" D

GLADETINO D824-LED Low Profile Gasmetri Luminaire

Digitize your light

Photometrics: Beam Spread, Illuminance, Foot-Candles

Lumen Chart (3000K): Table of Lumen Output vs. Power and Beam Angle

Dynamic Distribution - 3000K Lumen Output: Table of Lumen Output vs. Power and Beam Angle

Asymmetric Distribution - 3000K Lumen Output: Table of Lumen Output vs. Power and Beam Angle

GLADETINO D824-LED Low Profile Gasmetri Luminaire

Digitize your light

DLC Listed: Table of DLC Listed Models and Features

95 CRI Lumen Output: Table of Lumen Output vs. Power and Beam Angle

Projected LED Maintenance: Table of Maintenance Schedule

Rotated Glades: Gladetino LED Area Luminaire

Lumark XTOR CROSSTOUR LED

Description: The latest Lumark CrossTour LED Wall Pack Series of luminaires provides an additional area with light and energy efficient LED technology.

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

XTOR CROSSTOUR LED

Power and Lumen by Fixture Model: Table of Power and Lumen Output

Lumen Maintenance: Graph of Lumen Maintenance vs. Hours

Current Draw: Table of Current Draw vs. Power and Output

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

XTOR CROSSTOUR LED

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

SOLID STATE AREA LIGHTING VALUOLUME SERIES-PLED SPECIFICATIONS

Product Name: VALUOLUME SERIES-PLED

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

VALUOLUME SERIES - PLED

Pole Drilling Template: Diagram of Pole Drilling Template

Wall Mount: Diagram of Wall Mount

PLED Modules: Diagram of PLED Modules

Wall Plate: Diagram of Wall Plate

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

VALUOLUME SERIES - PLED LED/ELECTRICAL GUIDE

LED/Electrical Guide: Table of LED/Electrical Specifications

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

COOPER LIGHTING

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

COOPER LIGHTING

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

COOPER LIGHTING

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

COOPER LIGHTING

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

COOPER LIGHTING

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

COOPER LIGHTING

Specifications: Power, Output, Temp, Beam Angle, Mounting, Options

Applications: Street Lighting, Parking Lot Lighting, Area Lighting

Dimensions: 10" H, 10" W, 10" D

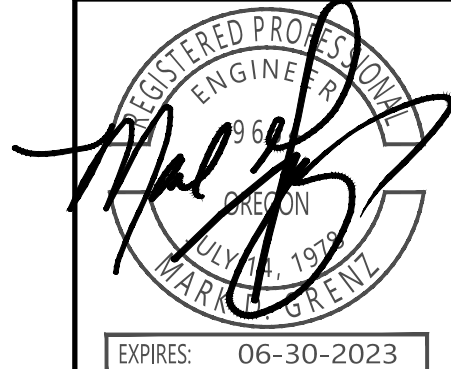
Ordering Information: Model, Power, Output, Temp, Beam Angle, Mounting, Options

PRELIMINARY PHOTOMETRIC PLAN

WOODBURN PLACE WEST

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 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

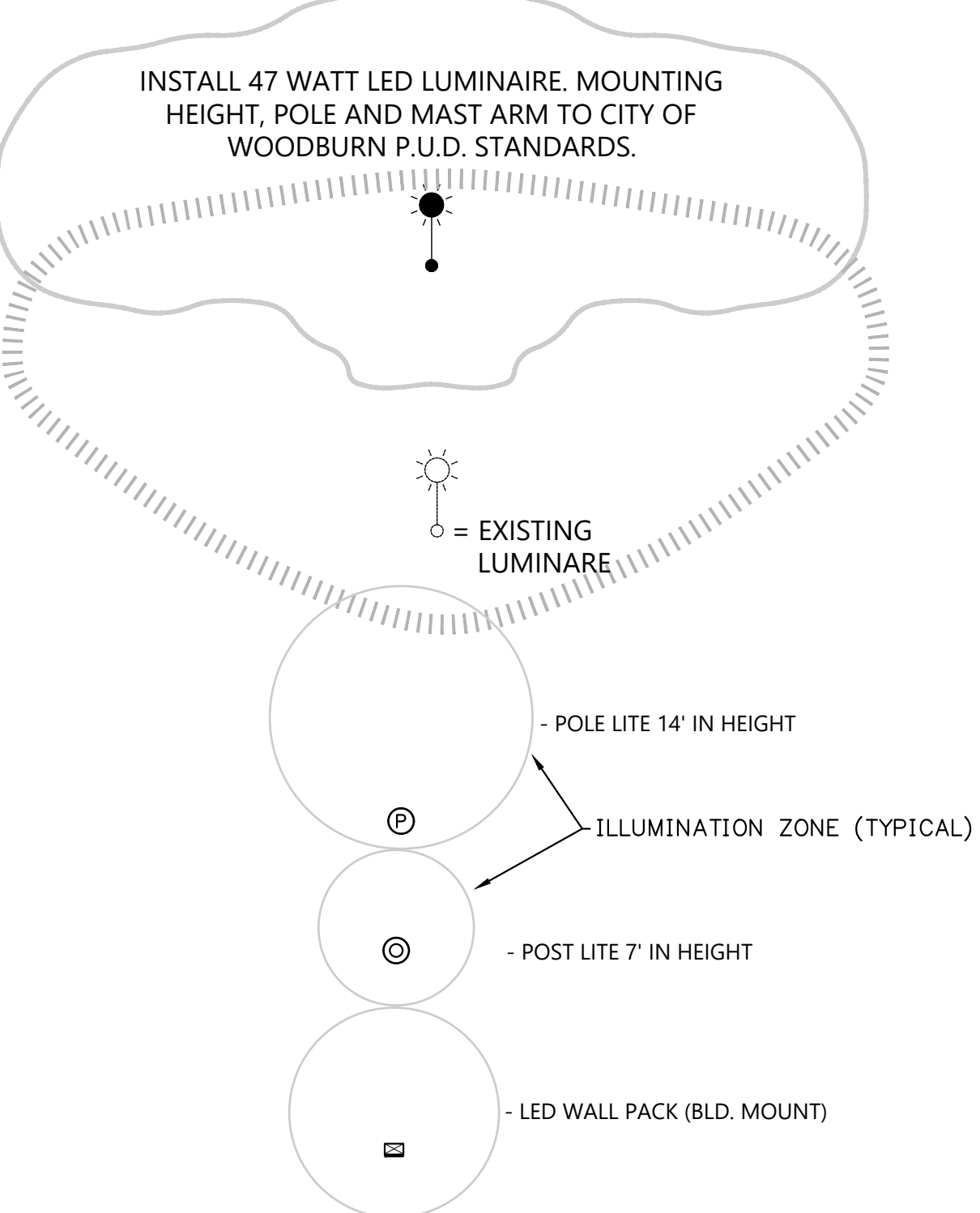
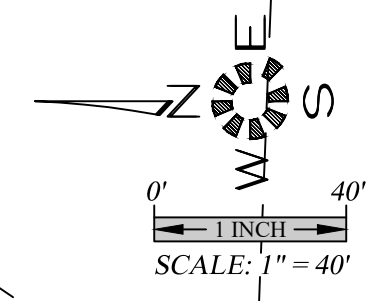
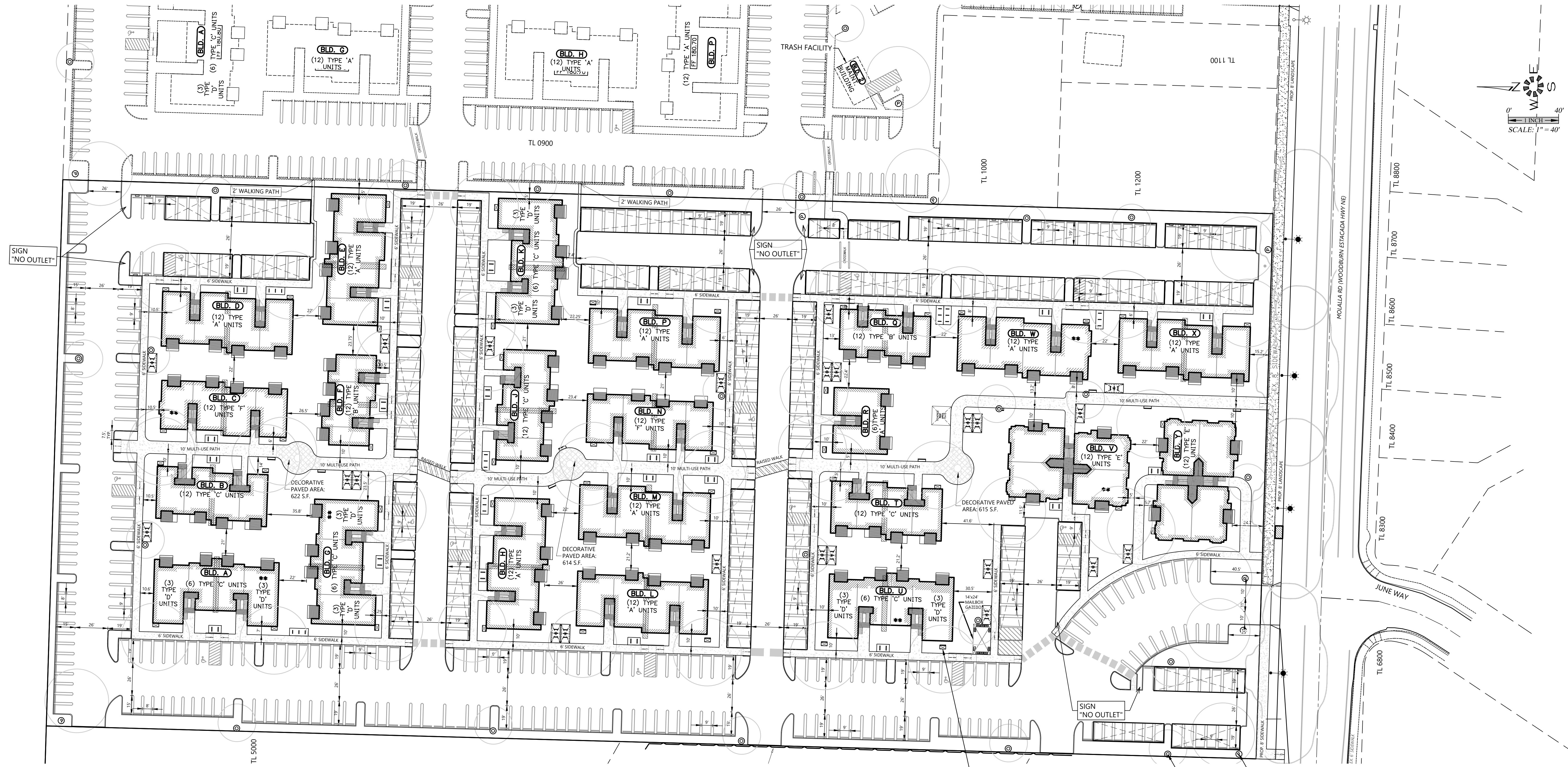
Design: M.D.G.
 Drawn: R.P.H.
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 Date: JUNE-2022
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EXPIRES 06-30-2023

JOB # 7402

SDR11



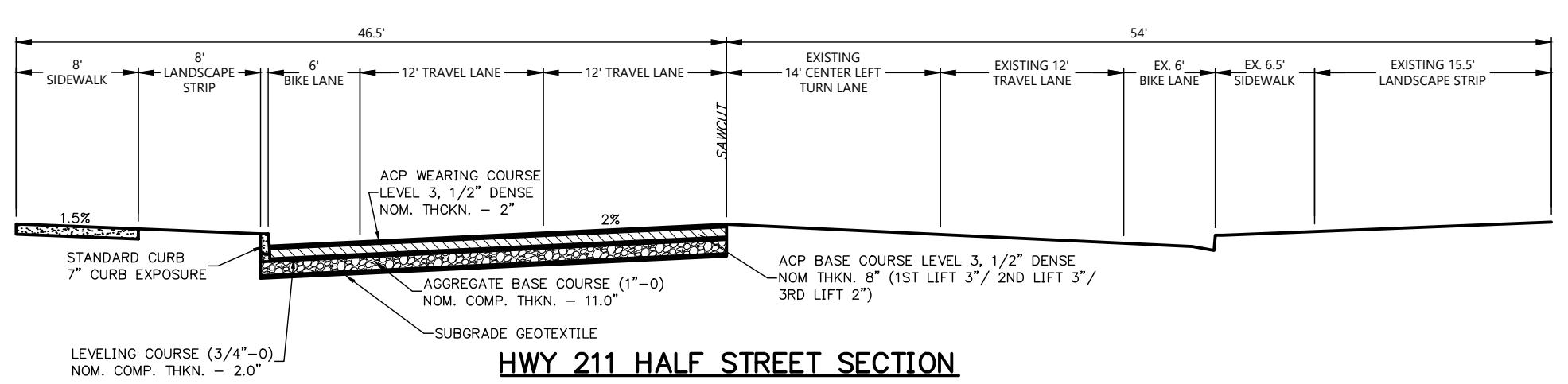
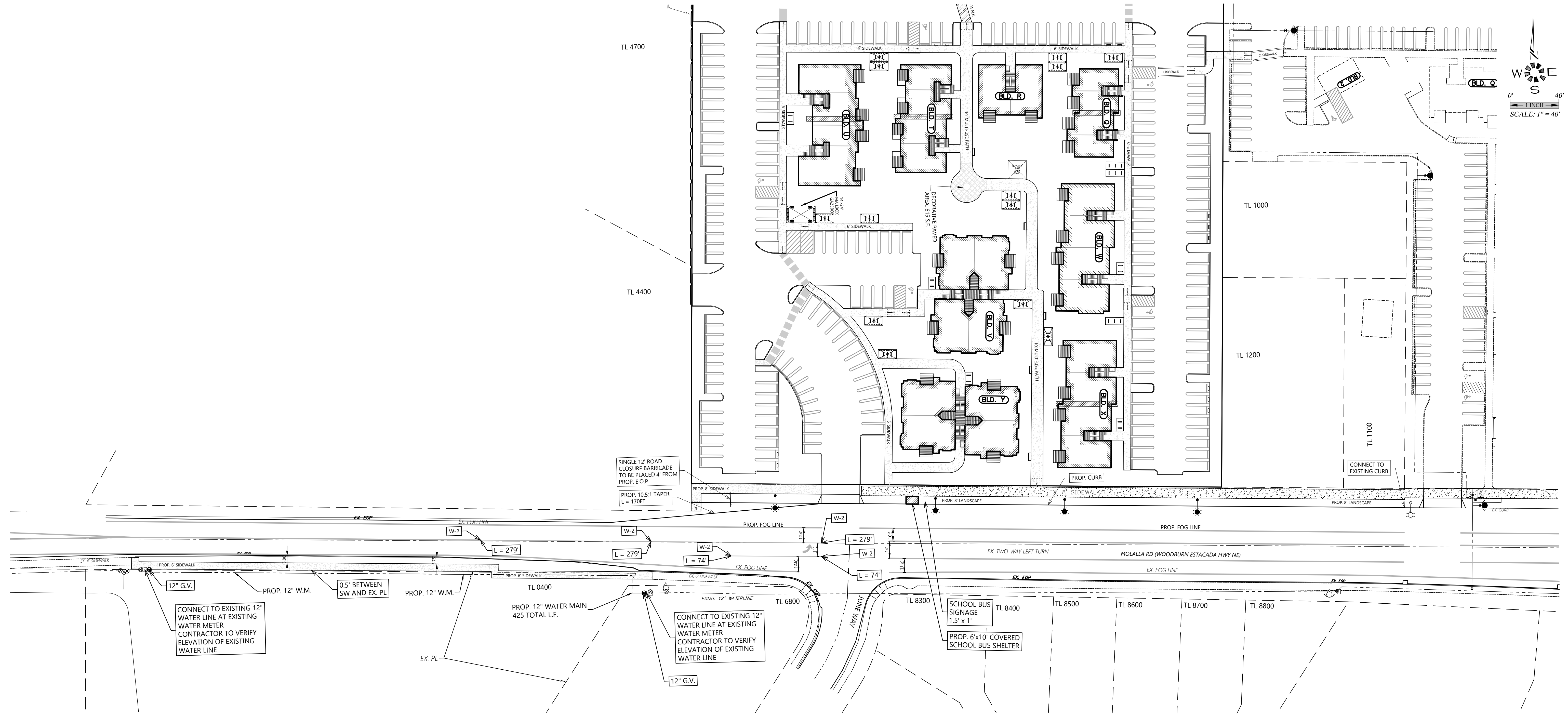
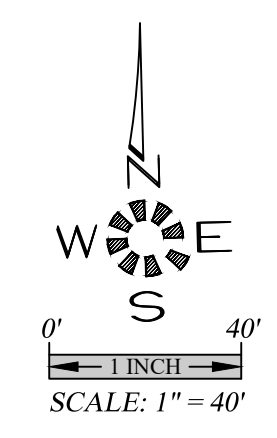
PRIVATE LUMINAIRE SCHEDULE							
PROJECT: WOODBURN APARTMENTS-WEST							
QTY	LABEL	DESCRIPTION	WATTS	LUMENS	ARM	ARRANGMENT	LLF
44	LP1-PROPOSED WALL PACK	LED, COOPER 'CROSSTOUR' XTOR48-Y WALL-PACK MOUNTED 8'	38	3995	0	SINGLE	0.8500
18	LP2-PROPOSED LIGHT	LED, LEOTEK "AR13" AR13-4M2-MV-WW-2-DB-350-WL MOUNTED ON A 7' POLE	20	2015	0.6700	SINGLE	0.8500
6	LP3-PROPOSED LIGHT	LED, U.S. ARCHITECTURAL VLL-LED-PLED-40LED-VSQ-M-350mA-240-WW-1-RAL-8019-T MOUNTED ON A 14' POLE	45	5874	0	SINGLE	0.8500

- (LP1) = INSTALL TYPICAL WALLPACK
- (LP2) = INSTALL TYPICAL LUMINAIRE MOUNTED ON A 7-FOOT POLE
- (LP3) = INSTALL TYPE V DISTRIBUTION LUMINAIRE MOUNTED ON A 14-FOOT POLE

J:\Task\7402-Woodburn\Drawings\Draw - 05\7402b.dwg - 5/26/2022 11:40:52 AM - Revision: Revision

PRELIMINARY FRONTAGE IMPROVEMENT PLAN

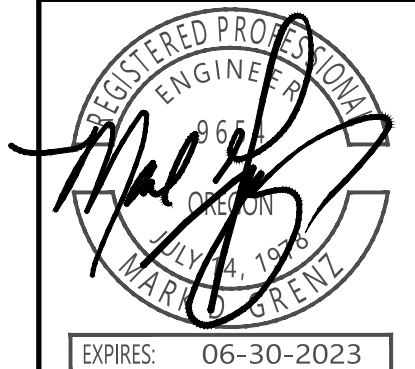
WOODBURN PLACE WEST



HWY 211 HALF STREET SECTION

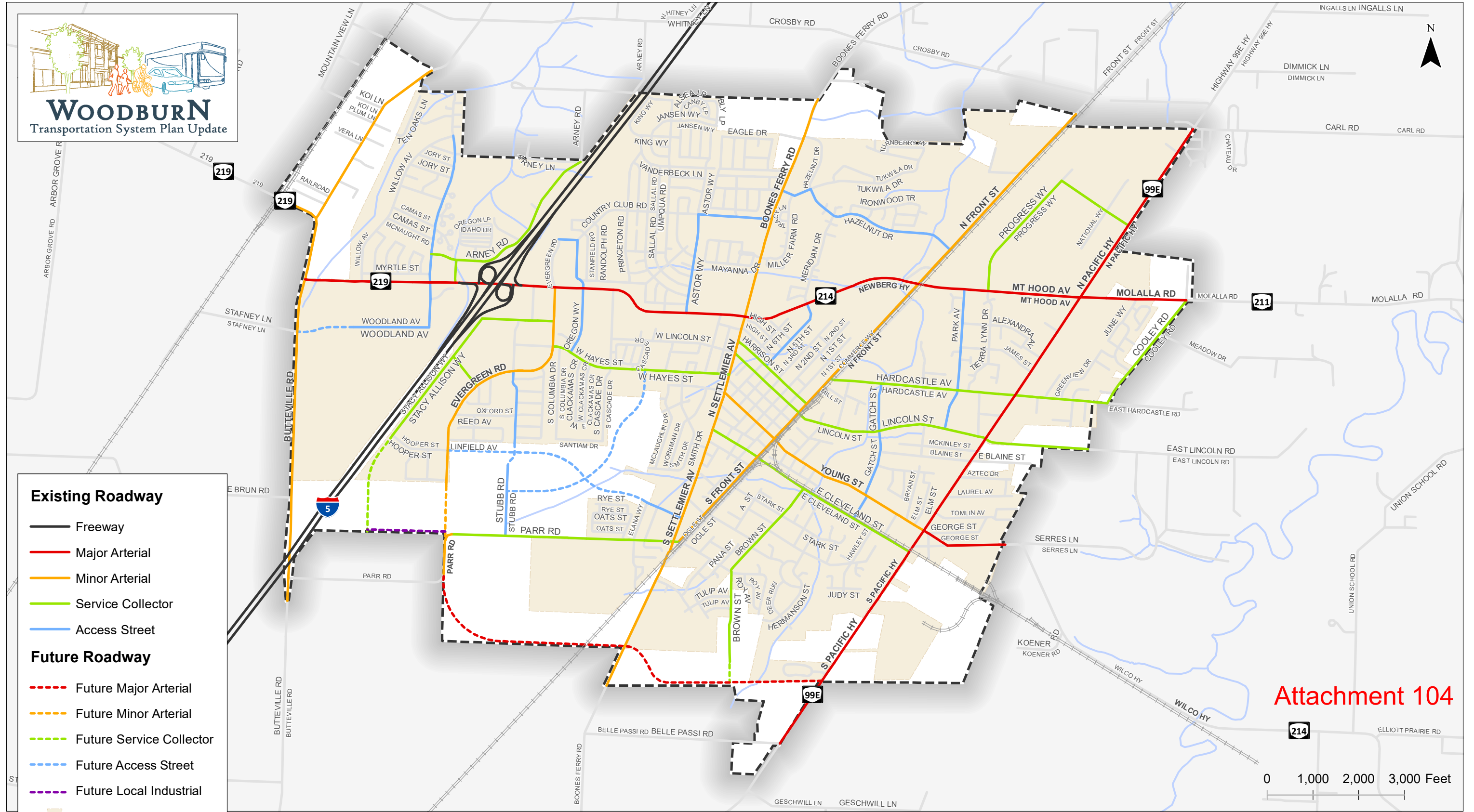
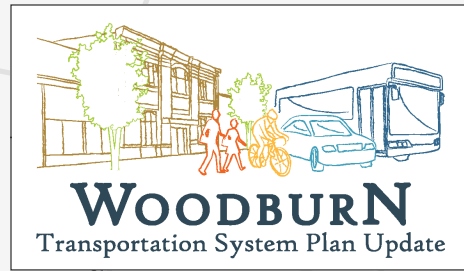
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7402B SDR12 STREET
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 JOB # 7402

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Existing Roadway

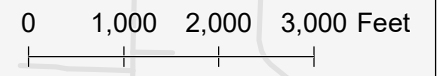
- Freeway
- Major Arterial
- Minor Arterial
- Service Collector
- Access Street

Future Roadway

- - - Future Major Arterial
- - - Future Minor Arterial
- - - Future Service Collector
- - - Future Access Street
- - - Future Local Industrial

City Boundary
 Urban Growth Boundary

Attachment 104



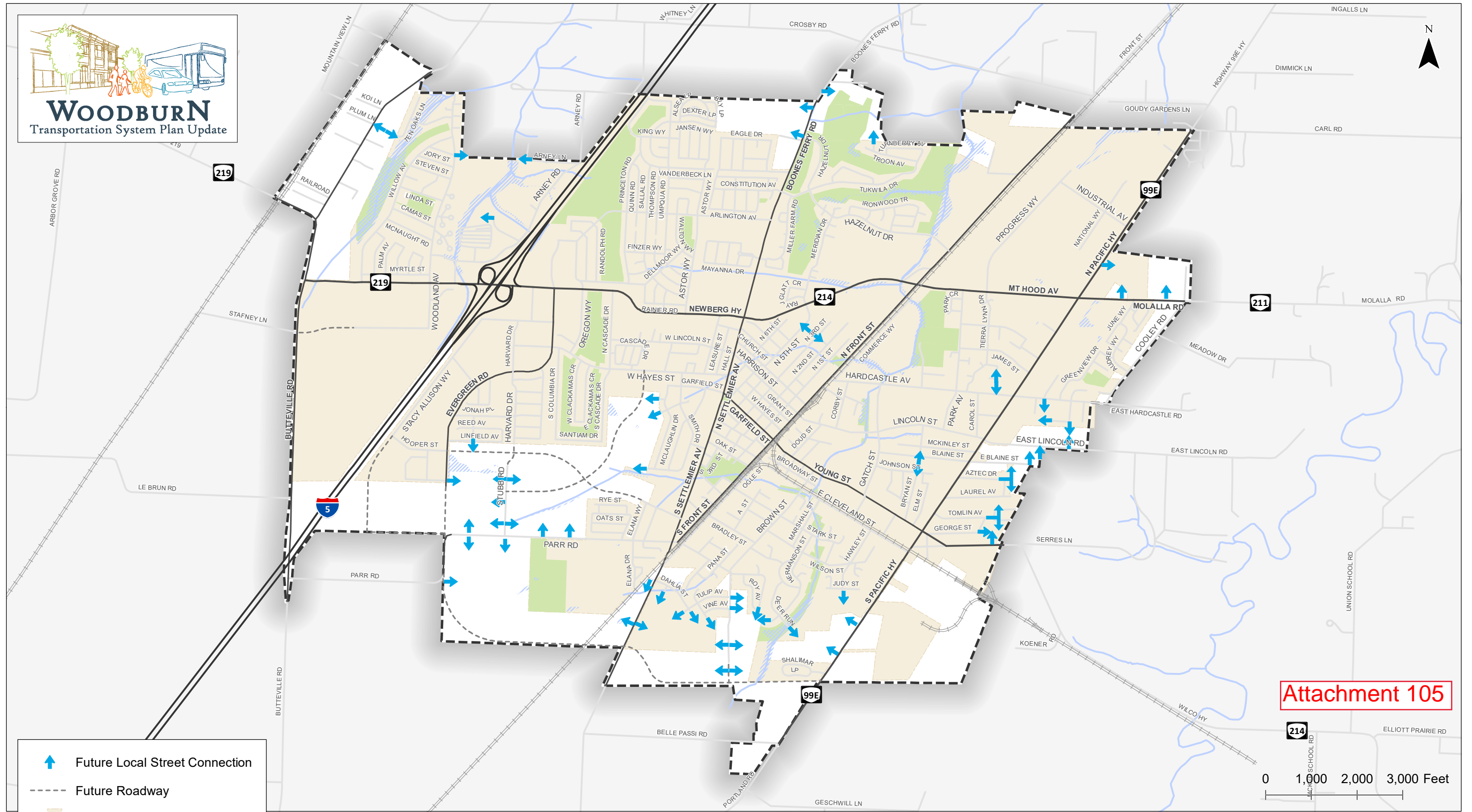
**Functional Roadway Classification
Woodburn, Oregon**

**Figure
2**





Note: Future roadway alignments are approximate and subject to further refinement.

Coordinate System: NAD 1983 HARN StatePlane Oregon North FIPS 3601 Feet Intl
 Data Source: City of Woodburn, Oregon Department of Transportation

H:\1212\1071 - Woodburn TSP Update\GIS\TSP02 Functional Roadway Classification.mxd - mmcormick - 5:25 PM 9/18/2019



Attachment 105

-  Future Local Street Connection
-  Future Roadway
-  City Boundary
-  Urban Growth Boundary

Note: Future roadway alignments are approximate and subject to further refinement.

**Local Street Connectivity Plan
Woodburn, Oregon**

**Figure
6**

H:\1212\1071 - Woodburn TSP Update\GIS\TSP\06 Local Street Connectivity Plan.mxd - mmccormick - 5:46 PM 9/16/2019

Coordinate System: NAD 1983 HARN StatePlane Oregon North FIPS 3601 Feet Intl
Data Source: City of Woodburn, Oregon Department of Transportation

CU 22-01 Woodburn Place West Apts.:

Attachment 201: Dictionary & Glossary

This document defines and explains abbreviations, acronyms, phrases, and words particularly in the context of conditions of approval.

- “ADA” refers to the federal Americans with Disabilities Act of 1990.
- “apt” refers to apartment.
- “C of O” refers to building certificate of occupancy.
- “CC&Rs” refers to covenants, conditions, and restrictions in the context of private contract among an association of owners and/or tenants within a PUD or a conventional subdivision with common area improvements.
- “CEP” refers to civil engineering plan review, which is a review process independent of land use review led by the Community Development Department Planning Division and that is led by the Public Works Department Engineering Division through any application forms, fees, and review criteria as the Division might establish. A staff expectation is that CEP follows land use review and approval, that is, a final decision, and precedes building permit application.
- “County” refers to Marion County.
- “Director” refers to the Community Development Director.
- “exc.” means excluding.
- “EV” refers to electric vehicle.
- “FOC” refers to face of curb.
- “ft” refers to feet.
- “highway” refers to Oregon Highway 211 / Molalla Road.
- “inc.” means including.
- “max” means maximum.
- “min” means minimum.
- “Modal share” means the percentage of travelers using a particular type of transportation or number of trips using a type, as examples walking, cycling, riding transit, and driving.
- “Modal shift” means a change in modal share.
- “MUTCD” refers to *Manual on Uniform Traffic Control Devices* of the U.S. Department of Transportation (U.S. DOT) Federal Highway Administration (FHWA).
- “NE” means northeast.
- “NW” means northwest.
- “OAR” refers to Oregon Administrative Rules.
- “o.c.” refers to on-center spacing, such as of trees or shrubs.
- “ODOT” refers to the Oregon Department of Transportation.

- “OR 211” refers to Oregon Highway 211 / Molalla Road.
- “ORS” refers to Oregon Revised Statutes.
- “PLA” refers to property line adjustment.
- “PU” refers to plant unit as WDO Table 3.06B describes.
- “PUE” refers to public utility easement, whether along and abutting public ROW (“streetside” PUE) or extending into or across the interior of private property (“off-street” PUE). In the context of property line adjustment, partition, or subdivision, the developer records through the plat with drawings and notes on the face of the plat. Absent this context, recordation is separate from land use review pursuant to a document template or templates established by PW. PW is the project manager for receiving, reviewing, accepting, obtaining City Council approval for, and recording public easement materials that a developer submits.
- “PW” refers to Public Works (the department).
- “Root barrier” refers to that illustrated by PW SS&Ds, [Drawing No. 1 “Street Tree Planting New Construction”](#).
- “ROW” refers to right-of-way.
- “RPZ” refers to root protection zone in the context of tree preservation.
- “SDCs” refers to system development charges, also known as impact fees.
- “SE” means southeast.
- “SDA” refers to site development area, the entire territory that is the subject of the land use application package.
- “sq ft” refers to square feet.
- “SS&Ds” refers to PW [standard specifications and drawings](#).
- “Street Corridor ‘C’” refers to a conceptual alignment of a street that implements TSP Figure 6 Local Street Connectivity Plan (2019), which through three blue arrows indicates street extensions into the northeastern area of the UGB east of OR 99E and north of OR 211, one each from OR 99E, June Way, and Cooley Road. The conceptual alignment as a street with 72 ft of ROW extends Cooley Road – hence the letter “C” – northwesterly towards the subject property, west across the north end of the subject property, and continuing west to OR 99E. The corridor refers to both the alignment and a public easement that reserves the segment of the corridor on the subject property (“reservation”) while allowing construction of private surface improvements other than buildings – and other than structures like carports and trash enclosures – and their use for an indefinite time.
- “Street Corridor ‘J’” refers to a conceptual alignment of a street that implements TSP Figure 6 Local Street Connectivity Plan (2019), which through three blue arrows indicates street extensions into the northeastern area of the UGB east of OR 99E and north of OR 211, one each from U.S. 99E, June Way, and Cooley Road. The conceptual alignment as a street with 60 ft of ROW extends June Way – hence the letter “J” – north/northwesterly across the west side of subject property and continuing west to OR 99E. The corridor refers to both the alignment and a public easement that reserves the segment of the corridor on the

subject property (“reservation”) while allowing construction of private surface improvements other than buildings – and other than structures like carports and trash enclosures – and their use for an indefinite time.

- “Street trees” refer to trees that conform to the WDO, including 3.06.03A and Tables 3.06B & C, and that have root barriers where applicable per PW [Drawing No. 1 “Street Tree Planting New Construction”](#).
- “SW” means southwest.
- “Tot.” means total.
- “TPU” means the [Transit Plan Update](#) Approved Final Report dated November 8, 2010.
- “TDM” refers to transportation demand management, which means according to the TSP (p. 82), “a policy tool as well as a general term used to describe any action that removes single occupant vehicle trips from the roadway during peak travel demand periods”, and according to Wikipedia as of October 13, 2020, “the application of strategies and policies to reduce travel demand, or to redistribute this demand in space or in time.”
- “TSP” means the [Woodburn Transportation System Plan \(TSP\)](#).
- “UGB” means urban growth boundary.
- “Walkway” refers to what would otherwise be called sidewalk except the paved walking surface is on private property outside of any of ROW or an easement granting public access.
- “WDO” refers to the [Woodburn Development Ordinance](#).
- “WFD” refers to the Woodburn Fire District.
- “Woodburn Place Apts.” refers to Woodburn Place Apartments at 2145 Molalla Rd approved through ANX 2019-01 and developed by the same developer as CU 22-01 Woodburn Place West Apts. at 2045 Molalla Rd.
- “WTS” refers to the Woodburn Transit System.
- “w/i” means within.
- “w/o” means without.
- “VCA” refers to vision clearance area as WDO 1.02 and 3.03.06 establish or as a specific condition establishes.

CU 22-01 Woodburn Place West Apts.:

Attachment 202: Future Street Corridors / Street Reservations

This attachment implements the street corridors condition of approval.

Part A. Street Corridor “C”

1. Min width 82 ft across the north end of the subject property and measured from the north lot line.
2. Details: In the legal instrument:
 - a. In the body or an Exhibit C, include a description that the easement serves to implement Woodburn Comprehensive Plan Policy H-2.2, Transportation System Plan (TSP) Figure 6 (2019), and Woodburn Development Ordinance (WDO) 3.01.05F as amended by Ordinance No. 2602 on May 9, 2022 by reserving on the subject property a segment of Street Corridor “C” for a future street that would connect Cooley Road and OR 99E and that would be ROW of 72 ft width with remainder 5-ft PUEs along the south side or both sides; and
 - b. One instance min of each of the phrases, “future street corridor”, "street reservation", and “Street Corridor ‘C’” in any of the title, body, map Exhibit B, and, if any, an Exhibit C.

Part B. Street Corridor “J”

1. Min width 70 ft across the west side of the subject property and measured from the west lot line, except that at the south end it shall curve southeasterly to align with and center on the T-intersection of the highway and June Way and the curvature shall be whatever PW would require were the easement instead a public street improved per 3.01G and the public works construction code.
2. Details: In the legal instrument:
 - a. In the body or an Exhibit C, include a description that the easement serves to implement Woodburn Comprehensive Plan Policy H-2.2, Transportation System Plan (TSP) Figure 6 (2019), and Woodburn Development Ordinance (WDO) 3.01.05F as amended by Ordinance No. 2602 on May 9, 2022 by reserving on the subject property a segment of Street Corridor “J” for a future street that would connect June Way and Street Corridor “C” and that would be ROW of 60 ft width with remainder 5-ft PUEs along the east side or both sides; and
 - b. One instance min of each of the phrases, “future street corridor”, "street reservation", and “Street Corridor ‘J’” in any of the title, body, map Exhibit B, and, if any, an Exhibit C.

Part C. Implementation

Expounding on WDO 3.01.05F.4:

1. For both corridors: Details: In the legal instrument:
 - a. Label the legal description as Exhibit A and the mapped area as Exhibit B;
 - b. In the body include text that references “land use case file number Conditional Use CU 22-01”.
 - c. In the body include text that, “This [blank] is revocable only with the written concurrence of the Community Development Director”, where [blank] is replaced with “easement” or the type of other instrument.
2. Easement/instrument: The assumed legal instrument is an off-street PUE dedicated to the City. Any legal instrument is acceptable if it meets the condition, is recorded, and otherwise satisfies PW. If the easement is an adapted off-street PUE per the PW template and relating to both corridors, then the recommended text to include within the opening paragraph would include, “..., grants to the City of WOODBURN, OREGON, hereinafter called CITY, a street reservation and public utility easement to implement Woodburn Comprehensive Plan Policy H-2.2 and Transportation System Plan (TSP) Figure 6 (2019) by reserving on the subject property Street Corridor "J" and a segment of Street Corridor "C", both for future streets, including the permanent right to construct ...”.
3. Due date: Expounding on and more restrictive than a general condition about recordation due dates, recordations are due prior to building permit application.

Part D. No Buildings and Such

No buildings and such: Per WDO 3.01.05F.3c & d, within the width and extent of a future street corridor where there is a street reservation easement instead of ROW, a developer shall not build buildings, install, mount, or place pre-fabricated buildings, or construct free-standing walls or structures such as carports and trash and recycling enclosures, and, the Public Works Director may direct a developer’s arrangement of private utility lines and facilities if and where they pass under a future street corridor.

CU 22-01 Woodburn Place West Apts.:

Attachment 203: Conditioned Fees

All of the following conditioned fees are due as applicable, whether or not mentioned directly by a condition of approval.

Refer to Condition G3 and/or Attachment 201 for a dictionary/glossary, including acronyms and shorthand text.

Part A. Fee Provisions

1. Any and all conditioned fees are in addition to, and not in place or as discounts of, any existing charge or fee however termed ordinarily assessed based on any existing ordinance, resolution, or administrative policy, inc. adopted fee schedules. If and when the City amends any ordinance, resolution, or administrative policy, inc. a fee schedule, to increase a charge or fee that is (a) the same kind of charge or fee that is conditioned, (b) the amended charge or fee amount would exceed the amount conditioned, and (c) the increase takes effect before the conditioned fee is due, then the developer shall pay the greater amount.
2. Payments of conditioned fees shall reference a final decision case file number and the condition of approval letter/number designation, be it in a check memo field or through a cover or transmittal letter.
3. For fees due by building permit issuance, a developer may request the Director to allocate payments the same as allowed for fees in-lieu by WDO 4.02.12A.2 through Ordinance No. 2603 (LA 21-02) adopted June 13, 2022, specifically, to pay across issuance of two or more structural building permits for the subject development.

For all administrative and logistical questions about payment of land use conditioned fees outside the context of assessment and payment through building permit, the developer is to contact the Administrative Assistant at (503) 982-5246 and refer to this attachment within the ANX 22-02 Marion Pointe PUD final decision.

For payment method policy details, the developer is to contact the Finance Department at (503) 982-5222, option zero, for payment method policy details.

Part B. Fee Table

<i>Table 203. Fee Table</i>					
<i>Condition Reference</i>	<i>Fee Type</i>	<i>Amount</i>	<i>Context</i>	<i>Timing</i>	<i>Staff Tracking:</i>
CU1c	Street tree fee in-lieu for street trees omitted through civil engineering plan (CEP) review, or, inspection missing tree fee	\$950 per tree	Applies to omitted street trees, or, ones missing from required number upon inspection	If CEP context, then by building permit issuance; if in inspection context, then prior to passing final inspection / obtaining certificate of occupancy	
CU2	Significant Tree removal	\$251 per tree assessed at min 7 trees	Significant Tree removal	Building permit issuance	
T-A1	Transportation: Automotive: OR 99E & OR 211/214	\$155,752	Proportionate share of 14.2% towards west-to-north right turn lane or pocket	Building permit issuance	
T-BP1	Wayfinding signage fee in-lieu	\$1,500 per location	Regardless of number of sign faces that would've been at the location	Building permit issuance	
T-T1	Bus transit and vanpool fee	\$417.27 per dwelling	See condition.	Building permit issuance	
G6 through this Attachment 205	Public Works Dept. (PW), or ODOT as applicable, civil engineering plan (CEP) review: Review by Planning Division	\$250; \$346	Original/1 st submittal; each subsequent inc. deferral/piecemeal	Upon CEP application to PW	
	Inspections by Planning Division	\$75; \$346	1st inspection or "walkthrough"; each subsequent	Inspection requests related to public (street) improvements and building permits	

Table 203. Fee Table

<i>Condition Reference</i>	<i>Fee Type</i>	<i>Amount</i>	<i>Context</i>	<i>Timing</i>	<i>Staff Tracking:</i>
G6 through this Attachment 205	Any of (1) Bond / bonding / performance guarantee or (2) public improvements deferral through PW per WDO 3.01.02E: Specifically any that would allow or allows the developer to delay construction of street improvements beyond either final plat, as applicable, or building permit issuance, with the exception of street trees.	\$4,474	Serves as bond or deferral application review request min fee and isn't a bond amount itself. Fee not applicable to warranty bonds or ordinary construction bonds if they do not authorize delay of construction of street improvements beyond building permit issuance. (See WDO 3.01.02E.)	If CEP context, then payment (through Planning Division) upon CEP application to PW; if developer applies for building permit review and there has been no CEP application to PW, then building permit issuance	

[General ledger (GL) account 363-000 3678 "Developer Contributions".]